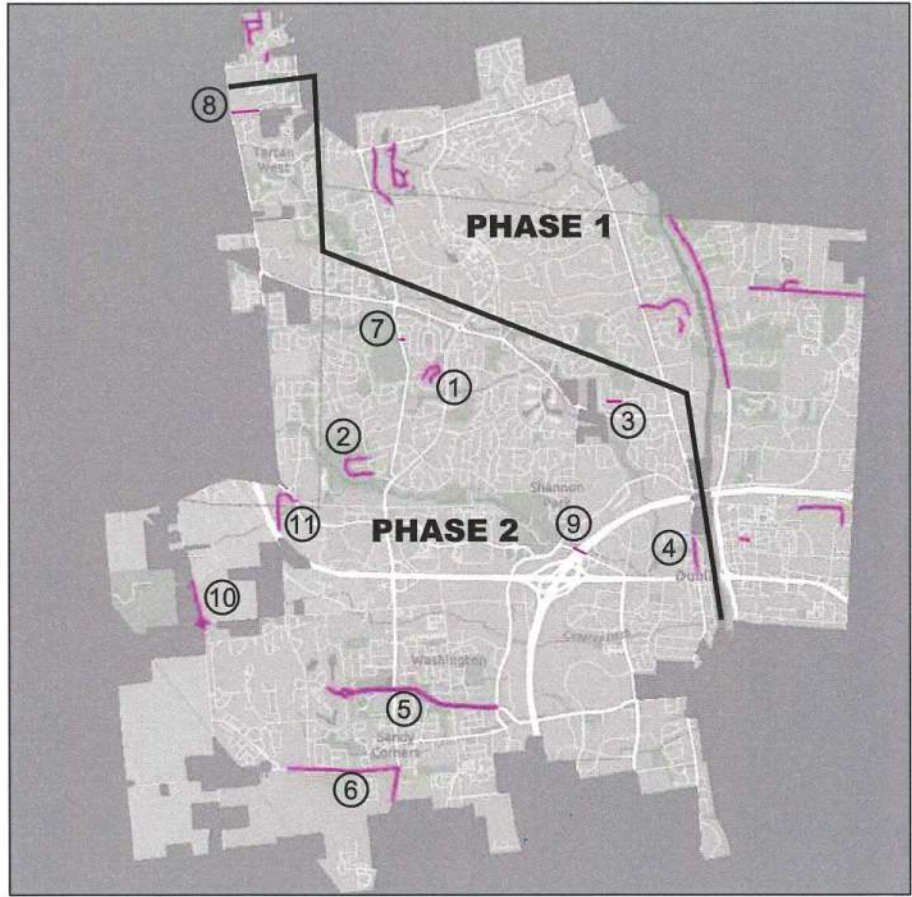


STREET MAINTENANCE PROGRAM 2025 - PHASE 2

25-002-CIP



LOCATION MAP
NTS

SHEET INDEX	
TITLE SHEET	1
GENERAL NOTES	2 - 9
MAINTENANCE OF TRAFFIC	10 - 23
GENERAL SUMMARY	24 - 28
ROADWAY IMPROVEMENTS	29 - 45
CURB RAMP DETAILS	46 - 47
TRAFFIC CONTROL PLAN	48
TRAFFIC SIGNAL PLANS	49 - 57
LIGHTING	58 - 60

MAP INDEX		
MAP NO.	LOCATION	SHEET NO.
1	MAYNOOTH DR. - WYNFORD DR.	29
2	PHOENIX PARK DR. - NEWGRANGE DR.	30
3	BALLYBRIDGE DR.	31
4	N HIGH ST.	32 - 33
5	WOERNER TEMPLE RD.	34 - 36
6	RINGS RD. - AVERY RD.	37
7	AVERY RD. - JACANA DR.	38
8	MCKITRICK RD.	39 - 40
9	POST RD.	41 - 43
10	ALTERNATE 1 - COSGRAY RD.	44
11	ALTERNATE 2 - LIGGETT RD.	45

PROJECT DESCRIPTION:

PAVEMENT RESURFACING, PAVEMENT RECONSTRUCTION, CURB RECONSTRUCTION, SIDEWALK RECONSTRUCTION, AND CURB RAMP RECONSTRUCTION OF VARIOUS CITY ROADS.

SIGNATURES BELOW SIGNIFY ONLY CONCURRENCE WITH THE GENERAL PURPOSES AND GENERAL LOCATION OF THE PROJECT AND DO NOT CONSTITUTE ASSURANCE TO OPERATE AS INTENDED. ALL TECHNICAL DETAILS REMAIN THE RESPONSIBILITY OF THE PROFESSIONAL CIVIL ENGINEER PREPARING THE PLANS.

APPROVED:

Paula A. Hammer 2-26-2025
DIRECTOR OF ENGINEERING/CITY ENGINEER
CITY OF DUBLIN, OHIO



1801 Watermark Drive, Suite 210
Columbus, OH 43215
614.210.0751 Fax 614.210.0752
Copyright: Glaus, Pyle, Schomer, Burns & DeHaven, Inc. 2023

ENGINEERS SEAL:



Kevin J. Grathwol
REGISTERED ENGINEER

02/26/25
DATE

UNDERGROUND UTILITIES
**Contact Two Working Days
Before You Dig**


Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non-members must be called directly)

STANDARD CONSTRUCTION DRAWINGS					
CITY OF DUBLIN		CITY OF COLUMBUS	ODOT		SUPPLEMENTAL SPECIFICATIONS
MD-05	RD-06	1645	BP-3.1	MT-102.20	
PD-02	RD-07		I-2A	MT-105.10	ODOT - 800
PD-03	RD-10		MT-95.30	MT-110.10	ODOT - 816
PD-04	RD-11		MT-95.31	TC-41.20	ODOT - 832
PD-06	SL-01		MT-95.32	TC-41.30	ODOT - 907
PD-10	SL-02		MT-97.10	TC-42.20	ODOT - 916
RD-01	SL-03		MT-97.11	TC-52.20	
RD-02	SL-04		MT-99.20	TC-83.20	
RD-03	SL-05		MT-101.60	TC-85.10	
RD-05			MT-101.90	AS-2-15	

OWNER

ALL REFERENCES TO THE "OWNER" OR THE "CITY" SHALL IDENTIFY THE CITY OF DUBLIN, OHIO.

GENERAL

THE REQUIREMENTS OF THE CITY OF DUBLIN, TOGETHER WITH THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS (CMSC) 2018 EDITION, THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS (ODOT CMS) 2023 EDITION AND ANY SUPPLEMENTS THERETO (HEREAFTER REFERRED TO AS STANDARD SPECIFICATIONS) SHALL GOVERN ALL CONSTRUCTION ITEMS UNLESS OTHERWISE NOTED. ITEM NUMBERS LISTED REFER TO CITY OF COLUMBUS ITEM NUMBERS UNLESS OTHERWISE NOTED.

THE CONTRACTOR INTENDING TO SUBMIT A BID FOR CITY OF DUBLIN (HEREAFTER REFERRED TO AS "CITY") CAPITAL IMPROVEMENT CONTRACTS SHALL BE PREQUALIFIED WITH THE OHIO DEPARTMENT OF TRANSPORTATION IN ACCORDANCE WITH SECTION 102 OF THE ODOT CONSTRUCTION AND MATERIAL SPECIFICATIONS AND CHAPTER 5525 OF THE OHIO REVISED CODE CONCERNING CONSTRUCTION CONTRACTS.

IT IS THE INTENTION OF THE CONTRACT DOCUMENTS TO PROVIDE AND REQUIRE A COMPLETED PROJECT READY FOR OPERATION. ANY WORK ITEMS OMITTED FROM THE CONTRACT DOCUMENTS WHICH ARE CLEARLY NECESSARY FOR COMPLETION OF THE WORK AND ITS APPURTENANCES SHALL BE CONSIDERED A PART OF SUCH WORK, THOUGH NOT DIRECTLY SPECIFIED OR CALLED FOR IN THE CONTRACT DOCUMENTS. THIS INCLUDES, BUT IS NOT LIMITED TO, SUCH INCIDENTAL ITEMS AS RELOCATION OF MAILBOXES, SAW CUTTING, AND REMOVAL AND/OR RELOCATION OF SIGNS, SPRINKLERS AND IRRIGATION COMPONENTS, OR OTHER MISCELLANEOUS ITEMS.

ALL ITEMS OF WORK CALLED FOR ON THE PLANS FOR WHICH NO SPECIFIC METHOD OF PAYMENT IS PROVIDED SHALL BE PERFORMED BY THE CONTRACTOR WITH THE COST TO BE INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS RELATED ITEMS.

THE CONTRACTOR SHALL NOTIFY THE CITY OF DUBLIN, DIVISION OF ENGINEERING AT LEAST 14 WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION AND REQUESTING APPROVAL TO COMMENCE CONSTRUCTION.

THE CITY ENGINEER SHALL NOT BE RESPONSIBLE FOR MEANS, METHODS, PROCEDURES, TECHNIQUES, OR SEQUENCES OF CONSTRUCTION THAT ARE NOT SPECIFIED HEREIN. THE CITY ENGINEER SHALL NOT BE RESPONSIBLE FOR SAFETY ON THE WORK SITE, OR FOR FAILURE BY THE CONTRACTOR TO PERFORM WORK ACCORDING TO THE CONTRACT DOCUMENTS.

THE CITY SHALL BE RESPONSIBLE TO OBTAIN ALL NECESSARY PERMITS.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL FEDERAL, STATE AND LOCAL SAFETY REQUIREMENTS, INCLUDING THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970. THE CONTRACTOR SHALL EXERCISE CAUTION ALWAYS FOR THE PROTECTION OF PERSONS (INCLUDING EMPLOYEES) AND PROPERTY. IT SHALL ALSO BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INITIATE, MAINTAIN, AND SUPERVISE ALL SAFETY REQUIREMENTS, PRECAUTIONS, AND PROGRAMS IN CONNECTION WITH THE WORK, INCLUDING THE REQUIREMENTS FOR CONFINED SPACES PER 29 CFR 1910.146

ANY MODIFICATION TO THE WORK AS SHOWN ON THESE APPROVED PLANS SHALL HAVE PRIOR WRITTEN APPROVAL OF THE CITY ENGINEER.

THE CONTRACTOR SHALL RESTRICT CONSTRUCTION ACTIVITY TO PUBLIC RIGHT-OF-WAY, AREAS DEFINED AS PERMANENT AND/OR TEMPORARY CONSTRUCTION EASEMENTS, AND/OR THE LIMITS OF DISTURBANCE SHOWN.

PROPERTY BOUNDARIES, INCLUDING PROPERTY LINES AND ROAD RIGHT-OF-WAY, ARE SHOWN FROM INFORMATION AVAILABLE AND ARE NOT NECESSARILY COMPLETE OR CORRECT.

NON-RUBBER TIRED VEHICLES SHALL NOT BE MOVED ON OR ACROSS PUBLIC STREETS OR HIGHWAYS WITHOUT WRITTEN PERMISSION FROM THE CITY ENGINEER.

THE CONTRACTOR SHALL SUBSTANTIALLY RESTORE ALL DISTURBED AREAS TO EQUAL OR BETTER CONDITION THAN EXISTED BEFORE CONSTRUCTION. DRAINAGE DITCHES OR WATERCOURSES THAT ARE DISTURBED BY CONSTRUCTION SHALL BE RESTORED TO THE GRADES AND CROSS-SECTIONS THAT EXISTED BEFORE CONSTRUCTION.

ALL NON-PAVEMENT AREAS DISTURBED WITHIN THE DESIGNATED EASEMENTS, RIGHTS-OF-WAY, AND LIMITS OF DISTURBANCE AS SHOWN, SHALL BE SEEDED AND MULCHED IN ACCORDANCE WITH ITEM 659, SEEDING AND MULCHING, AS PER PLAN. ALL AREAS DISTURBED OUTSIDE THESE LIMITS SHALL BE SEEDED AND MULCHED AT THE CONTRACTOR'S EXPENSE.

TRACKING OR SPILLING MUD, DIRT OR DEBRIS UPON STREETS, RESIDENTIAL OR COMMERCIAL DRIVES, SIDEWALKS OR SHARED-USE PATHS IS PROHIBITED AND ANY SUCH OCCURRENCE SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR AT NO COST TO THE CITY. IF THE CONTRACTOR FAILS TO REMOVE THE MUD, DIRT, DEBRIS OR SPILLAGE, THE CITY OF DUBLIN RESERVES THE RIGHT TO REMOVE THESE MATERIALS AND CLEAN AFFECTED AREAS, THE COST OF WHICH (MULTIPLIED BY 2.5) SHALL BE WITHHELD FROM MONIES THAT ARE DUE OR MAY BECOME DUE TO THE CONTRACTOR.

ALL SIGNS, LANDSCAPING, STRUCTURES, OR OTHER APPURTENANCES DISTURBED OR DAMAGED DURING CONSTRUCTION SHALL BE REPLACED OR REPAIRED TO THE SATISFACTION OF THE CITY ENGINEER. THE COST OF THIS WORK SHALL BE PAID FOR BY THE CONTRACTOR.

EXCESS EXCAVATED MATERIAL FROM THIS PROJECT SHALL BE HAULED OFFSITE BY THE CONTRACTOR AND COMPENSATION FOR HAULING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203, EXCAVATION.

MILLED SURFACES SHALL BE LIMITED TO 3 WORKING DAYS MAXIMUM. UPON COMPLETION OF PAVEMENT PLANING FOR EACH ROADWAY, THE CONTRACTOR SHALL RESURFACE SAID ROADWAY WITHIN THE SPECIFIED TIMEFRAME.

NO PARKING SIGNS SHALL NOT BE PLACED MORE THAN 3 DAYS PRIOR TO WORK COMMENCING.

DETOUR ROUTES MUST BE POSTED FOR ALL SIDEWALK OR ADA RAMP CLOSURES.

ONCE ANY CURB OR PAVEMENT IS REMOVED ON A STREET, FINAL PAVING ON THAT STREET MUST BE COMPLETE WITHIN 21 CALENDAR DAYS, UNLESS APPROVED IN WRITING BY THE ENGINEER. IF THIS DEADLINE IS NOT MET, LIQUIDATED DAMAGES WILL BE ASSESSED AT A RATE OF \$500 PER CALENDAR DAY BEYOND THE 21 DAY DEADLINE AND WORK ON NEW STREETS SHALL NOT BE PERMITTED UNTIL WORK ON STREETS EXCEEDING 21 CALENDAR DAYS IS COMPLETED.

DAILY WORK NOTIFICATION

THE CONTRACTOR SHALL SUBMIT A DAILY NOTIFICATION OF ALL SCHEDULED WORK TO BE PERFORMED TO construction@dublin.oh.us. THIS NOTIFICATION IS TO BE RECEIVED DAILY BY 7:00AM FOR THAT DAY'S PLANNED WORK. THE NOTIFICATION SHOULD ALSO INDICATE IF NO WORK IS PLANNED OR WORK IS CANCELLED DUE TO WEATHER. THE GENERAL CONTRACTOR IS RESPONSIBLE FOR THE COORDINATION AND NOTIFICATION OF WORK BY SUB-CONTRACTORS. THE CONTRACTOR SHALL INCLUDE:

- FOR EACH LOCATION AND WORK CREW:
1. WORK TO BE PERFORMED
 2. LOCATION(S) AND LIMITS OF WORK
 3. MAINTENANCE OF TRAFFIC DETAILS
 4. PLANNED HOURS OF WORK
 5. CONTRACTOR'S FOREMAN NAME AND PHONE NUMBER
 6. STREETS AND DATES WHEN RESIDENT NOTIFICATION AND PARKING RESTRICTIONS WERE DISTRIBUTED.

WORK PERFORMED WITHOUT ADVANCE NOTICE WILL BE SUBJECT TO NON-PAYMENT OR REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S COST. FOR PARTIAL DAY WORK, PROVIDE AN APPROXIMATE TIME WINDOW IN THE NOTICE AND STATE THAT THE PROJECT MANAGER WILL BE NOTIFIED VIA PHONE OR TEXT AT LEAST ONE (1) HOUR IN ADVANCE OF THE WORK TO CONFIRM WORK HOURS. AN ELECTRONIC COPY OF THE STANDARD DAILY NOTIFICATION TEMPLATE MAY BE OBTAINED VIA THE CONSTRUCTION PROJECT MANAGER.

MOBILIZATION

BY NATURE OF THE SCOPE OF THIS CONTRACT THE CONTRACTOR WILL NEED TO MOBILIZE TO DIFFERENT AREAS AT DIFFERENT TIMES. THE COSTS ASSOCIATED WITH SUCH MOBILIZATION(S) SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 624 MOBILIZATION.

UTILITIES

UTILITIES KNOWN TO BE LOCATED WITHIN THE LIMITS OF THIS PROJECT ARE LISTED BELOW WITH CONTACT INFORMATION.

COLUMBIA GAS
3550 JOHNNY APPLESEED CT.
COLUMBUS, OHIO 43231
CONTACT: ROB CALDWELL
TEL: 614-818-2104
CELL: 614-370-1906
EMAIL: rcaldwell@nisource.com

AMERICAN ELECTRIC POWER
700 MORRISON ROAD
GAHANNA, OHIO 43230
CONTACT: ROBERT MATTHEWS
CELL: 740-647-2080
EMAIL: centralhppr@aep.com

AT&T
111 N. 4TH STREET
COLUMBUS, OHIO 43215
CONTACT: DONALD MARSHALL
CELL: 614-216-2396
EMAIL: g01553@att.com

CROWN CASTLE FIBER
2 EASTON OVAL - SUITE 425
COLUMBUS, OHIO 43219
CONTACT: JON TARNOWSKI
TEL: 585-445-5813
CELL: 614-940-2462
EMAIL: jon.tarnowski@crowncastle.com
CONTACT: JOE TEPPER
EMAIL: joe.tepper@crowncastle.com

VERIZON BUSINESS
757 COMMERCE CT.
LEWIS CENTER, OHIO 43035
CONTACT: MAURICE JONES
CELL: 614-593-6685
EMAIL: maurice.jones@verizon.com
CONTACT: JOHN DEERE
CELL: 614-809-8866
EMAIL: john.deere@verizon.com
EMAIL: vzfiber-centralohio@verizon.com

EVERSTREAM
240 N 5TH STREET
SUITE 168
COLUMBUS, OHIO 43215
CONTACT: SCOTT LANCIA
TEL: 380-204-5465
CELL: 614-515-3479
EMAIL: slancia@everstream.net
EMAIL: ssaleh@everstream.net
EMAIL: kkhay@everstream.net

ALTA FIBER
201 E. 4TH STREET
CINCINNATI, OHIO 45201
BLDG: 121-900
CONTACT: BRADLEY KEARNS
TEL: 513-417-5206
EMAIL: bradley.kearns@altafiber.com
EMAIL: jtrench@altafiber.com
EMAIL: roadprojects@altafiber.com

THE CONTRACTOR SHALL GIVE NOTICE OF INTENT TO CONSTRUCT TO OHIO811 (TELEPHONE NUMBER 811 OR 800-362-2764) AND TO OWNERS OF UNDERGROUND UTILITIES THAT ARE NOT MEMBERS OF A REGISTERED UNDERGROUND PROTECTION SERVICE. NOTICE SHALL BE GIVEN AT LEAST TWO (2) WORKING DAYS PRIOR TO PERFORMING ANY EXCAVATION OR PAVEMENT OPERATIONS THAT REMOVE MORE THAN 3" OF PAVEMENT SURFACE.

THE CITY OF DUBLIN AND THE CITY ENGINEER ASSUME NO RESPONSIBILITY FOR THE ACCURACY OR DEPTHS OF UNDERGROUND FACILITIES SHOWN ON THE PLANS. IF DAMAGE IS CAUSED, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF THE SAME AND FOR ANY RESULTING CONTINGENT DAMAGE.

DUBLINK
1600 WALCUTT ROAD
COLUMBUS, OHIO 43228
CONTACT: SAM PUTKA
EMAIL: sjputka@columbusfiber.net

CHARTER COMMUNICATIONS/SPECTRUM
3760 INTERCHANGE ROAD
COLUMBUS, OHIO 43204
EMAIL: dl-moh-construction-frelo-team@charter.com

BREEZELINE
3675 CORPORATE DRIVE
COLUMBUS, OHIO 43231
CONTACT: JEREMY BORRESON
EMAIL: jborreson@breezeline.com
EMAIL: dl_cmhmf@atlanticbb.com

CITY OF DUBLIN
DIVISION OF ENGINEERING
6555 SHIER RINGS ROAD
DUBLIN, OHIO 4301
TEL: 614-410-4740

CITY OF COLUMBUS
DIVISION OF WATER
910 DUBLIN ROAD
2ND FLOOR
COLUMBUS, OHIO 43215
TEL: 614-645-7788

ZAYO GROUP
13430 NORTHWEST FREEWAY
SUITE 1100
HOUSTON, TEXAS 77040
CONTACT: WAYLON HIGGINS
CELL: 765-341-1199
EMAIL: waylon.higgins@zayo.com
zayo.relo.ohio@zayo.com
CONTACT: ERIC ALEXANDER
251 NEILSTON STREET
COLUMBUS, OHIO 43215
CELL: 614-989-9655
EMAIL: eric.alexander@zayo.com

GLOFIBER BUISNESS
1123 GOODALE BLVD.
SUITE 550
COLUMBUS, OHIO 43212
CONTACT: STACIE STEARNS
CELL: 614-570-4323
EMAIL: stacie.stearns@horizonconnects.com
CONTACT: WALTER MARTIN
CELL: 740-215-9066
EMAIL: walter.martin@horizonconnects.com

UTILITIES (CONT.)

CARE SHALL BE TAKEN DURING EXCAVATION IN THE VICINITY OF UTILITIES SUCH AS: STREETLIGHTS, TRAFFIC CIRCUITS, IRRIGATION SYSTEMS, AND OTHER UTILITIES NOT EXPRESSLY NAMED, SO AS TO NOT DAMAGE UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE TO UTILIZE CARE AND TO PROTECT ALL UTILITIES, PUBLIC AND PRIVATE. THE CONTRACTOR IS RESPONSIBLE TO SURVEY THE EXISTING CONDITIONS AND MARK THE LOCATIONS OF VISIBLE IRRIGATION SYSTEM COMPONENTS, ROOF DRAINS AND OTHER UTILITIES. THE CONTRACTOR IS TO UTILIZE CARE WHEN WORKING IN AREAS THAT CONTAIN IRRIGATION SYSTEMS. ANY DAMAGE TO IRRIGATION SYSTEMS DUE TO NEGLIGENCE ON THE PART OF THE CONTRACTOR SHALL BE REPAIRED AT THE EXPENSE OF THE CONTRACTOR. ANY DAMAGE TO SUCH UTILITIES THAT THE ENGINEER DETERMINES TO BE DUE TO THE CONTRACTOR'S NEGLIGENCE, SHALL BE REPAIRED AT THE EXPENSE OF THE CONTRACTOR, WITHOUT ADDITIONAL COST TO THE OWNER. IRRIGATION SYSTEM REPAIRS AND REPAIRS TO UNDERGROUND DOG FENCE SHALL BE COMPLETED IN A TIMELY MANNER. IRRIGATION COMPONENTS IN CONFLICT WITH PROPOSED WORK WILL BE RELOCATED (AS DIRECTED BY THE ENGINEER) BY THE CONTRACTOR UNDER FORCE ACCOUNT, OR UNDER OTHER PROVISIONS MADE BY THE CITY.

LOCATION, SUPPORT, PROTECTION, AND RESTORATION OF ALL EXISTING UTILITIES AND APPURTENANCES, SHOWN OR NOT SHOWN ON THE PLANS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

WHEN UNKNOWN OR INCORRECTLY LOCATED UNDERGROUND UTILITIES ARE ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY OWNER AND THE CITY ENGINEER.

IF DAMAGED BY THE CONTRACTOR, PRIVATE IRRIGATION SYSTEMS SHALL BE REPAIRED WITHIN 48 HOURS, AND DOG FENCES REPAIRED WITHIN 24 HOURS. REPAIR COSTS WILL BE INCIDENTAL TO THE VARIOUS ITEMS OF WORK.

MAILBOX RELOCATION

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT U.S. MAIL DELIVERY WITHIN THE PROJECT LIMITS IS NOT DISRUPTED BY CONSTRUCTION OPERATIONS. THIS RESPONSIBILITY IS LIMITED TO RELOCATION OF MAILBOXES TO A TEMPORARY LOCATION THAT WILL ALLOW THE COMPLETION OF THE WORK AND SHALL ALSO INCLUDE THE RESTORATION OF MAILBOXES TO THEIR ORIGINAL LOCATION OR APPROVED NEW LOCATION. ANY RELOCATION OF MAILBOX SERVICES MUST FIRST BE COORDINATED WITH THE U.S. POSTAL SERVICE AND AFFECTED PROPERTY OWNERS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

IF THE CONTRACTOR CHOOSES TO RELOCATE, OR REMOVE AND REPLACE MAILBOXES, THE CONTRACTOR SHALL PROVIDE A FUNCTIONAL AND ACCESSIBLE MAILBOX AT ALL TIMES. MAILBOXES RELOCATED OR REMOVED AND REPLACED BY THE CONTRACTOR FOR THE CONVENIENCE OR EFFICIENCY OF THE CONTRACTOR SHALL BE AT THE EXPENSE OF THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY THE CITY REPRESENTATIVE OF ANY STRUCTURAL DEFICIENCIES OF EXISTING MAILBOXES PRIOR TO REMOVAL OR RELOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE AS A RESULT OF THE REMOVAL/REPLACEMENT OR RELOCATION OF THE MAILBOXES. MAILBOXES SHALL BE INSTALLED PLUMB AND SECURELY AND IN ACCORDANCE WITH USPS GUIDELINES. THE CONTRACTOR SHALL REERECT ALL MAILBOXES WITHIN 5 DAYS OF THE COMPLETION OF THE CURB/CURB AND GUTTER WORK. REERECTED MAILBOXES SHALL BE PLACED AS CLOSE TO THE ORIGINAL LOCATION AS POSSIBLE.

EXISTING MAILBOXES IN DISREPAIR MAY BE REPLACED AT THE DISCRETION OF THE ENGINEER. THE CONTRACTOR WILL BE REIMBURSED FOR THE COST OF THE MAILBOX AND/OR POST IF THE ENGINEER AGREES TO THE REPLACEMENT PRIOR TO THE CONTRACTOR RELOCATING OR REMOVING AND REPLACING MAILBOX, OR IF RELOCATION DAMAGES THE MAILBOX AT NO FAULT OF THE CONTRACTOR. PAYMENT SHALL BE MADE AT THE UNIT PRICE BID PER EACH FOR ITEM SPECIAL - MAILBOX REPLACED AND SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO FURNISH AND INSTALL A NEW MAILBOX AND/OR POST WHEN DIRECTED BY THE ENGINEER.

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

25-002-CIP

SHEET

TOTAL

P.2

60

STORAGE OF MATERIALS

NO MATERIALS, INCLUDING PIPE, SHALL BE STORED WITHIN TWENTY (20) FEET OF ANY INTERSECTING STREET OR DRIVEWAY. DURING NON-WORKING HOURS, STORAGE OF EQUIPMENT SHALL COMPLY WITH THESE SAME REQUIREMENTS AND SHALL NOT IN ANY WAY RELIEVE THE CONTRACTOR OF THEIR LEGAL RESPONSIBILITIES OR LIABILITIES FOR THE SAFETY OF THE PUBLIC. NO MATERIAL SHALL BE STOCKPILED OR DUMPED WITHIN THE CITY RIGHT-OF-WAY OR ON CITY OWNED PROPERTY WITHOUT PRIOR APPROVAL FROM THE ENGINEER. ALL TRAILERS OR EQUIPMENT PARKED IN ROADWAY SHALL BE CONED OFF FOR SAFETY. LUMINARIES SHALL BE UTILIZED AS REQUESTED. EQUIPMENT AND MATERIALS SHALL NOT BE STORED IN STREETS OR RIGHTS OF WAY WITHOUT APPROVAL OF THE ENGINEER.

DRAINAGE

THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS SO AS TO MAINTAIN AT ALL TIMES STORM SEWER, DRAIN, AND DITCH FLOWS THROUGH EXISTING FACILITIES TO REMAIN IN PLACE AND THROUGH EXISTING FACILITIES TO BE REPLACED UNTIL NEW FACILITIES ARE COMPLETED AND PUT INTO SERVICE. THE FLOW OF ALL STORM SEWER, DRAINS, AND OTHER WATERCOURSES ENCOUNTERED AND DISTURBED OR DESTROYED DURING CONSTRUCTION SHALL BE RESTORED BY THE CONTRACTOR TO A CONDITION SATISFACTORY TO THE CITY ENGINEER. PAYMENT FOR THIS SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

ALL FIELD TILE BROKEN OR ENCOUNTERED DURING EXCAVATION SHALL BE REPLACED OR REPAIRED IN LIKE KIND AND CONNECTED TO THE NEAREST STORM SEWER SYSTEM OR OPEN CHANNEL OUTLET, RESTORING NORMAL FUNCTION TO THE TILE, AS DIRECTED BY THE CITY ENGINEER. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

WHERE THE WORK CALLS FOR RELOCATION OF EXISTING DITCHES OR STORM SEWER, THE CONTRACTOR SHALL REESTABLISH THE OUTLETS OF ALL PRIVATE DRAINS ENCOUNTERED; SUCH AS DOWNSPOUTS, FOUNDATION DRAINS, CATCH BASINS, YARD DRAINS, ETC., INTO THE NEAREST NEW DITCH OR STORM SEWER. THE COST FOR REESTABLISHING PRIVATE DRAIN OUTLETS SHALL BE INCLUDED IN THE VARIOUS STORM SEWER ITEMS.

ALL CONCRETE PIPE AND STRUCTURES ARE TO BE INSPECTED AT THE SUPPLIER BY THE CITY OF COLUMBUS, AND MARKED TO INDICATE APPROVAL FROM SUCH INSPECTION.

ALL CATCH BASINS AND CURB INLETS MUST BE FURNISHED WITH BIKE SAFE GRATES EJ 5110 M3.

ROADWAY UNDERDRAINS SHALL BE DISCHARGED INTO THE NEAREST STORM WATER STRUCTURE AVAILABLE ALONG THE LINE OF FLOW UNLESS SHOWN OTHERWISE ON THE PLANS.

REMOVAL OF CONSTRUCTION DEBRIS

THE CONTRACTOR IS RESPONSIBLE FOR CLEANING AND REMOVAL OF DEBRIS DUE TO CONSTRUCTION ACTIVITIES ON A DAILY BASIS. THE CONTRACTOR SHOULD DOCUMENT THE PRESENCE OF ANY DEBRIS PRESENT PRIOR TO CONSTRUCTION. ANY DEBRIS PRESENT THAT IS, IN THE ENGINEER’S OPINION, RELATED TO THE CONSTRUCTION ACTIVITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

NO CONSTRUCTION MATERIALS ARE TO BE STORED WITHIN ANY PUBLIC RIGHT-OF-WAY. THE CONTRACTOR MUST BE DILIGENT IN MAINTAINING BARRICADES TO PROTECT THE WORK AREA. ALL EXCAVATED MATERIAL AND DEBRIS SHALL BE IMMEDIATELY PLACED INTO TRUCK OR OTHER VEHICLE FOR DISPOSAL. NO TRASH SHALL BE LEFT ON SITE OR BURIED IN EXCAVATIONS. TRASH SHALL BE IMMEDIATELY PLACED INTO PROPER RECEPTACLES OR CONTAINERS AND DISPOSED OF BY THE CONTRACTOR. PAVING AREAS SHALL BE SWEEPED CLEAN OF DEBRIS WITHIN 48 HOURS OF COMPLETION OF THE PAVING OPERATIONS. ALL DRAINAGE STRUCTURES SHALL BE FREE OF DEBRIS AND IN GOOD WORKING CONDITION UPON PROJECT COMPLETION.

EROSION AND SEDIMENTATION CONTROL

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR PROVIDING NECESSARY AND ADEQUATE MEASURES FOR PROPER CONTROL OF EROSION AND SEDIMENTATION RUNOFF FROM THE SITE.

PRIOR TO CONSTRUCTION OPERATIONS IN A PARTICULAR AREA, ALL SEDIMENTATION AND EROSION CONTROL MEASURES SHALL BE IN PLACE. FIELD ADJUSTMENTS WITH RESPECT TO LOCATIONS AND DIMENSIONS MAY BE MADE BY THE ENGINEER AS REQUIRED.

CONTRACTOR SHALL PROVIDE, INSTALL, MAINTAIN AND REMOVE TEMPORARY EROSION CONTROL MEASURES AS NEEDED TO KEEP DEBRIS FROM ENTERING THE STORM SYSTEM DURING CONSTRUCTION ACTIVITIES.

TEMPORARY EROSION CONTROL FEATURES SHALL BE ADEQUATELY MAINTAINED AND SHALL BE REMOVED OR REPLACED WHEN DIRECTED BY THE CITY ENGINEER AT NO EXTRA COST TO THE CITY.

THE COSTS FOR EROSION AND SEDIMENT CONTROL SHALL BE INCLUDED AS A PART OF THE ASSOCIATED WORK ITEMS, UNLESS SPECIFICALLY DETAILED OTHERWISE IN THE CONSTRUCTION PLANS.

PAVEMENT

PAVEMENTS SHALL BE CUT IN NEAT, STRAIGHT LINES THE FULL DEPTH OF THE EXISTING PAVEMENT, OR AS REQUIRED BY THE CITY ENGINEER.

BUTT JOINTS BETWEEN EXISTING AND NEW PAVEMENT SHALL BE MADE WHERE INDICATED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH ODOT STD. CONSTRUCTION DWG. BP-3.1.

THE CONTRACTOR SHALL SUBMIT TO THE CITY ENGINEER AT THE PRECONSTRUCTION MEETING HIS PROPOSED DESIGN MIX FORMULA FOR ALL BITUMINOUS MIXTURES TO BE PLACED ON THE PROJECT FOR REVIEW AND APPROVAL. A DESIGN MIX FORMULA SHALL BE SUBMITTED FOR EACH MIXTURE AND EACH PRODUCER AND SHALL PROVIDE GRADATION OF ALL COMPONENT AGGREGATES, PERCENTAGE OF BLENDING OF AGGREGATES, PERCENTAGE OF BITUMEN, ANY ADDITIVES AND APPLICATION RATE, NAMES AND ADDRESSES OF AGGREGATE SUPPLIERS, MARSHALL MIX DESIGN DATA, AND THE THEORETICAL LABORATORY DENSITY.

THE CONTRACTOR IS NOT PERMITTED TO USE ANY RECLAIMED MATERIALS IN ITEM 304.

ITEM 441 ASPHALT CONCRETE SHALL BE SUBJECT TO THE PROVISIONS OF 448.02 DENSITY. TESTING WILL BE IN ACCORDANCE WITH SUPPLEMENT 1055 FOR DENSITY ACCEPTANCE REGARDLESS OF LENGTH OF PAVING OPERATIONS.

CONCRETE

FINISHING:
SIDEWALK / FLATWORK WIDTH AND FINISH SHALL BE THE SAME AS THE ADJACENT EXISTING SECTIONS UNLESS DIRECTED BY THE ENGINEER. ALL JOINTS AND EDGES ARE TO BE TOOLED AND RETRACED. SHARED USE PATHS AND ADA RAMPS SHALL BE SAWCUT AS DIRECTED BY THE ENGINEER.

CURING:
ALL CONCRETE SHALL BE CURED IN ACCORDANCE WITH SECTION 451.11 (CMSC) UTILIZING VOCOMP-20 WATER-BASED, ACRYLIC CONCRETE CURING AND SEALING COMPOUND, BY W.R. MEADOWS, INC.

WASHING OUT CONCRETE TRUCKS:
WASHING OUT THE CONCRETE TRUCKS ON THE STREET OR SIDEWALK SHALL NOT BE ALLOWED. CONCRETE WASHED OUT INTO EXCAVATIONS SHALL BE REMOVED WITHIN 24 HOURS. PROVIDING PROVISIONS FOR CONCRETE WASHOUT IS THE RESPONSIBILITY OF THE CONTRACTOR.

CONSTRUCTION MATERIALS

NO ALTERNATES OR DEVIATIONS SHALL BE PERMITTED FROM THOSE CONSTRUCTION MATERIALS SHOWN ON THE PLANS. BIDDERS SHALL NOT SUBMIT ALTERNATES WITH THEIR BID.

ITEM 203 EXCAVATION, ROADWAY UNDERCUT, AS PER PLAN

ALL EXCAVATION SHALL BE CONSIDERED AS UNCLASSIFIED, INCLUDING ROCK.

THIS ITEM CONSISTS OF ALL NECESSARY EQUIPMENT AND LABOR TO EXCAVATE AND HAUL UNSUITABLE REMAINING PAVEMENT AND BASE MATERIAL FROM DESIGNATED AREAS, RE-GRADE DISTURBED AREAS AND COMPACT SUBGRADE PER ITEM 204 PRIOR TO PLACING COMPACTED AGGREGATE BASE MATERIAL OR NO. 2 STONE. THIS ITEM INCLUDES SAW CUTTING OF ADJOINING PAVEMENT IF NEEDED IN ORDER TO PROVIDE A CLEAN NEAT PAVEMENT EDGE FOR THE PAVEMENT AREA SURROUNDING THE REPAIR AREA.

THIS ITEM IS NOT TO BE USED WHERE WORK IS DESIGNATED BY ENGINEER TO BE DONE UNDER ITEM 254 PAVEMENT PLANING. PAYMENT FOR THIS ITEM IS PER CUBIC YARD OF EXCAVATED MATERIAL, HAULED AWAY AND DISPOSED OF.

ITEM 203 – EXCAVATION AND ROOT PRUNING, AS PER PLAN

IN CONJUNCTION WITH EXCAVATION TO PLACE NEW PAVEMENT AT THE DESIRED GRADE, THE CONTRACTOR SHALL NEATLY SAW ANY ROOTS OVER 1” DIAMETER SUCH THAT THE ROOT ENDS ARE LEFT WITH A CLEAN CUT, AND NOT FRAYED, SPLINTERED OR BURNT. ROOTS LESS THAN 1” IN DIAMETER SHALL BE PRUNED WITH A SHARP TOOL. ROOTS SHALL NOT BE REMOVED IN A MANNER THAT MAY CAUSE THE DAMAGE CLOSE TO THE ROOT PLATE OF THE TREE. SAWS USED TO PRUNE TREE ROOTS SHALL HAVE BLADES OR CHAINS CHANGED REGULARLY AS NEEDED TO INSURE A CLEAN CUT. IF THE CONTRACTOR USES A GROUND SAW FOR ROOT PRUNING IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO EXPOSE AND PROTECT EXISTING UTILITIES (PUBLIC AND PRIVATE), THIS INCLUDES BURIED DOG FENCE, IRRIGATION, ROOF-DRAINS ETC.

STUMP GRINDERS SHALL NOT BE PERMITTED TO REMOVE TREE ROOTS. EXCAVATION INCLUDES ALL WORK AS OUTLINED IN THE CMSC, AS AMENDED HEREIN.

THIS ITEM CONSISTS OF ALL NECESSARY EQUIPMENT AND LABOR TO EXPOSE, PRUNE, TRIM OR CUT TREE ROOTS AND EXCAVATE AND HAUL SPOILS FROM DESIGNATED AREAS, RE-GRADE COMPACT DISTURBED AREAS IN ORDER TO MEET NEWLY ESTABLISHED GRADES, ADDITIONAL COMPENSATION WILL NOT BE MADE WHERE ITEM 203 IS TO BE INCLUDED IN THE UNIT COST OF OTHER ITEMS SUCH AS ITEM 202 CONCRETE/ PAVEMENT REMOVED AND DISPOSED OF, ITEM 608 CONC. FLATWORK/ WALK W/ AGG BASE, AS PER PLAN (ANY THICKNESS), ITEM 609 CURBING. PAYMENT FOR THIS ITEM IS PER CUBIC YARD OF MATERIAL EXCAVATED AND HAULED AWAY.

SUBGRADE

AT THE MCKITRICK ROAD IMPROVEMENTS, ALL SOIL SUBGRADES SHALL BE PREPARED AND COMPACTED IN ACCORDANCE WITH ITEM 204 – SUBGRADE COMPACTION, TO A DEPTH OF 12-INCHES BELOW THE SUBGRADE SURFACE. SUBGRADE SHALL MEET ITEM 204 COMPACTION REQUIREMENTS.

THE CONTRACTOR SHALL DEFINE THE LIMITS OF ANY WEAK SOILS ENCOUNTERED BY PROOF ROLLING. WHERE SOFT OR UNSUITABLE SUBGRADE IS ENCOUNTERED IN CUTS, DUE TO NO FAULT OF THE CONTRACTOR, AND SATISFACTORY COMPACTION CAN NOT BE OBTAINED, THE UNSTABLE MATERIAL SHALL BE REMOVED AND REPLACED PER SECTION 204.04, USING NO. 2 STONE CHOKED WITH 3” OF NO. 304 STONE, OR AS DIRECTED BY THE CITY ENGINEER.

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED AS AN ALLOWANCE FOR BIDDING PURPOSES:

ITEM 204 – EXCAVATION OF SUBGRADE	140 CY
ITEM 204 – GRANULAR EMBANKMENT, NO. 2 STONE	105 CY
ITEM 204 – GRANULAR EMBANKMENT, NO. 304 STONE	35 CY
ITEM 204 – GEOTEXTILE FABRIC, TYPE D	420 SY

ITEM 204 – PROOF ROLLING

THE FOLLOWING QUANTITY IS PROVIDED IN THE GENERAL SUMMARY TO ADDRESS LOCATIONS REQUIRING PROOF ROLLING:

ITEM 204 – PROOF ROLLING	3 HOURS
--------------------------	---------

GENERAL NOTES

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

25-002-CIP

SHEET

TOTAL

P.4

60

ITEM 653 – TOPSOIL FURNISHED AND PLACED, AS PER PLAN

A MINIMUM 4 INCHES OF PULVERIZED TOPSOIL SHALL BE PLACED IN ALL AREAS TO BE SEEDED OR MULCHED. PRIOR TO PLACING PULVERIZED TOPSOIL IN CUT AREAS, THE EARTH SHALL BE EXCAVATED TO A DEPTH SUFFICIENT TO PLACE 4 INCHES OF PULVERIZED TOPSOIL. THE COST OF EXCAVATION AND DISPOSAL OF SURPLUS MATERIALS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONTRACT ITEMS.

TOPSOIL SHALL BE REMOVED AND WASTED OR UTILIZED IN NON–LOAD BEARING FILLS IN ACCORDANCE WITH THE SPECIFICATIONS. NO EXTRA COMPENSATION SHALL BE PAID FOR THE REMOVAL OF EXCESS TOPSOIL AS REQUIRED TO OBTAIN A SUITABLE SUBGRADE. PAYMENT FOR TOPSOIL REMOVAL IS INCLUDED IN ITEM 203, EXCAVATION.

ALL TOPSOIL MATERIALS AND FINAL AREA OF SUBGRADE PREPARATION SHALL BE FREE FROM ROCK AND OTHER FOREIGN MATERIAL OF ½” OR GREATER IN ANY DIMENSION.

ITEM 659 – SEEDING AND MULCHING, AS PER PLAN

SEEDING AND MULCHING TO BE PER COLUMBUS C&MS ITEM 659, CLASS 1, EXCEPT FOR THE SEED SPECIFICATION AS FOLLOWS:

SEED MIXTURE

- A. THE FOLLOWING SEED MIXTURE SHALL BE USED:
SITE ONE LANDSCAPE SUPPLY, 50/50 MIX, ITEM NO. 20–13–250, OR APPROVED EQUAL.
1. TURF–TYPE PERENNIAL RYEGRASS 50:
2. KENTUCKY BLUEGRASS 50%
- B. THE SEED MIX MUST CONTAIN AT LEAST TWO (2) DIFFERENT CULTIVARS OF EACH SEED TYPE.
- C. THE SEED PURITY (AMOUNT FREE FROM INERT MATTER OR UNDESIRED NOXIOUS WEEDS) SHALL BE AT LEAST 98%.
- D. THE GERMINATION RATE OF THE APPLIED SEED SHALL BE AT LEAST 90%.

CONTRACTOR SHALL APPLY STRAW TO SEEDED AREAS.

ITEM 659 – WATER FOR IRRIGATION, AS PER PLAN

THIS WORK CONSISTS OF WATERING SEEDED AREAS TO ESTABLISH AND MAINTAIN QUALITY VEGETATIVE COVER.

SEEDED AREAS SHOULD BE WATERED A MINIMUM OF TWO TIMES PER WEEK, DEPENDING ON ADEQUATE NATURAL RAINFALL. THE CONTRACTOR SHALL PROVIDE FOR A MINIMUM OF 1” OF WATER PER WEEK FOR SEEDED AREAS (APPROXIMATELY ½” TWO TIMES PER WEEK). ADDITIONAL WATERING MAY BE REQUESTED BY THE ENGINEER. WATER FOR IRRIGATION WILL BE PAID FOR ACCEPTABLE WORK COMPLETED BY THE CONTRACTOR THAT IS VERIFIED WITH THE WATERING LOGS SUBMITTED BY THE CONTRACTOR. CONTRACTOR SHALL SUBMIT WATERING LOGS WEEKLY. THE UNIT OF MEASURE FOR WATER FOR IRRIGATION IS M GALLONS (MGAL), (M=1000)

ITEM SPECIAL – HARDWOOD MULCH

THIS WORK CONSISTS OF PLACING HARDWOOD MULCH IN PLANTING AREAS DISTURBED BY CONSTRUCTION ACTIVITY. HARDWOOD MULCH SHOULD BE PLACED FOLLOWING REMOVAL OF ALL DEBRIS FROM SITE, GRADING AND PLACEMENT OF TOPSOIL. THE MULCH SHOULD BE PLACED IN A 3 INCH LAYER, RAKED AND BLENDED INTO THE EXISTING AREA.

MATERIAL:

MULCH SHOULD BE ORGANIC, FREE FROM NOXIOUS WEEDS, MOLD OR OTHER DELETERIOUS MATERIALS AND SUITABLE FOR A TOP DRESSING OF TREES, SHRUBS AND PLANTINGS.

APPROVED MATERIALS ARE HARDWOOD MULCH FROM KURTZ BROTHERS, OR APPROVED ALTERNATE.

KURTZ BROTHERS MULCH MIXES:
MIX A: KURTZ BROTHERS HARDWOOD BLEND
MIX B: KURTZ BROTHERS AMERIMULCH BLACK, (MUIRFIELD AREA)
MIC C: KURTZ BROTHERS, EARTH BLEND LEAF COMPOST (WHERE SPECIFIED)

ALTERNATE SUPPLIER:

MIX B: OHIO MULCH ABSOLUTE BLACK (MUIRFIELD AREA)

MULCH SHOULD MATCH IN TYPE AND CONSISTENCY WITH EXISTING LANDSCAPE AREAS.

PAYMENT:

PAYMENT WILL BE FOR ALL LABOR, EQUIPMENT AND MATERIAL FOR THE PLACEMENT OF HARDWOOD MULCH BY THE CUBIC YARD (CY)

ITEM 630 – SIGNING, MISC.: 2 1/4” SQUARE ANCHOR POST

FOR STREET NAME SIGN SUPPORTS, ALL VISIBLE ELEMENTS OF THE SIGN SUPPORTS (I.E., SUPPORT POSTS, RAIN CAP) SHALL BE PAINTED. FEDERAL STANDARD COLOR #20040 FEDERAL BROWN. NUTS AND BOLTS NEED NOT BE PAINTED. PRIOR TO PAINTING, A PAINT SAMPLE SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL.

IN ADDITION TO ITEMS 630 AND 730, THE ANCHOR POST PROVIDED AND INSTALLED WITH THE GROUND MOUNTED SIGN SUPPORT SHALL BE AS PER ODOT STANDARD DRAWING TC–41.20, AND SHALL BE 36” IN LENGTH. THE ANCHOR SHALL BE 2 1/4–INCHES SQUARE, 12 GAUGE, WITH A 2 1/2–INCH OVERSLEEVE 12–INCHES LONG OVER TOP OF THE ANCHOR. THIS ITEM SHALL BE PAID FOR AT THE UNIT PRICE BID PER EACH.

PAVEMENT MARKINGS

ALL TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED OR OBSCURED PRIOR TO THE APPLICATION OF PERMANENT PAVEMENT MARKINGS. PERMANENT MARKINGS SHALL NOT BE INSTALLED UNTIL A MINIMUM OF 14 DAYS AFTER THE ASPHALT REJUVENATING AGENT IS APPLIED. ALL PAVEMENT MARKING/STRIPING SHALL BE PERFORMED WITHIN 21 DAYS OF PLACEMENT OF THE SURFACE COURSE, INCLUDING THE 14 DAY WAITING PERIOD AFTER THE REJUVENATING AGENT IS APPLIED.

THE INTENT IS TO REPLACE ALL EXISTING PAVEMENT MARKINGS IN KIND, UNLESS OTHERWISE NOTED IN THE PLANS. EXISTING WORD “ONLY” AND “MERGE” PAVEMENT MARKINGS SHALL BE REPLACED WITH LANE ARROWS. WHEN CURB RAMPS ARE RELOCATED, THE CORRESPONDING CROSSWALK LINES AND STOP LINES SHALL ALSO BE ADJUSTED ACCORDINGLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING EXISTING PAVEMENT MARKING LOCATIONS/LAYOUT PRIOR TO REMOVAL.

NO PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED WITHOUT PRIOR APPROVAL OF PRELIMINARY LAYOUT MARKINGS BY THE ENGINEER. OFFSET LONGITUDINAL JOINTS AND PAVEMENT MARKINGS BY 6 INCHES PER ODOT SCD BP–3.1.

FAILURE TO COMPLY WITH THIS NOTE WILL RESULT IN OPERATIONS BEING SHUT DOWN UNTIL BROUGHT BACK INTO COMPLIANCE.

SIGN RELOCATIONS

QUANTITIES PROVIDED FOR GROUND MOUNTED SIGN AND POST SUPPORT REMOVAL AND REERECTION ARE FOR STREET SIGNS AND STOP SIGNS IN CONFLICT WITH NEW CURB RAMPS. THE LOCATION FOR SIGN REERECTION SHALL BE PER DIRECTION OF THE ENGINEER.

ITEM 630 – REMOVAL OF GROUND MOUNTED SIGN AND STORAGE, AS PER PLAN

ALL EXISTING GROUND MOUNTED SIGNS WITHIN THE PROJECT AREA SHALL BE REMOVED BY THE CONTRACTOR AND DELIVERED TO THE CITY OF LOCATION TO BE DETERMINED WITHIN THE DUBLIN CITY LIMITS. THE CONTRACTOR SHALL COMPENSATE THE CITY IN AN AMOUNT EQUAL TO THE REPLACEMENT COST OF ANY SIGNS DAMAGED AS A RESULT OF THE CONSTRUCTION OPERATIONS. PAYMENT FOR THIS ITEM SHALL BE MADE AT THE UNIT PRICE BID PER EACH SIGN REMOVED AND DELIVERED.

ITEM 644 – PAVEMENT MARKING, MISC.: YIELD BAR, WHITE, 24”. (2’ LONG, 2’ GAP)

THIS ITEM SHALL MEET REQUIREMENTS FOR ITEM 644 IN THE C&MS. THE YIELD BAR SHALL BE 24” WIDE AND HAVE A 2’ SEGMENT. THERE SHALL BE A 2’ GAP BETWEEN EACH SEGMENT. THE CONTRACTOR SHALL PREMARK THE YIELD BAR IN ACCORDANCE WITH ODOT’S C&MS 641.06 FOR REVIEW BY THE CITY ENGINEER. UPON THE CITY ENGINEERS APPROVAL, THE CONTRACTOR MAY COMMENCE PERMANENT STRIPING OPERATIONS.

PAYMENT FOR THIS ITEM SHALL BE PER FOOT AT THE CONTRACT UNIT PRICE FOR ITEM 644 – PAVEMENT MARKING, MISC.: YIELD BAR, WHITE, 24”, (2’ LONG, 2’ GAP).

MAINTENANCE OF TRAFFIC PAVEMENT MARKING RESTORATION (POST ROAD)

WHEN IT IS NECESSARY FOR MAINTENANCE OF TRAFFIC LIMITS TO EXCEED THE WORK LIMITS, THE CONTRACTOR SHALL BE RESPONSIBLE TO RESTORE PAVEMENT MARKINGS TO THEIR ORIGINAL STATE.

IN LIEU OF A PAVEMENT MARKING PLAN, THE CONTRACTOR SHALL, PRIOR TO THE START OF CONSTRUCTION, PREPARE AN INVENTORY AND LOG ALL EXISTING PAVEMENT MARKINGS FOR USE IN RESTORING THE MARKINGS AT THE END OF CONSTRUCTION. THE CONTRACTOR SHALL DELIVER TWO (2) COPIES OF THE INVENTORY AND LOG TO THE ENGINEER BEFORE BEGINNING ANY PAVEMENT REMOVALS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LAYOUT OF THE VARIOUS FINAL PAVEMENT MARKINGS, INCLUDING LOCATION OF NO PASSING ZONES, CENTER LINES, EDGE LINES AND STOP LINES ON THE FINAL SURFACE COURSE IN ACCORDANCE WITH SECTION 641.06.

UNLESS DIRECTED OTHERWISE BY THE ENGINEER, THE FINAL PAVEMENT MARKINGS SHALL BE RESTORED IN THEIR ORIGINAL PATTERNS AND LOCATION. FINAL LOCATION OF ALL PAVEMENT MARKINGS (PRE–LINE LAYOUT) SHALL BE APPROVED BY THE CITY IN THE FIELD.

THE COST OF LOGGING AND REMARKING SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS PAVEMENT MARKINGS ITEMS. NO SEPARATE PAVEMENT WILL BE MADE.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 644 – LANE LINE, 4”	0.26 MILE
ITEM 644 – CENTER LINE	0.02 MILE
ITEM 645 – LANE LINE, 4”, TYPE A1, WITH CONTRAST	0.05 MILE

ITEM 630 – GROUND MOUNTED SIGN, NO. 3 POST, AS PER PLAN

ALL SIGN SUPPORTS SHALL BE MANUFACTURED TO THE CITY OF DUBLIN SPECIFICATIONS PER STANDARD DRAWING RD–10. SIGN SUPPORTS SHALL BE 2–INCH SQUARE GALVANIZED POSTS WITH DIE CUT KNOCK OUTS (ALLIED QUICK–PUNCH SUPPORTS OR APPROVED EQUAL). A SINGLE BREAKAWAY ANCHOR SHALL BE USED. A 2–INCH GALVANIZED RAIN CAP SHALL BE INSTALLED ON EACH POST, ABOVE THE TOP OF THE SIGN FACE. ALL SIGNS SHALL BE ERECTED WITH A 7–FOOT VERTICAL CLEARANCE BETWEEN THE TOP OF THE CURB AND THE BOTTOM OF EACH SIGN, UNLESS OTHERWISE DESIGNATED BY THE CITY ENGINEER. HORIZONTAL CLEARANCE FOR BOTH CURB AND DITCH SECTIONS SHALL BE AS PER ODOT STANDARDS AND SHALL ALLOW AT LEAST 2–FT HORIZONTAL CLEARANCE FROM THE FACE OF CURB. THE ANCHOR POST SHALL BE PAID FOR SEPARATELY. PAYMENT FOR THIS ITEM SHALL BE FOR THE LENGTH ONLY, INCLUDING THE 8” OVERLAP IN THE ANCHOR POST, AND ALL MISCELLANEOUS ATTACHMENT HARDWARE.

ITEM 630 – SIGNING, MISC.: REBOUNDABLE DELINEATOR POST ASSEMBLY

REBOUNDABLE DELINEATOR POST SHALL BE ROUND, WHITE, POLYURETHANE, 36” TALL, 3” ROUND POST WITH TWO (2) REFLECTIVE COLLARS. MUST BE OMUTCD COMPLIANT PER SECTION 31.05, LATEST EDITION.

THE FOLLOWING MANUFACTURERS SHALL BE USED OR AN APPROVED EQUAL:

TRAFFIC SAFETY STORE
1247 WRIGHT’S LANE, UNIT E
WEST CHESTER, PA 19380
1–800–429–9030
ITEM #: SU36WHT2WRCA–BLK

GRAINGER
CANTON BRANCH #163
1721 6TH ST SW
CANTON OH 44706
1–800–472–4643
ITEM #: 3UTX4
MODEL: 04–736W

ULINE
12575 ULINE DR
PLEASANT PRAIRE, WI 53158
1–800–295–5510
MODEL NO. H–4465W

PAYMENT FOR ITEM 630 – SIGNING, MISC.: REBOUNDABLE DELINEATOR POST ASSEMBLY SHALL BE MADE AT THE CONTRACT UNIT PRICE BID PER EACH. PAYMENT SHALL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS, TOOLS, EQUIPMENT, AND ALL PARTS NECESSARY TO INSTALL ONE POST.

ITEM 630 – SIGNING, MISC.: REMOVAL AND DISPOSAL OF REBOUNDABLE DELINEATOR POST ASSEMBLY

THE CONTRACTOR SHALL REMOVE AND DISPOSAL THE EXISTING REBOUNDABLE DELINEATOR POST ASSEMBLY.

PAYMENT FOR ITEM 630 – SIGNING, MISC.: REMOVAL AND DISPOSAL OF REBOUNDABLE DELINEATOR POST ASSEMBLY SHALL BE MADE AT THE CONTRACT UNIT PRICE BID PER EACH. PAYMENT SHALL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS, TOOLS, EQUIPMENT, AND ALL PARTS NECESSARY TO REMOVE ONE POST.

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

25-002-CIP

SHEET

TOTAL

P.5

60

ITEM 604 – MANHOLE, CATCH BASIN OR INLET, ADJUSTED TO GRADE, AS PER PLAN

THE CONTRACTOR WILL MAKE ALL FINAL GRADE ADJUSTMENT OF MANHOLE, CATCH BASIN AND INLET COVERS, AND SIMILAR FRAME ASSEMBLIES USING INJECTION MOLDED HIGH DENSITY POLYETHYLENE (HDPE) ADJUSTMENT RINGS WHERE PRACTICAL. THE ADJUSTMENT RINGS SHALL BE MANUFACTURED FROM POLYETHYLENE PLASTIC AS IDENTIFIED IN ASTM DESIGNATION D-1248 (STANDARD SPECIFICATION FOR POLYETHYLENE PLASTIC MOLDING AND EXTRUSION MATERIALS).

THE GRADE ADJUSTMENTS AND REPAIRS FOR INLET COVERS AND SIMILAR SHALL BE MADE WITH THE USE OF CLAY BRICK AND MORTAR OR CONCRETE.

DAMAGED CASTINGS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND, IF DIRECTED, SHALL BE REPLACED. THE COST OF THE CASTING(S) REPLACED SHALL BE IN ADDITION AS PROVIDED FOR IN THE GENERAL SPECIFICATIONS.

INSTALLATION SHALL BE PER MANUFACTURER’S RECOMMENDATIONS ONLY. THE STRUCTURE SHALL BE PROPERLY PREPARED PRIOR TO INSTALLATION OF ADJUSTMENT RINGS AND SHALL BE CLEAN AND FREE OF UNSOUND MATERIAL OR LOOSE DEBRIS. THE CASTING SHALL BE REMOVED, ALL BRICKS SHALL BE REMOVED AND DOWN TO THE ELEVATION OF THE FIRST SOLID COURSE. THE CONTRACTOR SHALL LAY NEW CLAY BRICK AS NEEDED TO LEVEL STRUCTURE AND PROVIDE LEVEL SEAT FOR INSTALLATION OF ADJUSTMENT RINGS. STRUCTURE SHALL BE PARGED AFTER INSTALLATION OF ADJUSTMENT RINGS. THE ANNULAR SPACE BETWEEN THE RINGS AND CONE BASIN, THE RINGS, AND THE RINGS AND COVER FRAME SHALL BE SEALED UTILIZING AN APPROVED BUTYL SEALANT. THE OVERDIG AROUND THE STRUCTURE, AFTER BEING CLEANED OF LOOSE DEBRIS AND EXCESS CONCRETE, SHALL BE FILLED WITH LSM, TYPE 2. THE WORK PERFORMED UNDER THIS SECTION IS PER STRUCTURE REGARDLESS OF NUMBER OF CASTINGS SITUATED ON STRUCTURE. LEVELING OF CASTING OR STRUCTURE AND THE FILLING OF VOIDS WITH CONCRETE SHALL NOT CONSTITUTE WORK FOR PAYMENT UNDER THIS SECTION, BUT RATHER BE INCIDENTAL TO CURB PLACEMENT. DRAINAGE STRUCTURE ADJUSTMENTS AND REPAIRS SHALL BE COMPLETED PRIOR TO FINISHING CURB AND GUTTER.

ITEM 604 – MANHOLE, CATCH BASIN OR INLET RECONSTRUCTED TO GRADE, AS PER PLAN

WORK SHALL BE PERFORMED UNDER THIS SECTION AS DIRECTED TO RECONSTRUCT EXISTING STORM STRUCTURES. THE WORK SHALL INCLUDE ALL NECESSARY LABOR AND MATERIALS TO COMPLETELY RECONSTRUCT THE STRUCTURES WHEN REPAIRS ARE REQUIRED GREATER THAN FOUR (4) FEET BELOW THE TOP OF THE STRUCTURE (NOT INCLUDING THE TOP OR CASTING.)

THE CONTRACTOR SHALL REMOVE AND STORE THE TOP OR CASTING FOR RE-INSTALLATION. EXCAVATE AS REQUIRED AROUND THE STRUCTURE IN ORDER TO REMOVE EXISTING BRICK WALLS OR DETERIORATED CONCRETE DOWN TO THE FOUNDATION OF THE STRUCTURE OR AS DIRECTED BY THE ENGINEER. THE STRUCTURE SHALL BE PROPERLY PREPARED PRIOR TO INSTALLATION OF NEW BRICK AND SHALL BE CLEAN AND FREE OF LOOSE DEBRIS. THE CONTRACTOR SHALL LAY NEW CLAY BRICK WALLS UTILIZING TYPE S MORTAR, ENSURING PLUMB AND LEVEL COURSES. THE STRUCTURE SHALL BE PARGED INSIDE AND OUT, PRIOR TO BACKFILL. THE OVERDIG AROUND THE STRUCTURE, AFTER BEING CLEANED OF LOOSE DEBRIS AND EXCESS CONCRETE, AND SHALL BE FILLED WITH LSM, TYPE 2 OR OTHER MATERIAL SUITABLE TO THE ENGINEER. THE CASTING SHALL BE INSTALLED AND LEVELED USING INJECTION MOLDED HIGH DENSITY POLYETHYLENE (HDPE) ADJUSTMENT RINGS WHERE PRACTICAL. THESE ADJUSTMENT RINGS SHALL BE MANUFACTURED FROM POLYETHYLENE PLASTIC AS IDENTIFIED IN ASTM DESIGNATION D-1248 (STANDARD SPECIFICATION FOR POLYETHYLENE PLASTIC MOLDING AND EXTRUSION MATERIALS). INSTALLATION SHALL BE PER MANUFACTURE’S RECOMMENDATIONS ONLY. DRAINAGE STRUCTURE ADJUSTMENTS AND REPAIRS SHALL BE COMPLETED PRIOR TO FINISHING CURB AND GUTTER.

PAYMENT FOR THIS ITEM WILL BE MADE PER EACH (EA) STRUCTURE RECONSTRUCTED TO GRADE. THE WORK PERFORMED UNDER THIS SECTION IS PER STRUCTURE REGARDLESS OF NUMBER OF CASTINGS SITUATED ON THE STRUCTURE.

ITEM SPECIAL – INLET REPAIR AND PARGE

THE CONTRACTOR WILL MAKE ALL FINAL GRADE ADJUSTMENT OF MANHOLE, CATCH BASIN AND INLET COVERS, AND SIMILAR FRAME ASSEMBLIES USING BRICK AND TYPE S MORTAR. DAMAGED CASTINGS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND IF DIRECTED SHALL BE REPLACED. THE COST OF THE CASTING(S) REPLACED SHALL BE IN ADDITION AS PROVIDED FOR IN THE GENERAL SPECIFICATIONS.

THE STRUCTURE SHALL BE PROPERLY PREPARED PRIOR TO REPAIR AND SHALL BE CLEAN AND FREE OF UNSOUND MATERIAL OR LOOSE DEBRIS. SINCE THE CASTING MAY NOT BE REMOVED, ALL BRICKS AND DEBRIS SHALL BE REMOVED AND DOWN TO THE ELEVATION OF THE FIRST SOLID COURSE WITHIN THE STRUCTURE. THE COURSES SHALL BE REPLACED AND PARGED WITHIN THE STRUCTURE. LEVELING OF CASTING OR STRUCTURE AND THE FILLING OF VOIDS WITH CONCRETE SHALL NOT CONSTITUTE WORK FOR PAYMENT UNDER THIS SECTION, BUT RATHER BE INCIDENTAL TO CURB PLACEMENT. INLET REPAIRS SHALL BE COMPLETED PRIOR TO FINISHING CURB AND GUTTER.

PAYMENT SHALL BE MADE AT THE UNIT PRICE BID PER EACH FOR ITEM SPECIAL – INLET REPAIR AND PARGE AND SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY FOR THE WORK NOTED ABOVE.

ITEM SPECIAL – DRAIN LEADER PIPE

THIS ITEM CONSISTS OF THE REMOVAL AND REPLACEMENT OF EXISTING ROOF DRAIN PIPES PERFORMED IN CONJUNCTION WITH SIDEWALK AND CURB AND GUTTER REPLACEMENT OR OTHER WORK AS DIRECTED BY THE ENGINEER AND IN ACCORDANCE WITH THE CMSC. CONNECTIONS ARE TO BE APPROVED BY THE CITY.

ALL DOWNSPOUT DRAIN TILE AND STORM SEWERS DAMAGED, DISTURBED, OR REMOVED AS A RESULT OF THE CONTRACTOR’S OPERATIONS SHALL BE REPLACED WITH SCHEDULE 40 PIPE OR BETTER, MAINTAINING THE SAME GRADIENT AS EXISTING. REPLACED DRAIN TILE SHALL BE LAID ON COMPACTED BEDDING EQUAL DENSITY TO SURROUNDING STRATUM. REPLACEMENT SHALL BE DONE AT THE TIME OF THE BACKFILL OPERATION. PIPE SIZE IS 3” DIAMETER FOR RESIDENTIAL AND 4” DIAMETER FOR COMMERCIAL, OR AS SPECIFIED.

THE UNIT PRICE FOR THIS ITEM SHALL INCLUDE ALL LABOR, MATERIALS INCLUDING APPROVED CONNECTORS, ADHESIVES, EQUIPMENT AND OTHER INCIDENTALS. WORK IS TO BE PAID PER LINEAR FOOT (LF) OF DRAIN LEADER PIPE REPLACED WHERE NOT INCLUDED UNDER OTHER PAY ITEM(S).

ITEM 611 – INLET NO. 2-A-10, AS PER PLAN

THIS ITEM SHALL CONSIST OF REPLACEMENT OF THE UPPER BOX OF THE EXISTING INLET WITH A STRUCTURE OF THE SAME SIZE PER ODOT STANDARD DRAWING I-2A, AT THE LOCATION SHOWN ON THE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/dsf6erwm)). PRIOR TO ORDERING REPLACEMENT STRUCTURE, CONTRACTOR SHALL CONFIRM SIZE OF THE EXISTING STRUCTURE IN THE FIELD.

IT IS ANTICIPATED THAT THE EXISTING STRUCTURE’S SUMP SECTION AND STORM SEWER PIPES SHALL NOT BE DISTURBED. IF DETERIORATED SUMP WALLS ARE ENCOUNTERED UPON REMOVAL OF THE UPPER BOX, THE CONTRACTOR SHALL REPAIR THE DETERIORATED WALLS AS DIRECTED BY THE ENGINEER. ANY NECESSARY CURB REPLACEMENT TO REPLACE THE UPPER BOX SHALL ALSO BE INCLUDED UNDER THIS ITEM.

PAYMENT SHALL BE MADE AT THE UNIT PRICE BID PER EACH FOR ITEM 611 – INLET NO. 2-A-10, AS PER PLAN AND SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY FOR THE WORK NOTED ABOVE.

ITEM 608 – DETECTABLE WARNING, TYPE A, AS PER PLAN

MATERIALS.
ALL PRODUCTS SHALL RECEIVE PRIOR WRITTEN APPROVAL BY THE ENGINEER. DETECTABLE WARNING SURFACES SHALL BE TEXTURED TO PROVIDE SLIP RESISTANCE AND SHALL CONTRAST VISUALLY WITH ADJACENT WALKING SURFACES – EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT. THE PREFERRED COLOR FOR A LIGHT BACKGROUND (CONCRETE) SHALL BE BRICK RED. THE PREFERRED COLOR FOR A BRICK BACKGROUND SHALL BE BLACK. COLOR SHALL BE INTEGRAL WITH THE DETECTABLE WARNING DEVICE AND SHALL NOT BE SURFACE APPLIED. PAINTS OR OTHER SURFACE COATINGS SHALL NOT BE USED UNLESS INTEGRAL TO THE SYSTEM BEING APPLIED. CAST IRON DETECTABLE WARNING PANELS SHALL BE FACTORY POWDER COATED. DETECTABLE WARNING SURFACES SHALL BE CLASSIFIED BY TYPE OF MATERIAL AND/OR APPLICATION METHOD:

TYPE A:
– CAST IN PLACE PANELS –
SYSTEM SHALL BE TUFTILE CAST IRON CAST IN PLACE ADA TILES 24”X24” PANELS: PART NUMBER: TTCI2424-WS-(COLOR ID) 24”X30” PANELS: PART NUMBER: TTCI2430-WS-(COLOR ID) TILE CONNECTOR: PART NUMBER: TT-CONNECTOR MANUFACTURER: TUFTILE 1200 FLEX CT. LAKE ZURICH, IL 888-960-8897
APPROVED FOR NEW CONSTRUCTION INSTALLATIONS ONLY.

DIMENSIONS.
TRUNCATED DOMES IN A DETECTABLE WARNING SURFACE SHALL HAVE A BASE DIAMETER OF 0.9 INCHES (23 MM) MINIMUM TO 1.4 INCHES (36 MM) MAXIMUM, A TOP DIAMETER OF 50% OF THE BASE DIAMETER MINIMUM TO 65% OF THE BASE DIAMETER MAXIMUM, AND A HEIGHT OF 0.2 INCHES (5 MM).

TRUNCATED DOMES IN A DETECTABLE WARNING SURFACE SHALL HAVE A CENTER-TO-CENTER SPACING OF 2.35 INCHES. DETECTABLE WARNING SURFACES SHALL EXTEND 24 INCHES (610 MM) MINIMUM IN THE DIRECTION OF TRAVEL AND THE FULL WIDTH OF THE CURB RAMP, LANDING, OR BLENDED TRANSITION. THE DETECTABLE WARNING SURFACE SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 3 INCHES (75 MM) MINIMUM AND 8 INCHES (205 MM) MAXIMUM FROM THE CURB LINE.

DOMES SHALL BE ALIGNED ON A SQUARE GRID, ALIGNED IN ROWS PARALLEL AND PERPENDICULAR TO THE PREDOMINANT DIRECTION OF TRAVEL. DOMES MUST BE NOT BE SKEWED DIAGONALLY TO THE DIRECTION OF TRAVEL.

APPLICATION.
DETECTABLE WARNING DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER’S SPECIFICATIONS AND IN ACCORDANCE WITH THIS SPECIFICATION, OR AS OTHERWISE SPECIFIED ON THE PLANS.

THE FINISHED SURFACE SHALL BE UNIFORMLY PROFILED TO MATCH THE ADJOINING SURFACES WITHOUT LIPS, OBSTRUCTIONS AND SHALL DRAIN COMPLETELY.

PANEL SIZES AND PLACEMENT:
4.5’ WIDE RAMPS: 4’ DETECTABLE WARNING PANELS
– TYPE A PANELS: 2 24”X24” PANELS WITH MENDING PLATES
5.5’ WIDE RAMPS: 5’ DETECTABLE WARNING PANELS
– TYPE A PANELS: 2 24”X30” PANELS WITH MENDING PLATES
6.5’ WIDE RAMPS: 6’ DETECTABLE WARNING PANELS
– TYPE A PANELS: 3 24”X24” PANELS WITH MENDING PLATES
8.5’ WIDE RAMPS: 8’ DETECTABLE WARNING PANELS
– TYPE A PANELS: 4 24”X24” PANELS WITH MENDING PLATES

DETECTABLE WARNING PANELS SHALL BE PLACED 3” – 4” BEHIND BACK OF CURB TO ALLOW FOR EDGING AND TOOLING OF FRONT EDGE OF RAMP BETWEEN DETECTABLE WARNING PANEL AND BACK OF CURB.

ITEM 608 – DETECTABLE WARNING, TYPE A, AS PER PLAN (CONT.)

METHOD OF MEASUREMENT.
THE QUANTITY OF DETECTABLE WARNING DEVICES SHALL BE THE ACTUAL SQUARE FOOT (SF) INSTALLED IN PLACE, COMPLETE AND ACCEPTED. THIS ITEM SHALL BE COMPLETE AND INCLUDE ALL WORK NECESSARY TO PROVIDE A COMPLETE AND USEABLE DETECTABLE WARNING DEVICE. THIS SHALL INCLUDE BUT NOT BE LIMITED TO: LAYOUT, SAW-CUTTING, REMOVALS, CONCRETE AND CEMENT BASE MATERIALS, BEDDING, SURFACE PREPARATION, SURFACE SEALANT, AND REPAIR OF ADJOINING AREAS DISTURBED BY THE INSTALLATION OF THE DETECTABLE WARNING SURFACE.

BASIS OF PAYMENT.
UNLESS SPECIFICALLY INCLUDED UNDER OTHER ITEMS ON THE PLANS, PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT UNIT PRICE FOR:

ITEM 608 – DETECTABLE WARNING, TYPE A, AS PER PLAN (SF)

NOTE:
PAYMENT SHALL INCLUDE ALL COSTS OF FURNISHING MATERIAL, EQUIPMENT, AND LABOR NECESSARY FOR THE INSTALLATION OF DETECTABLE WARNING DEVICES, AND SHALL BE IN ADDITION TO ANY OVERLAPPING PAYMENTS FOR SQUARE FOOT OF SIDEWALK / CURB RAMP.

ITEM 608 – BRICK PAVER REMOVE & RESET, (TYPE), AS PER PLAN

THIS ITEM CONSISTS OF THE REMOVAL AND RESETTING OF EXISTING BRICK OR CONCRETE MODULAR PAVER WALKS AND DRIVES. THE PAVERS SHALL BE REMOVED IN A MANNER TO PREVENT DAMAGE TO PAVERS SO THEY MAY BE RE-INSTALLED. THE REMOVED PAVERS SHALL BE NEATLY STACKED AT A LOCATION DESIGNATED BY THE ENGINEER. THE PAVERS SHALL BE INSTALLED AS FOLLOWS DEPENDING ON THE TYPE OF PAVER SYSTEM INSTALLED:

TYPE 1: MODULAR PAVERS ON SAND BED, OVER COMPACTED AGGREGATE.
THE BRICK PAVERS SHALL BE PLACED ON A 1-INCH BASE OF COMPACTED AND LEVELED SAND OVER A 4-INCH SUBGRADE OF COMPACTED 304. STEEL EDGING SHALL BE INSTALLED ALONG THE EDGES OF THE REPLACED PAVERS.

TYPE 2: MODULAR PAVERS ON BITUMINOUS SETTING BED ON CONCRETE BASE.
PROVIDE/INSTALL MODULAR PAVERS ON BITUMINOUS BED. PLACEMENT OF CONCRETE BASE SHALL BE PAID UNDER ITEM 608, CONCRETE WALK

TYPE 3: MODULAR PAVERS ON SAND BED, OVER CONCRETE BASE.
THE BRICK PAVERS SHALL BE PLACED ON A 1-INCH BASE OF COMPACTED AND LEVELED SAND OVER A POURED CONCRETE BASE. INSTALLATION OF THE CONCRETE BASE SHALL BE PAID UNDER ITEM 608, CONCRETE WALK. STEEL EDGING SHALL BE INSTALLED ALONG THE EDGES OF THE REPLACED PAVERS.

PAVERS SHALL BE INSTALLED SO THE WALKING/DRIVING SURFACE IS SMOOTH AND INLINE WITH THE ADJACENT WALK/DRIVE SECTIONS. INDIVIDUAL PAVERS SHALL BE INSTALLED AS CLOSE AS POSSIBLE TO EACH OTHER IN PLANE, WITHOUT UNNECESSARY DIFFERENTIAL BETWEEN ADJACENT PAVERS.

BRICK PAVERS REMOVED AND RESET SHALL BE PAID UNDER THIS ITEM; BRICK PAVERS REMOVED AND NOT RESET SHALL BE PAID UNDER ITEM 202.

PAYMENT FOR 608 BRICK PAVER REMOVE & RESET, (TYPE), AS PER PLAN SHALL BE PAID BY THE SQUARE FOOT AND INCLUDE MATERIAL, LABOR, EDGING, SAND, 304 AGGREGATE, POLYMERIC JOINT SAND, EXCAVATION, ITEM 204 COMPACTION OF SUBGRADE, ROOT REMOVAL AND PRUNING, AND OTHER INCIDENTALS NECESSARY FOR REMOVAL AND REPLACEMENT OF PAVER WALKS AND DRIVES.

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

25-002-CIP

SHEET

P.6

TOTAL

60

ITEM 202 – CURB / CURB AND GUTTER REMOVED AND DISPOSED OF. AS PER PLAN

THIS ITEM CONSISTS OF REMOVAL AND DISPOSAL OF EXISTING CURB OR COMBINATION CURB AND GUTTER IN ACCORDANCE WITH ITEM 202 OF THE CMSC AS AMENDED IN VARIOUS LOCATIONS THROUGHOUT THE CITY, AS DIRECTED BY THE ENGINEER.

THE EXISTING CURB AND GUTTER SHALL BE REMOVED IN SUCH A MANNER AS TO CAUSE NO DAMAGE TO THE ADJACENT PAVEMENT, DRIVEWAY APPROACH, ADJOINING SIDEWALK, AND/OR CURB AND GUTTER AND MINIMIZE DAMAGE TO THE TREE LAWN STRIP BETWEEN THE CURB AND SIDEWALK. THE CONTRACTOR SHALL SAW CUT AROUND CURB AND GUTTER SECTIONS PRIOR TO REMOVAL. ALL ADJOINING PAVEMENT SHALL BE SAW CUT PRIOR TO REMOVAL OF CURB/ CURB AND GUTTER. IN THE EVENT THAT DAMAGE SHOULD OCCUR TO THE PAVEMENT, EXISTING CURB AND GUTTER OR THE GRASS STRIP, REPAIRS SHALL BE COMPLETED IN A MANNER APPROVED BY THE ENGINEER AS SOON AS POSSIBLE AT NO COST TO THE CITY.

CURB AND GUTTER THAT HAS BEEN REMOVED, AND IS TO BE REPLACED, SHALL BE REPLACED WITHIN 7 CALENDAR DAYS.

THE BID PRICE FOR THIS ITEM INCLUDES ALL COSTS FOR TRAFFIC CONTROL DEVICES, REMOVAL AND DISPOSAL. NO PAYMENT WILL BE MADE FOR ANY SECTION OF THE WORK UNTIL ALL ITEMS ASSOCIATED WITH THE SECTION ARE COMPLETE. WORK IS TO BE PAID PER LINEAR FOOT (FT) OF CURB / CURB AND GUTTER REMOVED AND DISPOSED OF.

ITEM 202 – CONCRETE REMOVED AND DISPOSED OF. AS PER PLAN

THIS ITEM CONSISTS OF THE REMOVAL AND DISPOSAL OF EXISTING CONCRETE OR BRICK PAVEMENT, CURB RAMPS, DRIVE APRONS (4” – 8” THICK) OR OTHER WORK AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE TO SAW CUT AND PROTECT ADJACENT WALK/PAVEMENT SECTIONS INTENDED TO REMAIN. IN THE EVENT THAT ADJACENT WALK/PAVEMENT SECTIONS BECOME DAMAGED, THE DAMAGED SECTIONS WILL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE CITY. THIS ITEM SHALL ALSO INCLUDE NECESSARY EXCAVATION AND HAULING OF SPOILS NECESSARY TO MEET REQUIRED GRADE ELEVATIONS FOR THE INSTALLATION OF NEW ADA COMPLIANT CURB RAMPS AND ADJACENT WALKS, CURBING, AND RETAINING WALLS.

TREE ROOTS ENCOUNTERED DURING EXCAVATION SHALL BE REMOVED IN LOCATIONS WHERE THEY INTERFERE WITH THE PLACEMENT OF THE WALK, RAMP OR APRON. ROOTS SHALL BE NEATLY SAWED (FREE OF SPLINTERS, SPLITS, AND BURNS) AND REMOVED TO A DEPTH OF AT LEAST FOUR (4) INCHES BELOW THE BOTTOM OF THE CONCRETE. IF THE CONTRACTOR ENCOUNTERS ROOTS LARGER THAN 2” IN DIAMETER, THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR DIRECTION ON HOW TO PROCEED.

THE BID PRICE FOR THIS ITEM INCLUDES ALL COSTS FOR TRAFFIC CONTROL DEVICES, REMOVAL AND DISPOSAL, NO PAYMENT WILL BE MADE FOR ANY SECTION OF THE WORK UNTIL ALL ITEMS ASSOCIATED WITH THE SECTION ARE COMPLETE. NO PAYMENT WILL BE MADE UNDER THIS SECTION FOR WORK DESIGNATED TO BE PERFORMED UNDER ITEM 203 EXCAVATION. WORK IS TO BE PAID PER SQUARE FOOT (SF) OF CONCRETE / PAVEMENT REMOVED AND DISPOSED OF.

ITEM 609 – COMBINATION CURB AND GUTTER. 6” THICK. AS PER PLAN
ITEM 609 – COMBINATION CURB AND GUTTER. 8” THICK. AS PER PLAN
ITEM 609 – STRAIGHT 18” CURB. AS PER PLAN
ITEM SPECIAL – CURB CAN HOLES

THIS ITEM CONSISTS OF INSTALLATION OF NEW CURB OR COMBINATION CURB AND GUTTER IN ACCORDANCE WITH ITEM 609 OF THE CMSC AS AMENDED IN VARIOUS LOCATIONS THROUGHOUT THE CITY AS DIRECTED BY THE ENGINEER. ALL CURBING SHALL BE PLACED ON GRADED AND COMPACTED AGGREGATE BASE. ALL DEBRIS SHALL BE REMOVED FROM EXCAVATED AREA(S) PRIOR TO GRADING AND COMPACTING OF AGGREGATE BASE. ALL CURBING SHALL BE INSTALLED USING FRONT AND BACK FORMS UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

ITEM 609 – COMBINATION CURB AND GUTTER, 6” THICK, AS PER PLAN SHALL BE ACCORDING TO CITY OF DUBLIN STANDARD DRAWING RD-02. ITEM 609 – COMBINATION CURB AND GUTTER, 8” THICK, AS PER PLAN SHALL BE ACCORDING TO CITY OF DUBLIN STANDARD DRAWING RD-02 EXCEPT THAT THE GUTTER PAN THICKNESS SHALL BE 8” AND SHALL BE INSTALLED ON ARTERIAL STREETS.

THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING LINE AND GRADE.

THE CONTRACTOR SHALL IDENTIFY, MARK AND RESTORE ALL DRAIN OPENINGS IN THE CURB. DRAIN OPENINGS IN THE CURB ARE TO BE CORED, AND NOT FORMED IN PLACE. PAYMENT FOR CORING THE DRAIN OPENINGS WILL BE UNDER "ITEM SPECIAL- CURB CAN HOLES". INCIDENTAL TO THE CURBING, DRAIN LEADERS SHALL BE REPLACED UP TO TWO (2) FEET BEHIND THE CURB AND RECONNECTED IN A MANNER APPROVED BY THE ENGINEER AS SOON AS POSSIBLE AFTER CURB FORMS ARE REMOVED. PAYMENT WILL BE MADE UNDER "ITEM SPECIAL DRAIN LEADER PIPE" FOR ADDITIONAL PIPE INSTALLED BEYOND THE TWO FEET LIMIT, WHERE DIRECTED BY THE ENGINEER.

THE EXCAVATION BEHIND THE CURB SHALL BE BACKFILLED WITH COMPACTED TOPSOIL TO A POINT ONE (1) INCH ABOVE THE CURB AS SOON AS POSSIBLE AFTER CURB FORMS AND OTHER DEBRIS ARE REMOVED. THE TOP ONE (1) INCH OF TOPSOIL SHALL BE PREPARED FOR SEED APPLICATION. THE EXISTING GROUND SHALL BE ROLLED OR GRADED TO PROVIDE A CONSISTENT GRADE BEHIND THE CURB FREE OF HIGH AND LOW AREAS DUE TO EXCAVATION, CURB REPLACEMENT AND RELATED WORK. DISTURBED AREAS SHALL BE SEEDED AND MULCHED IN ACCORDANCE WITH ITEM 659.

THE OVERDIG IN FRONT OF THE CURB, AFTER BEING CLEANED OF LOOSE DEBRIS AND EXCESS CONCRETE, SHALL BE FILLED WITH LSM, TYPE 2, AND LEVELED UNIFORMLY TO 1-½” BELOW FACE OF GUTTER OR ADJOINING PAVEMENT. CLASS C CONCRETE MAY ALSO BE USED PROVIDED THE CONCRETE IS PROPERLY CONSOLIDATED TO AVOID VOIDS AND LEVELED UNIFORMLY. ON STREETS WHERE THE REMAINING PAVEMENT SECTION IS BEING REMOVED AND REPLACED COMPACTED GRANULAR MATERIAL MAY BE USED FOR TEMPORARY FILL OF THE OVERDIG AREA(S). THE OVERDIG SHALL BE FILLED WITHIN 24 HOURS OF REMOVAL OF FRONT FORMS OR PLACEMENT OF CURB USING CURB MACHINE.

DRIVEWAY DROPPED CURBS SHALL BE CONSTRUCTED UTILIZING CLASS MS CONCRETE CONFORMING WITH SECTION 499 OF THE CMSC. IF THE DROPPED CURB AREAS ARE PLACED IN CONJUNCTION WITH ADJOINING CURBING UTILIZING A SLIP FORM MACHINE, THE CONTRACTOR WILL NOT BE REQUIRED TO UTILIZE MS CONCRETE, HOWEVER THE CONTRACTOR WILL STILL BE REQUIRED TO OPEN THE DRIVEWAYS TO TRAFFIC WITHIN THE SPECIFIED TIMEFRAME. FULL DEPTH EXPANSION MATERIAL ONE HALF (1/2) INCHES THICK CONFORMING TO ASTM D1752, TYPE 1 SHALL BE UTILIZED WHERE THE NEW WORK MEETS EXISTING CONCRETE. INCREASED 10” GUTTER PAN THICKNESS REQUIRED FOR COMMERCIAL DRIVE ENTRANCES SHALL BE INCIDENTAL TO THE CURB AND GUTTER PAY ITEMS.

CURB REPLACEMENT IS TO BE COMPLETED PRIOR TO THE START OF ASPHALT SURFACE OVERLAYS ON ANY STREET INCLUDED IN THIS CONTRACT.

THE BID PRICE FOR THIS ITEM INCLUDES ALL COSTS FOR TOPSOIL AND SEEDING UP TO 2’ BEHIND BACK OF CURB, TRAFFIC CONTROL DEVICES, REMOVAL AND DISPOSAL, SETTING OF GRADE STAKES, CURB OR CURB AND GUTTER INSTALLATION, DRAIN LEADER CONNECTION, SAW CUTTING, AND FINAL RESTORATION. NO PAYMENT WILL BE MADE FOR ANY SECTION OF THE WORK UNTIL ALL ITEMS ASSOCIATED WITH THE SECTION ARE COMPLETE. WORK IS TO BE PAID PER LINEAR FOOT (FT) OF CURB OR CURB AND GUTTER INSTALLED AND ACCEPTED.

ITEM 608 – 4” CONCRETE WALK WITH AGGREGATE BASE. AS PER PLAN
ITEM 608 – 6” CONCRETE WALK WITH AGGREGATE BASE. AS PER PLAN

THIS ITEM CONSISTS OF THE PLACEMENT OF NEW FOUR (4) AND SIX (6) INCH THICK CONCRETE SIDEWALKS AND DRIVEWAY APRONS AND OTHER INCIDENTAL FLATWORK OVER 4” COMPACTED AGGREGATE BASE. THE AGGREGATE BASE SHALL BE PLACED UPON PROPERLY PREPARED AND COMPACTED SUBGRADE. CONCRETE SHALL BE PLACED AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER AND CONSTRUCTED IN ACCORDANCE WITH ITEM 608 OF THE CMSC AND THE STANDARD DRAWINGS OF THE CITY OF DUBLIN.

FULL DEPTH EXPANSION MATERIAL ONE HALF (1/2) INCHES THICK CONFORMING TO ASTM D 1752, TYPE 1 (RUBBER EXPANSION MATERIAL) SHALL BE UTILIZED WHERE THE WORK MEETS EXISTING WALKS, CURBS OR STRUCTURES. FIBROUS TYPE EXPANSION MATERIAL IS NOT TO BE USED. THE CONTRACTOR IS RESPONSIBLE TO SAW CUT AND PROTECT ADJACENT WALK SECTIONS INTENDED TO REMAIN. IN THE EVENT THAT ADJACENT WALK SECTIONS BECOME DAMAGED, THEY WILL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE CITY.

TREE ROOTS ENCOUNTERED DURING EXCAVATION SHALL BE REMOVED IN LOCATIONS WHERE THEY INTERFERE WITH THE PLACEMENT OF THE WALK, RAMP OR APRON. ROOTS SHALL BE NEATLY SAWED (FREE OF SPLINTERS, SPLITS, AND BURNS) AND REMOVED TO A DEPTH OF AT LEAST FOUR (4) INCHES BELOW THE BOTTOM OF THE CONCRETE. IF THE CONTRACTOR ENCOUNTERS ROOTS LARGER THAN 2” IN DIAMETER, THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR DIRECTION ON HOW TO PROCEED.

THE CONTRACTOR SHALL KEEP THE AREAS DISTURBED ADJACENT TO THE CONCRETE WALKS TO A MINIMUM.

THIS ITEM INCLUDES ALL MATERIAL AND LABOR AND OTHER INCIDENTALS INCLUDING, BUT NOT LIMITED TO, EXCAVATION AND HAULING OF SPOILS, SETTING GRADES, FORMWORK, FINISHING, AND RESTORATION AS DETAILED UNDER ITEM 653 AND ITEM 659. WORK IS TO BE PAID PER SQUARE FOOT (SF) OF:

- FOUR (4) INCH CONCRETE PLACED, FINISHED AND ACCEPTED.
- SIX (6) INCH CONCRETE PLACED, FINISHED AND ACCEPTED.

FOR WALK/FLATWORK THAT WAS PLACED AT 8” THICK AT THE DIRECTION OF THE ENGINEER, THE CONTRACTOR WILL BE PAID THE CONTRACT UNIT COST FOR 608 6” CONCRETE WALK W/ AGG. BASE, WITH THE SQUARE FOOT MEASUREMENT OF THE 8” THICK WALK ADJUSTED BY TWENTY PERCENT, (SF*1.2).

ITEM SPECIAL – CHEEK WALL

THIS ITEM CONSISTS OF CONSTRUCTING CONCRETE CHEEK WALL PER STD. DWG. PD-10 AND CMSC 609. CHEEK WALL FUNCTIONS AS A BACK OF SIDEWALK CURB AT CURB RAMPS WHERE SPACE RESTRICTIONS DO NOT ALLOW FOR GRADING BEHIND THE WALK. CHEEK WALL SHALL ONLY BE CONSTRUCTED AT LOCATIONS AS DIRECTED BY THE ENGINEER.

PAYMENT SHALL BE MADE AT THE UNIT PRICE BID PER LINEAR FOOT FOR ITEM SPECIAL – CHEEK WALL AND SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PERFORM THE WORK NOTED ABOVE.

LAWN RESTORATION

THE COST FOR ALL GRADING AND RESTORATION OF DISTURBED AREAS RESULTING FROM CURB, OR CURB AND GUTTER REPLACEMENT SHALL BE INCLUDED IN EACH RESPECTIVE PAY ITEM. TOPSOIL PLACED SHALL BE PAID UNDER ITEM 653. BACKFILLING, SEEDING AND MULCHING SHALL BE COMPLETED NO LATER THAN 10 DAYS AFTER FORMS ARE REMOVED. ALL RESTORATION SHALL BE COMPLETE WITHIN 10 DAYS OF RELATED WORK. WHERE SIDEWALKS ARE LOWERED, GRASS BETWEEN THE BACK OF SIDEWALK AND THE RIGHT-OF-WAY LINE SHALL BE EVENLY GRADED TO THE LEAST POSSIBLE SLOPE. ALL DISTURBED AREAS SHALL BE RESTORED, SEEDED AND MULCHED AND MAINTAINED IN ACCORDANCE WITH ITEMS 653 AND 659. THE SAME SHALL APPLY FOR THE LAWN BEHIND PROPOSED CURB WORK.

ITEM 617 – RECONDITION SHOULDERS. AS PER PLAN

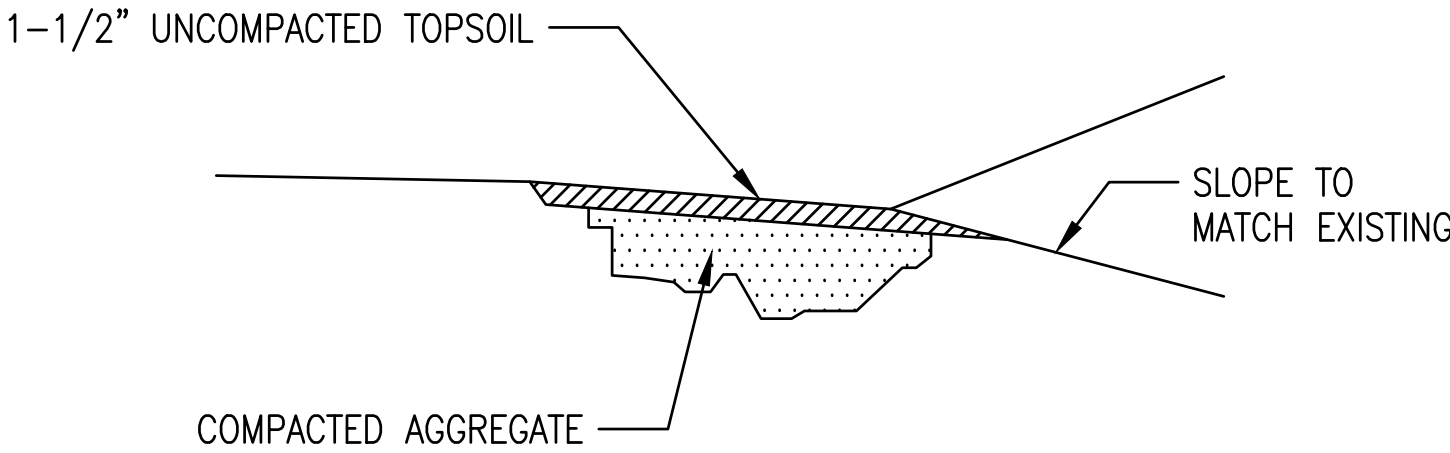
THIS ITEM CONSISTS OF RECONDITIONING SHOULDERS IN ACCORDANCE WITH ODOT CMS ITEM 617 RECONDITIONING SHOULDERS INCLUDING THE FOLLOWING PROVISIONS:

THE SHOULDERS SHALL BE GRADED AND PREPARED PRIOR TO PLACING COMPACTED AGGREGATE MATERIAL. ALL UNSUITABLE MATERIAL SHALL BE REMOVED AND DISPOSED OF. THE SHOULDERS SHALL BE GRADED AND COMPACTED AS DIRECTED OR SPECIFIED IN THE PLANS.

THE BID PRICE FOR THIS ITEM SHALL INCLUDE ALL MATERIAL, LABOR AND INCIDENTALS NECESSARY TO COMPLETE THE RECONDITIONING OF SHOULDERS USING COMPACTED AGGREGATE CONFORMING TO 703.18.

COMPACTED AGGREGATE SHALL BE COVERED WITH 1.5” OF ITEM 653 PULVERIZED TOPSOIL TO PROVIDE FOR SEED BED. SEEDING AND MULCHING SHALL FOLLOW AS OUTLINED UNDER ITEM 659 SEEDING AND MULCHING, AS PER PLAN. WATER AND USE OF A PNEUMATIC ROLLER FOR COMPACTION OF AGGREGATE SHALL BE CONSIDERED INCIDENTAL TO ITEM 617 RECONDITION SHOULDERS, COMPACTED AGGREGATE, AS PER PLAN

WORK IS TO BE PAID BY:
FOR ADDITIONAL MATERIAL PLACED:
ITEM 617 RECONDITION SHOULDERS, 4” COMPACTED AGGREGATE, AS PER PLAN (CY)
ITEM 617 RECONDITION SHOULDERS, 1.5” ITEM 653 PULVERIZED TOPSOIL, AS PER PLAN, (CY)
ITEM 659 SEEDING AND MULCHING, AS PER PLAN



ITEM SPECIAL – ASPHALT REJUVENATING AGENT

RECLAMITE OR APPROVED EQUAL

SCOPE

THIS WORK SHALL CONSIST OF FURNISHING ALL LABOR, EQUIPMENT, MATERIAL TO PERFORM ALL OPERATIONS NECESSARY FOR THE REJUVENATION AND IN-DEPTH SEALING OF ASPHALTIC-CONCRETE SURFACE COURSE BY SPRAY APPLICATION OF PETROLEUM OIL AND RESINS EMULSIFIED WITH WATER, COMPLETE, IN ACCORDANCE WITH THE SPECIFICATIONS, THE APPLICABLE DRAWINGS AND SUBJECT TO TERMS AND CONDITIONS OF THE CONTRACT.

1. SUBMITTAL MATERIALS

THE ASPHALT REJUVENATING AGENT SHALL BE COMPOSED OF A PETROLEUM RESIN OIL BASE UNIFORMLY EMULSIFIED WITH WATER. THE CONTRACTOR SHALL SUBMIT A CERTIFIED STATEMENT FROM ASPHALT REJUVENATING MANUFACTURER SHOWING THAT THE ASPHALT REJUVENATING EMULSION CONFORMS TO THE FOLLOWING PHYSICAL AND CHEMICAL REQUIREMENTS.

2. SPECIFICATION

DESIGNATION	TEST METHOD	REQUIREMENTS
VISCOSITY, S.F. AT 77 F, SEC	ASTM D244-60	15-40
RESIDUE, % MIN. (A)	ASTM D244-60 (MOD.)	60-65
MISCIBILITY TEST (B)	ASTM D244-60 (MOD.)	NO COAGULATION
SIEVE TEST, % MAX.	ASTM D244-60 (MOD.)	0.10
PARTICLE CHARGE TEST	ASTM D244-60	POSITIVE
TESTS ON RESIDUE	ASTM D244-60 (MOD).	
VISCOSITY CS, 140 F	ASTM-D445	100-200
ASPHALTENES, % MAX.	ASTM-D2006-65-T	0.75
MALTENES DIST. RATIO	ASTM-D2006-65-T	0.3-0.5

$$\frac{PC + A1}{S + A2} \text{ (D)}$$

ASTM D244 MODIFIED EVAPORATION TEST FOR PERCENT OF RESIDUE IS MADE BY HEATING 50 GRAM SAMPLE TO 3,000°F UNTIL FOAMING CEASES, THEN COOL IMMEDIATELY AND CALCULATE RESULTS.

TEST PROCEDURE IDENTICAL WITH ASTM D244-60 EXCEPT THAT 0.02 NORMAL CALCIUM CHLORIDE SOLUTION SHALL BE USED IN PLACE OF DISTILLED WATER.

TEST PROCEDURE IDENTICAL WITH ASTM D244 EXCEPT THAT DISTILLED WATER SHALL BE USED IN PLACE OF 2% SODIUM OLEATE SOLUTION.

IN THE MALTENES DISTRIBUTION RATIO TEST BY ASTM METHOD D2006-65-T;

PC – POLAR COMPOUNDS A1 – FIRST ACIDAFFINS
A2 – SECOND ACIDAFFINS S – SATURATES

THE MATERIAL SHALL HAVE A RECORD OF AT LEAST FIVE YEARS OF SATISFACTORY SERVICE AS AN ASPHALT REJUVENATING AGENT AND IN-DEPTH SEALER; SUCH SATISFACTORY SERVICE BEING BASED ON THE CAPABILITY OF THE MATERIAL TO INCREASE THE DUCTILITY AND PENETRATION VALUE OF THE ASPHALT BINDER IN THE PAVEMENT SURFACE AND TO SEAL THE PAVEMENT IN-DEPTH TO THE INTRUSION OF AIR AND WATER.

THE CONTRACTOR SHALL FURNISH THE MANUFACTURERS CERTIFICATION THAT THE MATERIAL PROPOSED FOR USE IS IN COMPLIANCE WITH THE SPECIFICATION REQUIREMENTS AND INCLUDE COPIES OF SUPPORTING TESTS AND PREVIOUS USE DOCUMENTATION. THE PRODUCT RECLAMITE, OR APPROVED EQUAL, IS ACCEPTABLE.

3. APPLICATION TEMPERATURE

THE TEMPERATURE OF THE EMULSION AT THE TIME OF APPLICATION SHALL BE THAT AS RECOMMENDED BY THE MANUFACTURER.

ITEM SPECIAL – ASPHALT REJUVENATING AGENT (CONT.)

4. HANDLING OF ASPHALT REJUVENATING AGENT

CONTENTS IN TANK CARS OR STORAGE TANKS SHALL BE CIRCULATED AT LEAST TEN MINUTES BEFORE WITHDRAWING ANY MATERIAL FOR APPLICATION. WHEN LOADING THE DISTRIBUTOR, THE ASPHALT REJUVENATING AGENT CONCENTRATE SHALL BE LOADED FIRST AND THEN THE REQUIRED AMOUNT OF WATER SHALL BE ADDED. THE WATER SHALL BE INTRODUCED INTO THE DISTRIBUTOR WITH ENOUGH FORCE TO CAUSE AGITATION AND THOROUGH MIXING OF THE TWO MATERIALS. TO PREVENT FOAMING, THE DISCHARGE END OF THE WATER HOSE OR PIPE SHALL BE KEPT BELOW THE SURFACE OF THE MATERIAL IN THE DISTRIBUTOR TANK.

5. EQUIPMENT

ALL TOOLS, MACHINES AND EQUIPMENT USED IN THE PERFORMANCE OF THE WORK SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER AND SHALL BE MAINTAINED IN A SAFE AND SATISFACTORY WORKING CONDITION AT ALL TIMES.

6. CLEAN EQUIPMENT

EQUIPMENT USED FOR THE APPLICATION OF THE PETROLEUM OIL AND RESIN EMULSIONS SHALL BE CLEAN OF ALL FOREIGN AND OBJECTIONABLE MATERIAL. AN ASPHALT DISTRIBUTOR WHICH SHALL BE USED AS A SPREADER WILL BE CLEANED OF ALL OF ITS ASPHALT MATERIALS AND WASHED OUT TO THE EXTENT THAT NO DISCOLORATION OF THE EMULSION MAY BE PERCEPTIBLE. CLEANLINESS OF THE SPREADING EQUIPMENT SHALL BE SUBJECT TO THE APPROVAL AND SATISFACTION OF THE ENGINEER.

THE DISTRIBUTOR FOR SPREADING THE EMULSION SHALL BE SELF-PROPELLED AND SHALL HAVE PNEUMATIC TIRES. THE DISTRIBUTOR SHALL BE DESIGNED AND EQUIPPED TO DISTRIBUTE THE EMULSION UNIFORMLY ON VARIABLE WIDTHS OF SURFACE AT READILY DETERMINED AND CONTROLLED RATES FROM 0.05 TO 0.5 GALLONS PER SQUARE YARD OF SURFACE AND WITH AN ALLOWABLE VARIATION FROM ANY SPECIFIED RATE NOT TO EXCEED FIVE PERCENT. DISTRIBUTOR EQUIPMENT SHALL INCLUDE FULL CIRCULATION SPRAY BARS, PUMP, ACHOMETER, VOLUME MEASURING DEVICE AND A HARD HOSE ATTACHMENT SUITABLE FOR APPLICATION OF THE EMULSION MANUALLY TO COVER AREAS OR PATCHES INACCESSIBLE TO THE DISTRIBUTOR. THE DISTRIBUTOR SHALL BE EQUIPPED TO CIRCULATE AND AGITATE THE EMULSION WITHIN THE TANK.

A CHECK OF DISTRIBUTOR RATE AND UNIFORMITY OF DISTRIBUTION SHALL BE MADE WHEN DIRECTED BY THE ENGINEER.

7. WEATHER LIMITATIONS

THE EMULSION SHALL BE APPLIED ONLY WHEN THE EXISTING SURFACE TO BE TREATED IS THOROUGHLY DRY AND WHEN THE WEATHER IS CLEAR AND IS NOT THREATENING RAIN. THE EMULSION SHALL NOT BE APPLIED WHEN THE ATMOSPHERIC TEMPERATURE IS BELOW 40° F.

8. APPLICATION

FOLLOWING PRE-CLEANING OF THE ASPHALTIC SURFACE, THE ASPHALT REJUVENATING AGENT SHALL BE APPLIED BY DISTRIBUTOR AT THE TEMPERATURE RECOMMENDED BY THE MANUFACTURER AND AT THE PRESSURE REQUIRED FOR THE PROPER DISTRIBUTION. THE EMULSION SHALL BE SO APPLIED THAT UNIFORM DISTRIBUTION IS OBTAINED AT ALL POINTS OF THE AREAS TO BE TREATED. DISTRIBUTION SHALL BE COMMENCED WITH A RUNNING START TO INSURE FULL RATE OF SPREAD OVER THE ENTIRE AREA TO BE TREATED. AREAS INADVERTENTLY MISSED SHALL RECEIVE ADDITIONAL TREATMENT AS MAY BE REQUIRED BY HAND SPRAYER APPLICATION.

APPLICATION OF ASPHALT REJUVENATING AGENT SHALL BE ON ONE-HALF WIDTH OF THE PAVEMENT AT A TIME.

ITEM SPECIAL – ASPHALT REJUVENATING AGENT (CONT.)

WHEN APPLICATION IS MADE UNDER TRAFFIC AND/OR A PREVIOUSLY CLOSED ROAD MUST BE OPENED TO TRAFFIC AND SLOW PENETRATION OCCURS AND IN THE OPINION OF THE CITY ENGINEER, A HAZARD EXISTS TO THE TRAVELING PUBLIC, A LIGHT COATING OF DRY, GRITTY SAND SHALL BE APPLIED TO THE SURFACE IN SUFFICIENT AMOUNTS TO PROTECT THE TRAVELING PUBLIC. SAND SHALL BE APPLIED AT THE RATE OF ONE POUND PER SQUARE YARD.

BEFORE SPREADING, THE ASPHALT REJUVENATING AGENT SHALL BE BLENDED WITH WATER AT THE RATE OF TWO (2) PARTS OF REJUVENATING AGENT TO ONE (1) PART WATER, BY VOLUME OR AS SPECIFIED BY THE MANUFACTURER. THE COMBINED MIXTURE OF ASPHALT REJUVENATING AGENT AND WATER SHALL BE SPREAD AT THE RATE OF 0.05 TO 0.08 GALLONS PER SQUARE YARD, OR AS APPROVED BY THE ENGINEER.

EXCESS SAND, POST-TREATMENT AND STREET CLEANING SHALL OCCUR WITHIN TWO DAYS FOLLOWING APPLICATION.

WHEN MORE THAN ONE APPLICATION IS TO BE MADE, SUCCEEDING APPLICATIONS SHALL BE MADE AS SOON AS PENETRATION OF THE PRECEDING APPLICATION HAS BEEN COMPLETED AND APPROVAL IS GRANTED FOR ADDITIONAL APPLICATIONS BY THE ENGINEER.

GRADES OR SUPER ELEVATIONS OF SURFACES THAT MAY CAUSE EXCESSIVE RUNOFF, IN THE OPINION OF THE ENGINEER, SHALL HAVE THE REQUIRED AMOUNTS APPLIED IN TWO OR MORE APPLICATIONS AS DIRECTED.

THE REJUVENATING AGENT SHALL BE APPLIED BY AN EXPERIENCED APPLICATOR OF SUCH MATERIAL. THE APPLICATOR SHALL HAVE A MINIMUM OF THREE YEARS EXPERIENCE IN APPLYING THE PRODUCT PROPOSED FOR USE. THEY SHALL SUBMIT A LIST OF THE LAST FIVE PROJECTS ON WHICH THEY APPLIED SAID REJUVENATOR.

THE REJUVENATING AGENT SHALL BE APPLIED WITHIN 7 DAYS AFTER SURFACE COURSE IS PLACED.

APPLICATION OF THE ASPHALT REJUVENATING AGENT SHALL BE PRIOR TO ANY THERMOPLASTIC STRIPING, WITH A TWO WEEK WAITING PERIOD.

9. TRAFFIC

THE CONTRACTOR SHALL SCHEDULE THEIR OPERATIONS AND CARRY OUT THE WORK IN A MANNER TO CAUSE THE LEAST DISTURBANCE AND/OR INTERFERENCE WITH THE NORMAL FLOW OF TRAFFIC OVER THE AREAS TO BE TREATED. TREATED PORTIONS OF THE BITUMINOUS SURFACES SHALL BE KEPT CLOSED AND FREE FROM TRAFFIC UNTIL PENETRATION HAS BECOME COMPLETE AND THE AREA IN THE OPINION OF THE ENGINEER, IS SUITABLE FOR TRAFFIC. SUITABLE METHODS SUCH AS PENNANTS, BARRICADES, FLAGMEN, PILOT CARS, ETC., SHALL BE USED TO PROTECT THE UNCURED SURFACE FROM ALL TYPES OF TRAFFIC. ANY DAMAGE TO THE UNCURED SURFACE WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE ENGINEER SHALL GIVE FINAL APPROVAL AS TO THE METHOD USED. IF DAMAGE OCCURS WHERE SUITABLE MEANS HAVE BEEN MADE TO PROTECT THE UNCURED SURFACE, VIOLATORS WILL BE PROSECUTED AND THE CONTRACTOR WILL BE REIMBURSED FOR THE AMOUNT OF DAMAGES.

RESIDENTS AFFECTED BY THE WORK SHALL BE NOTIFIED BY THE CONTRACTOR IN WRITING, THE DAY BEFORE THE WORK IS TO BE DONE.

10. METHOD OF MEASUREMENT

CONSTRUCTION SEALING WITH REJUVENATING AGENT WILL BE MEASURED BY THE SQUARE YARD AS PROVIDED FOR IN THE CONTRACT DOCUMENTS.

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

25-002-CIP

SHEET

TOTAL

P.8

60

ITEM SPECIAL – PAVERS, REMOVED AND RESET

THIS ITEM CONSISTS OF THE REMOVAL AND RESETTING OF THE EXISTING BRICK PAVER CROSSWALKS SHOWN ON THE PLANS, DOWN TO BUT NOT INCLUDING THE EXISTING CONCRETE BASE. THE PAVERS SHALL BE REMOVED IN A MANNER TO PREVENT DAMAGE TO PAVERS SO THEY MAY BE RE-INSTALLED. THE REMOVED PAVERS SHALL BE NEATLY STACKED AT A LOCATION DESIGNATED BY THE ENGINEER. THE CONTRACTOR SHALL NOT DISTURB THE EXISTING GRANITE BANDING.

PAVERS: THE CITY WILL ALLOW THE REUSE OF EXISTING BRICK PAVERS THAT ARE REMOVED AS PART OF THIS PROJECT, PROVIDED THAT THE PAVERS ARE NOT CRACKED OR DAMAGED BEYOND NORMAL WEAR. REUSE OF EXISTING PAVERS MUST BE APPROVED BY THE CITY PRIOR TO RESET. IF EXISTING PAVERS ARE REUSED, THE PAVERS MUST BE CLEAN OF DIRT AND DEBRIS PRIOR TO INSTALLATION ON BITUMINOUS SETTING BED. ANY NEW PAVERS NECESSARY SHALL BE FURNISHED PER ITEM SPECIAL – BRICK PAVERS. REUSED PAVERS SHALL BE MIXED WITH NEW PAVERS TO PRODUCE A UNIFORM AESTHETIC.

ALL PAVERS, NEW OR REUSED, SHALL BE INSTALLED PER THE BRICK PAVER DETAIL SHOWN ON SHEET 33. PAVERS SHALL BE INSTALLED TO MATCH THE EXISTING LAYOUT PATTERN. PAVERS SHALL BE INSTALLED SO THE WALKING SURFACE IS SMOOTH AND INLINE WITH THE ADJACENT ROADWAY SECTIONS. INDIVIDUAL PAVERS SHALL BE INSTALLED AS CLOSE AS POSSIBLE TO EACH OTHER IN PLANE, WITHOUT UNNECESSARY DIFFERENTIAL BETWEEN ADJACENT PAVERS.

ALL BRICK SHALL BE FIELD CUT AS REQUIRED FOR PROPER INSTALLATION ADJACENT TO ALL CURBS, AROUND UTILITIES, DRAIN INLETS, MANHOLES, MANHOLE COVERS, OBSTACLES, ETC.

IN ORDER TO MATCH THE EXISTING PAVER LAYOUT PATTERN, ALL BRICK PAVERS SHALL BE SECURED IN PLACE BY A BRICK SOLDIER COURSE WITH PAVE EDGE EDGING. SOLDIER COURSE PAVERS SHALL BE PERPENDICULAR TO THE GRANITE BAND. REMAINING PAVERS INSIDE THE SOLDIER COURSE SHALL BE LAID IN A 45° HERRINGBONE PATTERN.

NEOPRENE–MODIFIED ASPHALT ADHESIVE: FURNISH ADHESIVE THAT CONTAINS 2% GRADE WM1 NEOPRENE, OXIDIZED ASPHALT WITH A 150 DEGREE SOFTENING POINT (77 PENETRATION), AND 10% LONG-FIBERED INERT MATERIAL, AS SUPPLIED BY SEIDEL COMPANY, INC., NEWBURYPORT, MA, (617) 649–6740; HASTINGS PAVEMENT COMPANY, INC., LAKE SUCCESS, NY, (516) 379–3500; OR APPROVED EQUAL.

BITUMINOUS SETTING BED, 3/4 INCH: FURNISH ASPHALT CEMENT CONFORMING TO ASTM D 3381, PG64–22. FINE AGGREGATE SHALL BE NATURAL SAND AND/OR STONE SAND COMPOSED OF HARD, DURABLE, UNCOATED PARTICLES, FREE FROM CLAY, SILT, ORGANIC MATERIAL, OR OTHER DELETERIOUS SUBSTANCES. ALL SAND SHALL BE UNIFORMLY GRADED AND PASS A NO. 4 SIEVE, MEETING THE REQUIREMENTS OF ASTM C 136. THE DRIED FINE AGGREGATE SHALL BE MIXED WITH HOT ASPHALT CEMENT AT THE PLANT AND HEATED TO APPROXIMATELY 300 DEGREES (F). APPROXIMATE MATERIAL PROPORTIONS SHALL BE 7% ASPHALT CEMENT AND 93% FINE AGGREGATE; OR 140 LBS ASPHALT TO 1,860 LBS FINE AGGREGATE PER TON.

ASPHALT TACK COAT: PER ITEM 407, WITH APPLICATION RATE OF 0.10 GAL./S.Y.

SAND: THE CONTRACTOR SHALL INSTALL SAND TO ALL JOINTS WITH ALLIANCE GATOR MAXX POLYMERIC SAND, OR APPROVED EQUAL PER MANUFACTURER’S RECOMMENDATIONS AND SWEEP SURFACE CLEAN. PAVERS: SLATE GREY

MANUFACTURER:	DISTRIBUTOR:
ALLIANCE DESIGNER PRODUCTS	OBERFIELD’S
225 BOULEVARD BELLEROSE WEST	1165 ALUM CREEK DRIVE
LAVAL, QUEBEC, CANADA H7L 6A1	COLUMBUS, OHIO 43209
450–624–1161	614–252–0955

EDGING: PERMALOC ASPHALT EDGE, OR APPROVED EQUAL;
COLOR: BLACK

PAYMENT: PAYMENT FOR THE WORK NOTED ABOVE WILL BE MADE AT THE UNIT PRICE BID PER SQUARE FOOT FOR ITEM SPECIAL – PAVERS, REMOVED AND RESET AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, ASPHALT ADHESIVE, BITUMINOUS SETTING BED, TACK COAT, SAND, EDGING AND ANY OTHER INCIDENTALS NECESSARY FOR PLACEMENT OF PAVERS ON EXISTING CONCRETE BASE. FURNISHMENT OF NEW PAVERS SHALL BE PAID FOR SEPARATELY UNDER ITEM SPECIAL – BRICK PAVERS.

ITEM SPECIAL – BRICK PAVERS

THIS ITEM INCLUDES FURNISHING NEW BRICK PAVERS. THE PAVERS SHALL BE GEORGIAN EDGE BEALE STREET HD LUGGED BRICKS, 8” x 4” x 2–3/4”, OR APPROVED EQUAL. COST TO INSTALL PAVERS SHALL BE INCLUDED IN ITEM SPECIAL – PAVERS, REMOVED AND RESET. ANY UNUSED NEW PAVERS SHALL BE DELIVERED TO THE CITY.

PAYMENT WILL BE MADE AT THE UNIT PRICE BID PER SQUARE FOOT FOR ITEM SPECIAL – BRICK PAVERS AND SHALL ONLY INCLUDE FURNISHMENT OF THE PAVERS AND DELIVERY TO THE CITY. ALL LABOR, MATERIAL, AND EQUIPMENT NECESSARY FOR INSTALLATION SHALL BE PAID FOR UNDER ITEM SPECIAL – PAVERS, REMOVED AND RESET.

OVERALL GENERAL CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE CITY ENGINEER.

THE FOLLOWING OVERALL GENERAL CONTINGENCY QUANTITIES HAVE BEEN INCLUDED FOR USE AS NEEDED AND AS DIRECTED BY THE ENGINEER. A PORTION OF THE CONTINGENCY QUANTITIES MAY BE INCLUDED WITH SPECIFIC WORK AREAS (LISTED IN ESTIMATED QUANTITIES FOR SPECIFIC AREAS):

ESTIMATED CONTINGENCY QUANTITIES			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	FT	500
202	CURB REMOVED AND DISPOSED OF, AS PER PLAN	FT	50
202	CONCRETE REMOVED AND DISPOSED OF, AS PER PLAN	SF	500
203	EXCAVATION	CY	20
203	EXCAVATION, ROADWAY UNDERCUT, AS PER PLAN	CY	100
203	EXCAVATION AND ROOT PRUNING, AS PER PLAN	CY	30
251	PARTIAL DEPTH PAVEMENT REPAIR (DRIVE REPAIR) (4")	SY	100
253	PAVEMENT REPAIR (DRIVE REPAIR) (6")	SY	100
255	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT (DRIVE REPAIR)	SY	200
255	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT (8")	SY	25
301	ASPHALT CONCRETE BASE, PG64-22	TON	40
304	AGGREGATE BASE	CY	100
441	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	TON	100
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	100
604	MANHOLE, CATCH BASIN, OR INLET ADJUSTED TO GRADE, AS PER PLAN	EACH	5
604	MANHOLE, CATCH BASIN, OR INLET RECONSTRUCTED TO GRADE, AS PER PLAN	EACH	5
608	4" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	250
608	6" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	250
608	CURB RAMPS	EACH	2
608	DETECTABLE WARNING, TYPE A, AS PER PLAN	SF	16
608	BRICK PAVER REMOVE & RESET, TYPE 2, AS PER PLAN	SF	50
609	COMBINATION CURB AND GUTTER, 6" THICK, AS PER PLAN	FT	300
609	COMBINATION CURB AND GUTTER, 8" THICK, AS PER PLAN	FT	100
609	STRAIGHT 18" CURB, AS PER PLAN	FT	100
624	MOBILIZATION - UNDERCUT / BASE REPAIR	EACH	5
644	STOP LINE	FT	100
644	CROSSWALK LINE, 12"	FT	200
653	TOPSOIL FURNISHED AND PLACED, AS PER PLAN	CY	400
659	SEEDING AND MULCHING, AS PER PLAN	SY	500
659	WATER FOR IRRIGATION, AS PER PLAN	MGAL	100
659	COMMERCIAL FERTILIZER	TON	1.00
807	VALVE BOXES ADJUSTED TO GRADE	EACH	5
807	CURB BOXES ADJUSTED TO GRADE	EACH	2
SPECIAL	CURB CAN HOLES	EACH	10
SPECIAL	HARDWOOD MULCH	CY	100
SPECIAL	DRAIN LEADER PIPE	FT	300
SPECIAL	MAILBOX REPLACED	EACH	5

ADDITIONAL MISCELLANEOUS CURB AND PAVEMENT REPAIR LOCATIONS

ALL LOCATIONS WILL BE MARKED IN THE FIELD BY THE CITY OF DUBLIN PRIOR TO CONSTRUCTION. (QUANTITIES ARE INCLUDED IN THE GENERAL SUMMARY).

7658 TULLYMORE DRIVE:	
ITEM 202 – CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	125 FT
ITEM 609 – COMBINATION CURB AND GUTTER, 6" THICK AS PER PLAN	125 FT



GPD GROUP
Claus, Pyke, Schorrer, Burns & Dehaven, Inc.
Consulting Engineers, Architects, Planners, Inc. 2022

DESIGNER	
MSS	
REVIEWER	
LMO 2/18/25	
PROJECT ID	
25-002-CIP	
SHEET	TOTAL
P.9	60

SEQUENCE OF CONSTRUCTION (POST ROAD)

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SAFE TRANSITION OF TRAFFIC BETWEEN VARIOUS CONSTRUCTION OPERATIONS AND MAINTENANCE OF TRAFFIC PHASES. THE FOLLOWING SEQUENCE OF CONSTRUCTION NOTES ARE INTENDED TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC WHILE PROVIDING ADEQUATE WORK ZONE SPACE FOR CONSTRUCTION ACTIVITIES.

PHASE 1

THE CONTRACTOR SHALL CLOSE THE SOUTHBOUND INSIDE LEFT TURN LANE ON EMERALD PARKWAY. THE CONTRACTOR SHALL CLOSE THE CURB LANES ON POST ROAD IN ACCORDANCE WITH ODOT SCD MT-95.31. THE CONTRACTOR SHALL CONSTRUCT THE ROADWAY IMPROVEMENTS TO THE CURB LANES ON POST ROAD.

PHASE 2

THE INSIDE SOUTHBOUND LEFT TURN LANE SHALL REMAIN CLOSED ON EMERALD PARKWAY. THE CONTRACTOR SHALL CLOSE THE INSIDE LANES ON POST ROAD IN ACCORDANCE WITH ODOT SCD MT-95.32. THE CONTRACTOR SHALL CONSTRUCT THE ROADWAY IMPROVEMENTS TO THE INSIDE LANES OF POST ROAD.

PHASE 3

THE CONTRACTOR SHALL PLACE THE FINAL PAVEMENT MARKINGS WHILE MAINTAINING TRAFFIC IN ACCORDANCE WITH ODOT SCD MT-99.20.

METHOD OF CONSTRUCTION (WOERNER TEMPLE ROAD REALIGNMENT)

THE CONTRACTOR SHALL MAINTAIN TRAFFIC IN THE EASTBOUND DIRECTION AT ALL TIMES. WESTBOUND TRAFFIC SHALL BE DETOURED AS SHOWN ON SHEET 15. THE CONTRACTOR SHALL CONSTRUCT THE IMPROVEMENTS FOR THE REALIGNMENT TO WOERNER TEMPLE ROAD.

METHOD OF CONSTRUCTION (AVERY ROAD/JACANA DRIVE RECONSTRUCTION)

THE CONTRACTOR SHALL CLOSE THE AVERY ROAD / JACANA DRIVE INTERSECTION. TRAFFIC SHALL BE DETOURED AS SHOWN ON SHEET 16-17. THE CONTRACTOR SHALL RECONSTRUCT THE INTERSECTION OF AVERY ROAD / JACANA DRIVE.

METHOD OF CONSTRUCTION (MCKITRICK ROAD RECONSTRUCTION)

THE CONTRACTOR SHALL CLOSE THE MCKITRICK ROAD / BRENHAM WAY INTERSECTION. TRAFFIC SHALL BE DETOURED AS SHOWN ON SHEET 18 AND IN APPENDIX A. THE CONTRACTOR SHALL RECONSTRUCT THE INTERSECTION OF MCKITRICK ROAD / BRENHAM WAY.

DETOUR SIGNING

THE CONTRACTOR SHALL FURNISH, ERECT, MAINTAIN, AND SUBSEQUENTLY REMOVE SIGNS, SIGN SUPPORTS AND BARRICADES OF THE TYPE AND LOCATION SHOWN ON THESE PLANS.

ALL COSTS ASSOCIATED WITH THE COST OF PROVIDING, INSTALLING, MAINTAINING AND REMOVING DETOUR SIGNING SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 – DETOUR SIGNING.

DUST CONTROL (WOERNER TEMPLE ROAD REALIGNMENT, MCKITRICK ROAD RECONSTRUCTION AND AVERY ROAD / JACANA ROAD IMPROVEMENTS)

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616, WATER 25 MGAL

DRUM REQUIREMENTS

PAYMENT FOR DRUMS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.

SIGNS, BARRICADES, AND DRUMS

ALL CONSTRUCTION SIGNS, BARRICADES, AND DRUMS SHALL CONFORM TO THE OMUTCD AND THE LATEST REVISION OF THE ODOT CONSTRUCTION AND MATERIAL SPECIFICATIONS.

ALL CONSTRUCTION SIGNAGE SHALL BE INSTALLED AND COVERED BEFORE CONSTRUCTION BEGINS. AFTER CONSTRUCTION SIGN INSTALLATION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER THREE (3) WORKING DAYS BEFORE WORK BEGINS AND REQUEST AN INSPECTION OF ALL SIGNAGE.

FACES OF CONSTRUCTION SIGNS AND REFLECTIVE SHEETING ON BARRICADES SHALL BE TYPE "H" VIP. ALL ORANGE CONSTRUCTION SIGNS SHALL BE FLUORESCENT ORANGE. ALL SHEETING WILL BE TESTED FOR REFLECTIVITY PER ODOT 730.192. DRUM BANDS SHALL BE REFLECTORIZED WITH TYPE "G" (HIGH INTENSITY) SHEETING COMPLYING WITH THE REQUIREMENTS OF 730.19. ALL SIGNS, BARRICADES, AND DRUMS WILL BE LIKE NEW AND IN GOOD CONDITION IN CONFORMANCE WITH "QUALITY GUIDELINES FOR TEMPORARY TRAFFIC CONTROL DEVICES AND FEATURES" PUBLISHED BY ATSSA.

MAINTENANCE OF ALL CONTRACTOR SUPPLIED SIGNS, BARRICADES, AND DRUMS IS THE CONTRACTOR'S RESPONSIBILITY. IF THE CONTRACTOR FAILS TO CORRECT DEFICIENCIES WITHIN FOUR (4) HOURS OF NOTIFICATION, THE CITY WILL CORRECT OR HIRE SOMEONE TO CORRECT THE DEFICIENCIES. THE CONTRACTOR WILL THEN BE BACK CHARGED PER ODOT SPECIFICATIONS 614.14. IN THE CASE THAT BACK CHARGING THE CONTRACTOR IS NOT APPLICABLE, THE CITY WILL RESCIND AND WITHHOLD ALL PERMITS ISSUED TO THE CONTRACTOR TO WORK WITHIN CITY RIGHT-OF-WAY UNTIL THE ISSUE IS SETTLED.

THESE PROVISIONS SHALL NOT IN ANY WAY RELIEVE THE CONTRACTOR OF ANY OF HIS LEGAL RESPONSIBILITIES OR LIABILITIES FOR THE SAFETY OF THE PUBLIC.

ALL BARRICADES AT CLOSURES SHALL HAVE YELLOW TYPE "C" STEADY BURN LIGHTS. DRUMS IN TAPERS SHALL HAVE TYPE "A" FLASHING LIGHTS.

ALL ADVANCE SIGNING SHALL BE EQUIPPED WITH TYPE "A" FLASHING LIGHTS AND TWO (2) ORANGE FLAGS (24"x24"). LIGHTS ARE NOT REQUIRED ON SIGNS IN PLACE DURING DAYLIGHT HOURS.

CONES ARE NOT APPROVED FOR USE AT NIGHT.

DRUMS SHALL BE PLACED AS FOLLOWS: 25' C/C ON TANGENTS AND TAPERS, AND 8' C/C ON RADII.

WORK HOURS AND RESTRICTIONS

- OFF-PEAK HOURS ARE DEFINED AS ANY PERIOD OTHER THAN 6:00-8:00AM AND 3:00-6:00PM (MONDAY THRU FRIDAY) AND LEGAL HOLIDAYS.
- NIGHTTIME HOURS ARE DEFINED AS BETWEEN 9:00PM AND 7:00AM.
- WEEKEND HOURS ARE DEFINED AS BETWEEN 7:00PM FRIDAY THRU 7:00AM MONDAY.
- THE CONTRACTOR SHALL COMPLETE ALL WORK ON POST ROAD WITHIN SIX (6) WEEKS (42 CONSECUTIVE CALENDAR DAYS) OF THE START OF ANY CONSTRUCTION ACTIVITIES. LIQUIDATED DAMAGES PER CM&S 108.07 SHALL BE ASSESSED FOR EACH CALENDAR DAY THE LANES REMAIN CLOSED TO TRAFFIC BEYOND THE SPECIFIC LIMITS.
- ALL LANES ON POST ROAD SHALL BE OPEN TO VEHICLES FOR THE MEMORIAL TOURNAMENT, THE DUBLIN IRISH FESTIVAL AND THE DUBLIN IRISH FESTIVAL 5K. SEE SHEET 3 FOR THE SPECIFIC DATES.
- THE CONTRACTOR SHALL NOT CLOSE WOERNER TEMPLE ROAD NOR CONSTRUCT THE ROAD REALIGNMENT WHEN THE DUBLIN COMMUNITY POOL SOUTH IS OPEN FOR THE SEASON (5/26/25 THRU 9/1/25).
- THE CONTRACTOR SHALL BE PERMITTED TO CLOSE THE ROUNDABOUTS FOR 3 CONSECUTIVE NIGHTS DURING NIGHTTIME HOURS PER ROUNDABOUT FOR CONSTRUCTION.
- THE CONTRACTOR SHALL COMPLETE ALL WORK ON THE COSGRAY ROAD / SHIER RINGS ROUNDABOUT BETWEEN JUNE 14, 2025 AND JULY 25, 2025.
- THE CONTRACTOR SHALL COMPLETE ALL WORK ON AVERY ROAD / JACANA DRIVE INTERSECTION BETWEEN 8:00 AM MONDAY, AUGUST 11TH, 2025 AND 9:00 PM FRIDAY, AUGUST 15TH, 2025.

ITEM 614 – MAINTAINING TRAFFIC (POST ROAD, WOERNER TEMPLE ROAD REALIGNMENT, MCKITRICK ROAD RECONSTRUCTION AND AVERY ROAD / JACANA ROAD IMPROVEMENTS)

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE CONTRACTOR SHALL MAINTAIN ACCESS FROM PUBLIC ROADWAYS TO ALL ABUTTING PROPERTIES THROUGHOUT CONSTRUCTION OF THIS PROJECT, AS WELL AS ACCESS FOR MAIL, WATER, SANITARY SERVICE, AND EMERGENCY VEHICLES.

LENGTH AND DURATION OF SIDEWALK CLOSURE AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO PEDESTRIAN TRAFFIC. SIDEWALK CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH THE CONTRACT.

THE CONTRACTOR SHALL PROVIDE, ERECT, AND SUBSEQUENTLY REMOVE ALL SIGNS, SIGN SUPPORTS, FLAGS AND BARRICADES, AND PROVIDE ALL FLAGGERS AND WATCHERS AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

IN THE EVENT THAT IT BECOMES NECESSARY FOR THE CITY TO PERFORM WORK OF AN IMMEDIATE NATURE (SUCH AS THE PLACEMENT OF BARRICADES OR REPLACEMENT OF SIGNS AND OTHER WARNING OR PROTECTIVE DEVICES) BECAUSE OF FAILURE OR REFUSAL OF THE CONTRACTOR TO PERFORM SUCH WORK AS REQUIRED BY THE CONTRACT, THE CONTRACTOR SHALL REIMBURSE THE CITY AT A RATE OF 2.5 TIMES ACTUAL COST OF LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PERFORM SUCH WORK. THE CITY SHALL BE REQUIRED TO NOTIFY OR ATTEMPT TO NOTIFY THE DESIGNATED REPRESENTATIVE OF THE CONTRACTOR OF THE NECESSITY TO PERFORM SUCH WORK. IF THE CONTRACTOR REFUSES OR FAILS WITHIN A REASONABLE TIME TO PERFORM OR CAUSE THE PERFORMANCE OF SUCH WORK, THE CITY SHALL BE REIMBURSED BY THE CONTRACTOR IN THE AMOUNT PROVIDED HEREIN BY WAY OF A DEDUCTION FROM THE CONTRACTOR'S NEXT PAYMENT UNDER THE CONTRACT. REASONABLE TIME FOR ALL STREETS INVOLVED IN THIS CONTRACT IS ONE (1) HOUR FROM THE TIME OF NOTIFICATION BY THE CITY.

THE CONTRACTOR SHALL NOTIFY THE CITY OF DUBLIN FOURTEEN (14) DAYS PRIOR TO THE START OF CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL IMMEDIATELY INFORM THE CITY OF DUBLIN OF ANY AND ALL DELAYS AND/OR CHANGES REGARDING THE CONSTRUCTION PROJECT. THE CITY ENGINEER WILL PROVIDE CLARIFICATION FOR ANY QUESTIONS ABOUT THE NOTIFICATION REQUIREMENT.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION, LATEST REVISION). PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

MAINTENANCE OF TRAFFIC

ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, INSTALLED, AND MAINTAINED IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION. THE LIMITS OF THE CONSTRUCTION WORK SHALL BE MARKED WITH THE APPROPRIATE SIGNAGE (ROAD WORK, AND END ROAD WORK) PRIOR TO THE COMMENCEMENT OF ANY WORK OR STAGING OF EQUIPMENT. SPECIAL DUTY POLICE OFFICERS HAVING JURISDICTION IN THE CITY OF DUBLIN SHALL BE SUBSTITUTED WHERE REFERENCE IS MADE TO "FLAGGER" FOR THE FOLLOWING: DURING MILLING AND PAVING OF SIGNALIZED INTERSECTIONS AND AS DIRECTED BY THE ENGINEER. ALL EXCAVATION AND WORK AREAS ARE TO BE ADEQUATELY MARKED AND SECURED WITH THE APPROPRIATE WARNING DEVICES. ALL NECESSARY TRAFFIC CONTROL DEVICES MUST BE ON SITE, ERECTED, AND MAINTAINED PRIOR TO BEGINNING ANY PHASE OF THE WORK. THE USE OF A LAW ENFORCEMENT OFFICER WITH PATROL CAR SHALL BE REQUIRED WHEN TRAFFIC IS BEING DIRECTED THROUGH SIGNALIZED INTERSECTIONS.

THE COSTS FOR ERECTION, MAINTENANCE AND REMOVAL OF ALL DEVICES SHALL BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 – MAINTAINING TRAFFIC. COSTS FOR SPECIAL DUTY POLICE OFFICERS SHALL BE PAID UNDER ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR. COSTS FOR FLAGGERS SHALL BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 – MAINTAINING TRAFFIC.

WORK ZONES SHALL BE ADEQUATELY SECURED DURING AND OUTSIDE OF WORKING HOURS.

WORK PLAN: THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE ENGINEER A WORK AND TRAFFIC CONTROL PLAN FOR THE PHASES OF WORK PLANNED FOR THE WORK AREAS BY THE END OF DAY WEDNESDAY THE WEEK BEFORE WORK IN QUESTION WILL BEGIN. WORK PLAN SHALL INCLUDE AT A MINIMUM: THE VARIOUS SCOPES OF WORK AND PHASES, PLANNED START AND COMPLETION DATES FOR THE PHASES OF WORK, PLANNED ROUTE OR DIRECTION FOR WORK TO PROGRESS DURING PHASES, MAINTENANCE OF TRAFFIC DURING THE PHASES OF WORK, AND OTHER PERTINENT INFORMATION.

ACCESS TO ALL ADJOINING PROPERTIES AS WELL AS ACCESS FOR MAIL, WATER, SANITARY SERVICE AND EMERGENCY VEHICLES SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT. THE CONTRACTOR MAY SUBMIT ALTERNATIVE METHODS FOR THE MAINTENANCE OF TRAFFIC, PROVIDED THE INTENT OF THE PROVISIONS CONTAINED HEREIN ARE FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC RESULTS THEREFROM. THE CONTRACTOR SHALL PROVIDE 10' STEEL PLATES FOR EACH AFFECTED RESIDENCE. DRIVEWAY REPAIRS TO BE PERFORMED WITHIN 3 DAYS OF THE PLATES BEING REMOVED. THE MAINTENANCE OF TRAFFIC PLAN MUST INCORPORATE ANY TRAFFIC CONTROL DETAILS CONTAINED WITHIN THESE CONSTRUCTION DRAWINGS. THE MAINTENANCE OF TRAFFIC PLAN PROPOSED BY THE CONTRACTOR MUST BE APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION.

THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE-LANE, TWO-WAY TRAFFIC AT ALL TIMES UNLESS OTHERWISE SHOWN ON THE CONSTRUCTION PLANS. FLAGGERS WILL BE PROVIDED ANY TIME ONE-LANE, TWO-WAY TRAFFIC IS ESTABLISHED (PER ITEM 614) AND AS DIRECTED BY THE ENGINEER.

IF THE CITY ENGINEER DETERMINES PROPER PROVISIONS FOR THE TRAFFIC CONTROL ARE NOT BEING PROVIDED BY THE CONTRACTOR, THE CITY ENGINEER MAY STOP WORK UNTIL MAINTENANCE OF TRAFFIC ISSUES ARE CORRECTED OR THE CITY ENGINEER MAY ASSIGN OFF-DUTY POLICE OFFICERS TO THE PROJECT AT THE CONTRACTOR'S EXPENSE.

PAYMENT FOR THIS AND ALL TRAFFIC MAINTENANCE ITEMS (UNLESS OTHERWISE SPECIFIED) SHALL BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 – MAINTAINING TRAFFIC.

DESIGN AGENCY



DESIGNER

SRT

REVIEWER

AKF 2/18/25

PROJECT ID

25-002-CIP

SHEET

TOTAL

P.10

60

ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR, AS PER PLAN

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).
- DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL FOR THE FLOW OF TRAFFIC OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

ALL LEOS SHALL BE OFF-DUTY CITY OF DUBLIN POLICE OFFICERS, FRANKLIN COUNTY SHERIFF'S DEPUTIES, OR OHIO HIGHWAY PATROL HIRED BY THE CONTRACTOR.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK IS AT THE DIRECTION OF THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR, AS PER PLAN. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR, AS PER PLAN 60 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR, AS PER PLAN.

NOTIFICATION OF ACCESS RESTRICTION TO PROPERTY OWNER

THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 24 AND NO MORE THAN 48 HOURS WRITTEN NOTICE TO PROPERTY OWNERS AND RESIDENTS PRIOR TO CLOSING ANY DRIVEWAY OR PORTION THEREOF. CONFIRMATION OF THE WRITTEN NOTICES SHALL BE PROVIDED TO THE CITY ENGINEER BY THE CONTRACTOR ON A DAILY BASIS. NO DRIVEWAY IS TO BE CLOSED FOR A PERIOD OF MORE THAN FIVE (5) DAYS. IN THE EVENT THAT THE FIVE (5) DAY MAXIMUM IS EXCEEDED, THE CONTRACTOR WILL NOT BE PERMITTED TO CLOSE ANY MORE DRIVEWAYS OR PROCEED WITH OTHER UNRELATED WORK ON THE PROJECT UNTIL COMPLIANCE IS ACHIEVED AND THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LIQUIDATED DAMAGES NOTE ON THIS SHEET. THE CONTRACTOR IS REQUIRED TO MAINTAIN ACCESS TO COMMERCIAL DRIVES AT ALL TIMES UNLESS PRIOR ARRANGEMENTS HAVE BEEN MADE WITH THE PROPERTY OWNER, PROPRIETOR, AND ENGINEER. THE CONTRACTOR MAY BE PERMITTED TO CLOSE A PORTION OF A COMMERCIAL ENTRANCE WITH THE PERMISSION OF THE ENGINEER AS LONG AS TRAFFIC IS MAINTAINED THROUGH THE REMAINING SECTION OF THE ENTRANCE.

THE CONTRACTOR WILL IMMEDIATELY RE-ESTABLISH ACCESS IF NOTIFICATION WAS NOT PERFORMED. A DEDUCTION OF \$500 PER DAY PER INSTANCE SHALL BE ASSESSED FOR RESTRICTING ACCESS WITHOUT PRIOR NOTICE OR CONFORMANCE WITH THIS NOTE.

PUBLIC ACCESS MANAGEMENT

IT IS THE INTENT OF THIS CONTRACT THAT ALL ITEMS OF STREET REPAIR BE SCHEDULED AND PERFORMED BY THE CONTRACTOR IN A MANNER AS TO MINIMIZE INCONVENIENCE TO PEDESTRIAN AND VEHICULAR TRAFFIC. IT IS ALSO THE INTENT TO PROVIDE FOR AND MAINTAIN ACCESS TO RESIDENTIAL, COMMERCIAL, INDUSTRIAL, AND PUBLIC PROPERTIES AT ALL TIMES DURING THE PERFORMANCE OF THE WORK. THE CONTRACTOR SHALL PHASE WORK AND PROVIDE A 10' STEEL PLATE FOR EACH AFFECTED RESIDENCE IN ORDER TO MAINTAIN ACCESS TO DRIVEWAYS. DRIVEWAY REPAIRS TO BE COMPLETED WITHIN 3 DAYS OF PLATES BEING REMOVED. THE CONTRACTOR SHALL PROVIDE A SAFE AND UNOBSTRUCTED PEDESTRIAN ACCESS CORRIDOR ACROSS AREAS OF CURB EXCAVATION AT INTERVALS OF NO MORE THAN 200 FEET.

SIDEWALK CLOSURES

WARNING SIGNS AND BARRICADES SHALL BE POSTED IMMEDIATELY IN ADVANCE OF THE CLOSURE AND AT THE CLOSEST LEGAL CROSSWALK INDICATING THE CLOSURE AHEAD AND PROVIDE A DETOUR ROUTE AROUND THE WORK ZONE. THE WORK ON SIDEWALKS SHALL BE SCHEDULED AND COORDINATED SUCH THAT AN UNOBSTRUCTED DETOUR MAY BE PROVIDED AROUND THE WORK ZONE. SIDEWALK CLOSINGS SHALL NOT EXCEED 10 DAYS UNLESS OTHERWISE NOTED IN THE CONSTRUCTION DOCUMENTS.

FAILURE TO COMPLY WITH THIS NOTE WILL RESULT IN ALL OPERATIONS BEING SHUT DOWN UNTIL BROUGHT BACK INTO COMPLIANCE.

STREET CLOSURES

NO STREET MAY BE CLOSED NOR MAY ANY TRAFFIC MOVEMENTS BE RESTRICTED WITHOUT PRIOR APPROVAL BY THE ENGINEER.

THE CONTRACTOR SHALL PROVIDE THE CITY WITH 14 DAYS ADVANCE NOTICE REQUESTING APPROVAL BY THE ENGINEER PRIOR TO ANY ROAD CLOSURE.

WORK ZONE ESTIMATED QUANTITIES

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE ON POST ROAD:

PHASE 1

ITEM 614 – WORK ZONE EDGE LINE, CLASS I, 4”, (WHITE) 0.31 MILE

PHASE 2

ITEM 614 – WORK ZONE EDGE LINE, CLASS I, 4”, (YELLOW) 0.31 MILE

ITEM 614. PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A PORTABLE CHANGEABLE MESSAGE SIGN, THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCE OF 800 FEET AND 650 FEET RESPECTIVELY.

EACH SIGN SHALL BE TRAILER MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM TO DIM THE SIGN DURING DARKNESS AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. PCMS TRAILERS SHOULD BE DELINEATED.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE PCMS SHOULD NOT BE LOCATED IN THE MEDIAN OF THE HIGHWAY UNLESS IT IS PROTECTED FROM BOTH DIRECTIONS OF TRAFFIC. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE THE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS WILL BE OFF. ADDITIONALLY WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED, FACING AWAY FROM ALL TRAFFIC AND SHALL DISPLAY ONE OR MORE TYPE G YELLOW REFLECTIVE SHEETING SURFACES OF 9-INCH BY 15-INCH MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE CONTRACTOR. A LIST OF ALL PROPOSED PREPROGRAMMED MESSAGES WILL BE GIVEN TO THE ENGINEER PRIOR TO CONSTRUCTION. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PREPROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE LINE PRESENTATION FORMATS WITH UP TO OF SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DE-ACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR AREAS) ALLOW REMOTE SIGN ACTIVATION, DEACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF 614.07. THE CONTRACTOR SHALL PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC ACCRUED BY THE DEPARTMENT WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24 HOURS PER DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THEIR USE. THE REQUIREMENT TO FURNISH, INSTALL, MAINTAIN AND REMOVE A PCMS UNIT ON THIS PROJECT SHALL NOT IN ANY WAY RELIEVE THE CONTRACTOR OF HIS RESPONSIBILITIES AS OUTLINED IN 614.02.

ITEM 614. PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN (CONTINUED)

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

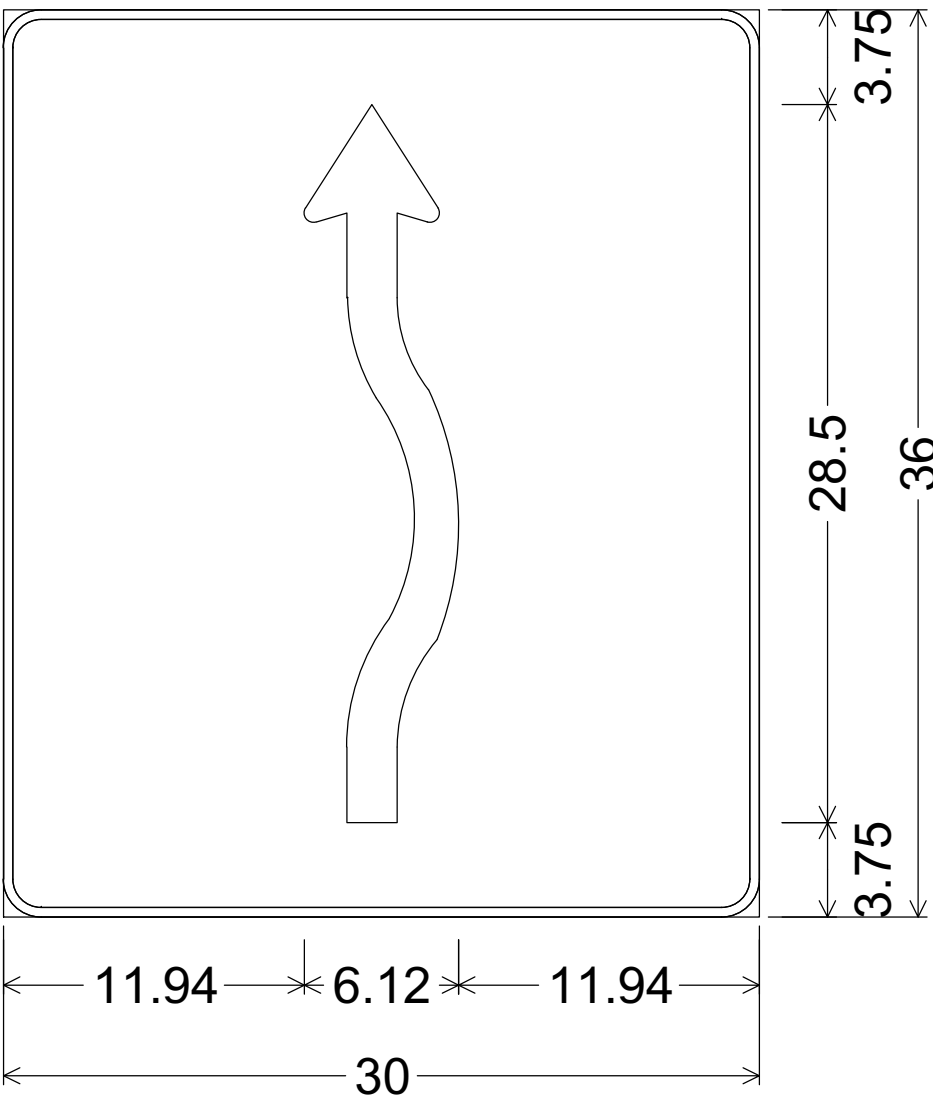
ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 14 SNMT

ASSUMING 2 PCMS SIGNS FOR 1.5 MONTHS (POST ROAD)

ASSUMING 2 PCMS SIGNS FOR 3 MONTHS (WOERNER TEMPLE ROAD REALIGNMENT)

ASSUMING 2 PCMS SIGNS FOR 0.5 MONTHS (AVERY ROAD/JACANA ROAD INTERSECTION)

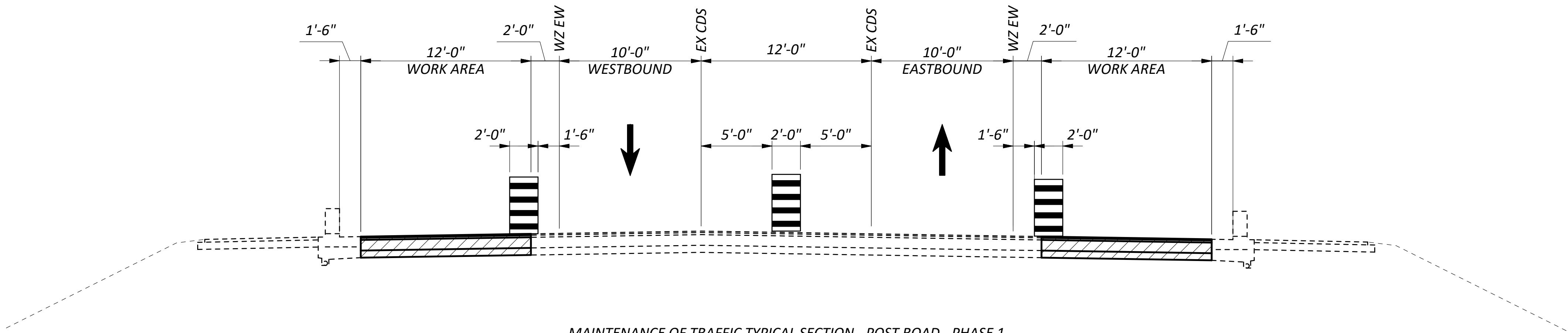
ASSUMING 2 PCMS SIGNS FOR 2 MONTHS (MCKITRICK ROAD/BRENHAN WAY INTERSECTION)



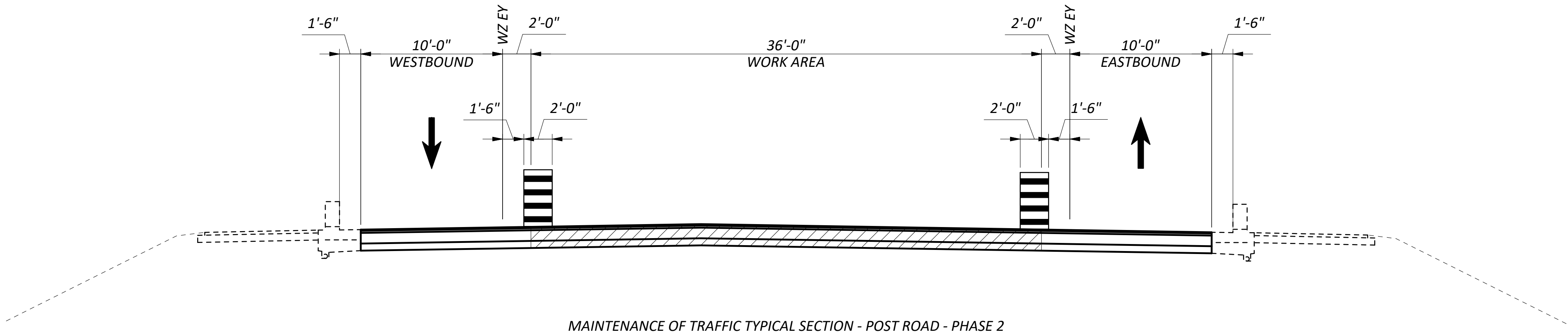
1.50" Radius, 0.38" Border, Black on Orange;
Table of widths and spaces

11.94	6.12	11.94
-------	------	-------

- NOTES:
- ALL DIMENSIONS ARE INCHES.



MAINTENANCE OF TRAFFIC TYPICAL SECTION - POST ROAD - PHASE 1



MAINTENANCE OF TRAFFIC TYPICAL SECTION - POST ROAD - PHASE 2

DESIGN AGENCY



DESIGNER

SRT

REVIEWER

AKF 2/18/25

PROJECT ID

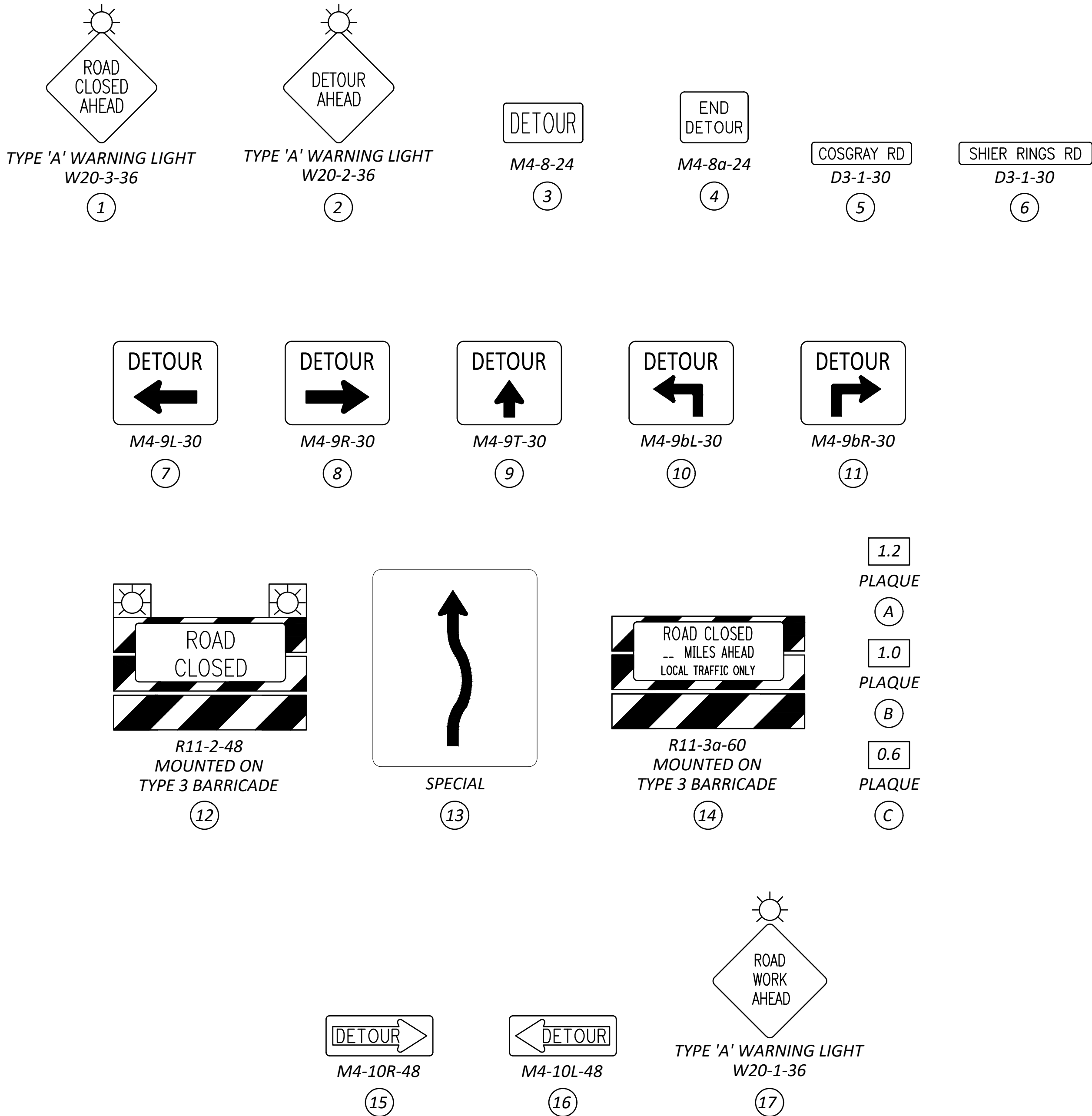
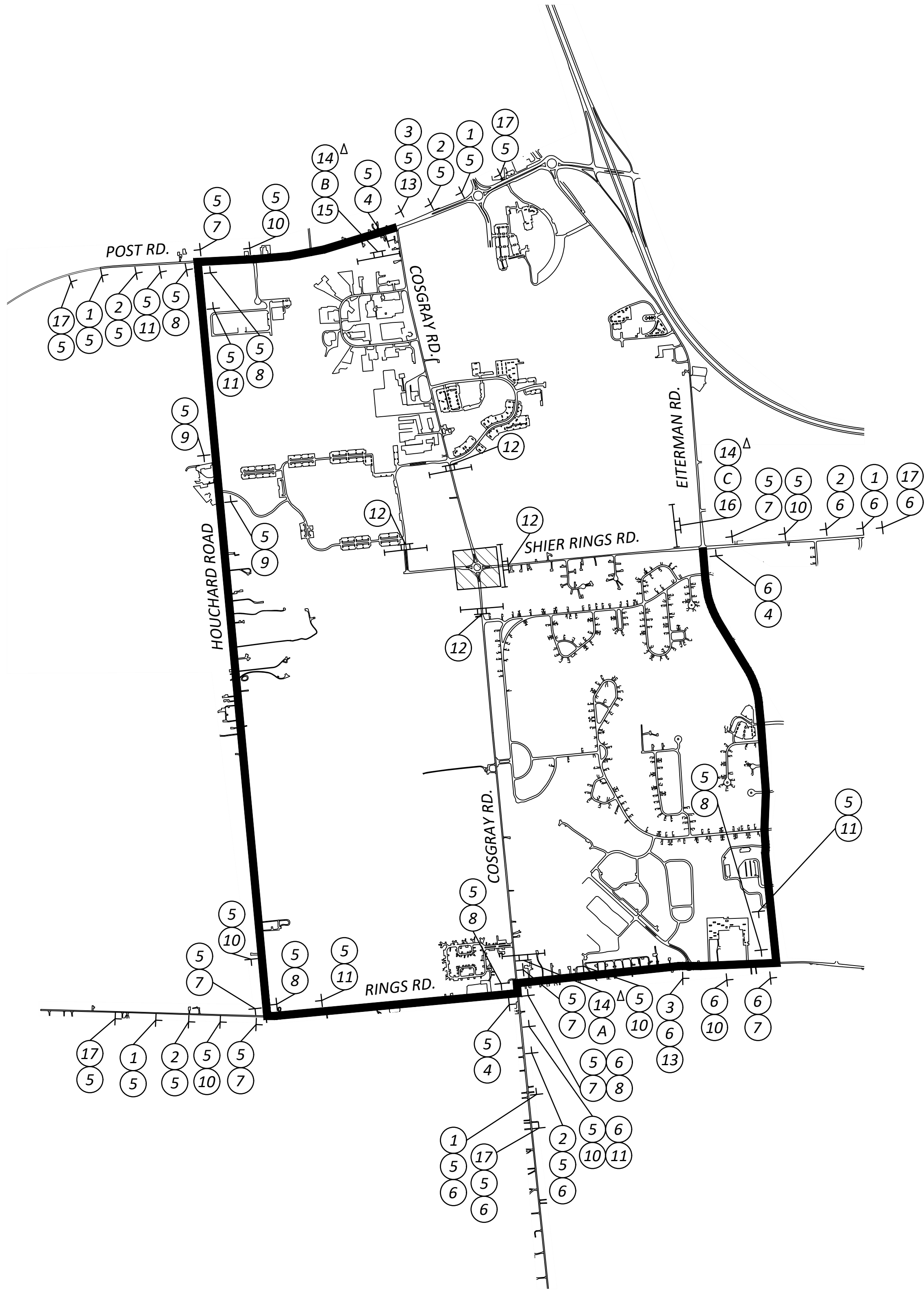
25-002-CIP

SHEET

P.12

TOTAL

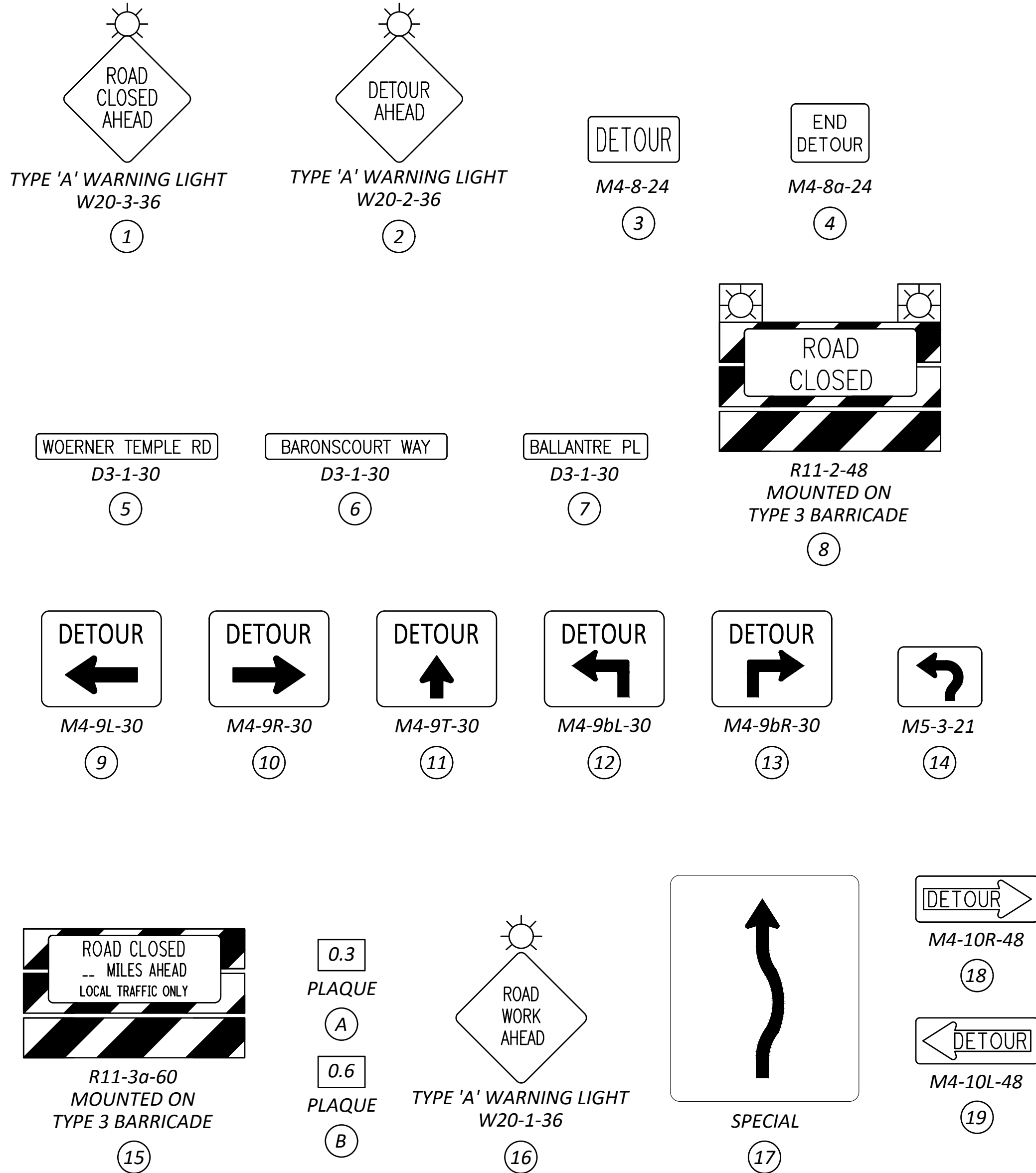
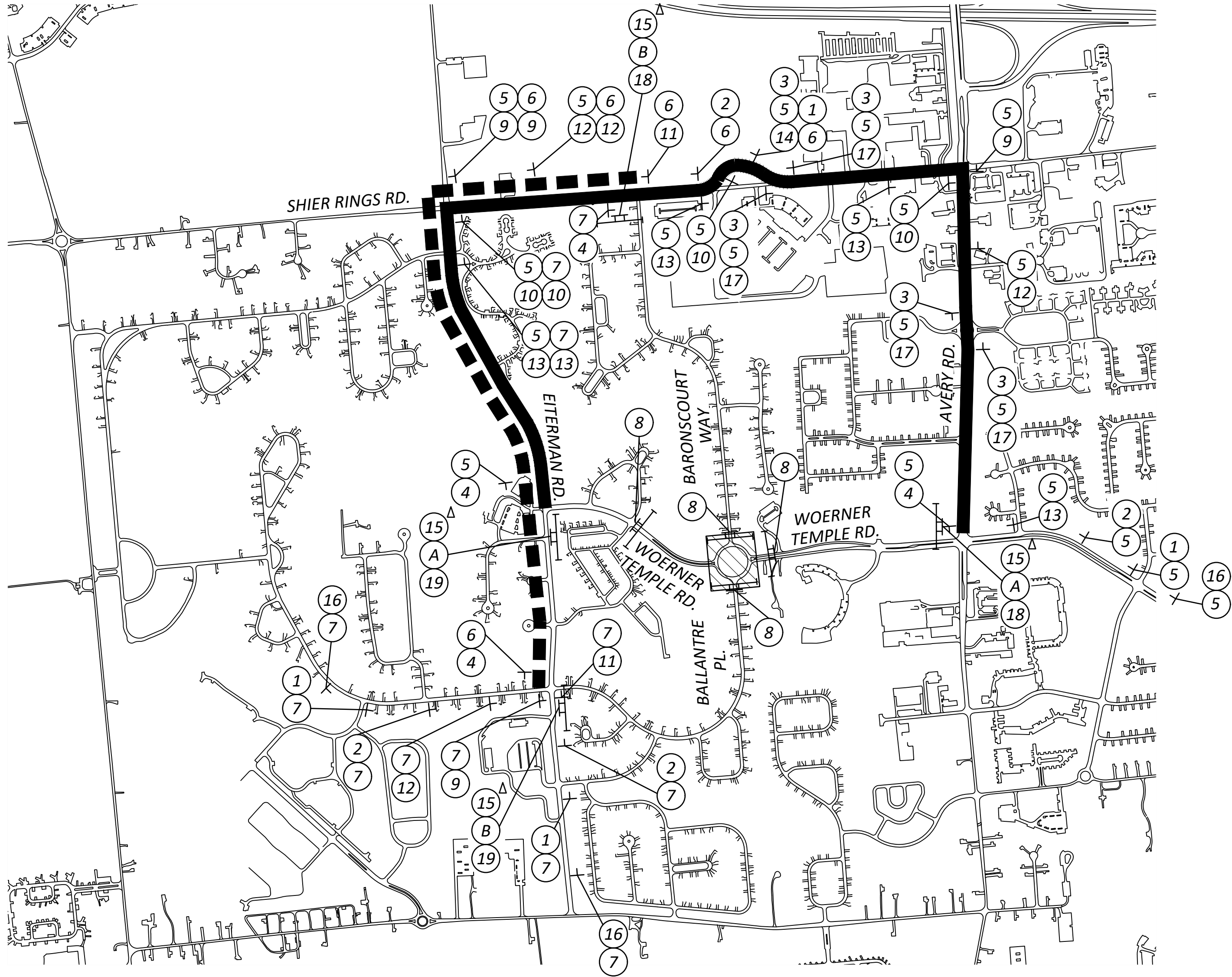
60



Δ BARRICADE SHALL BE PLACED ADJACENT TO ROAD.

DETOUR LEGEND	
	SIGN
	TYPE 3 BARRICADE
	WORK AREA
	DETOUR ROUTE

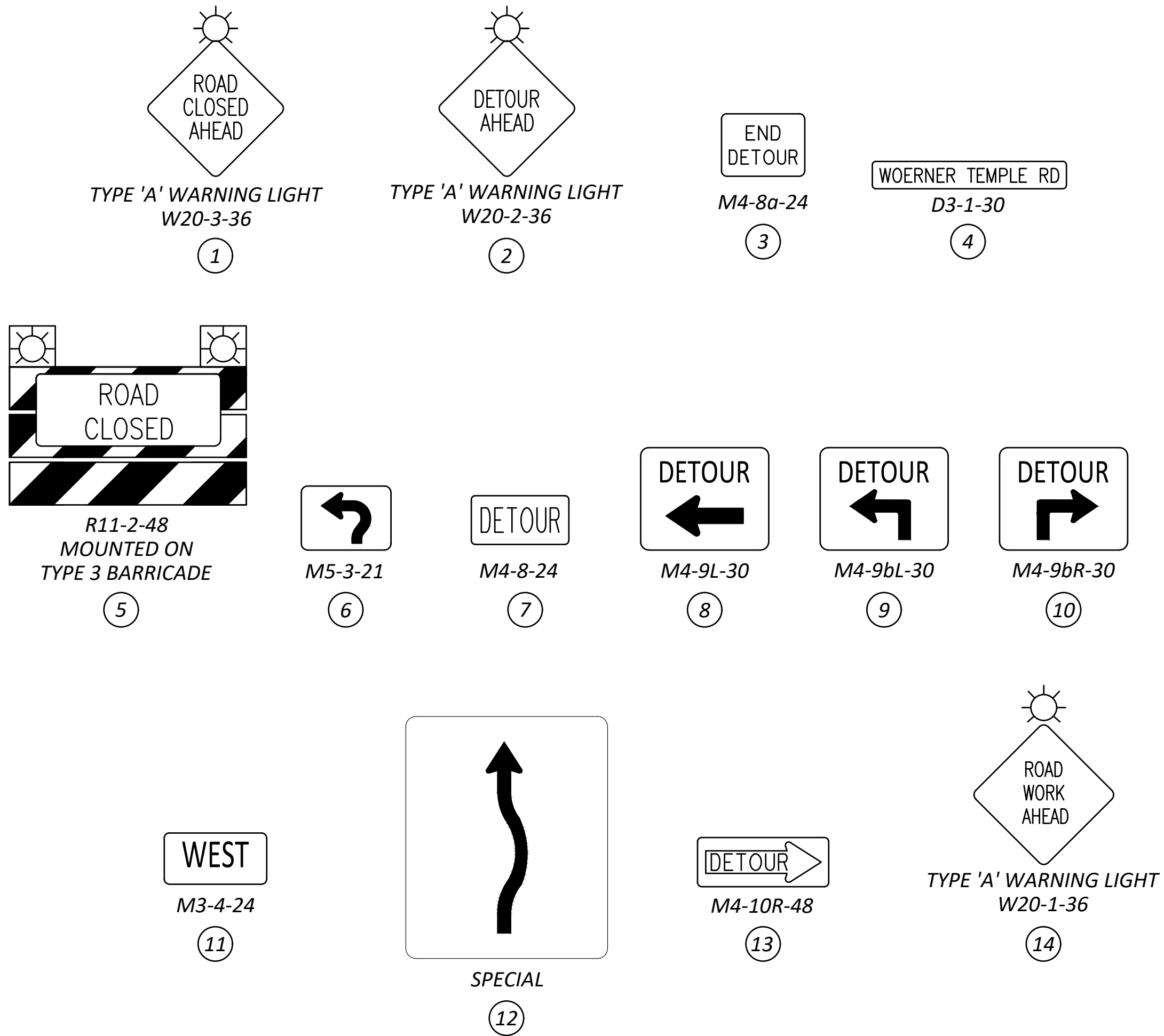
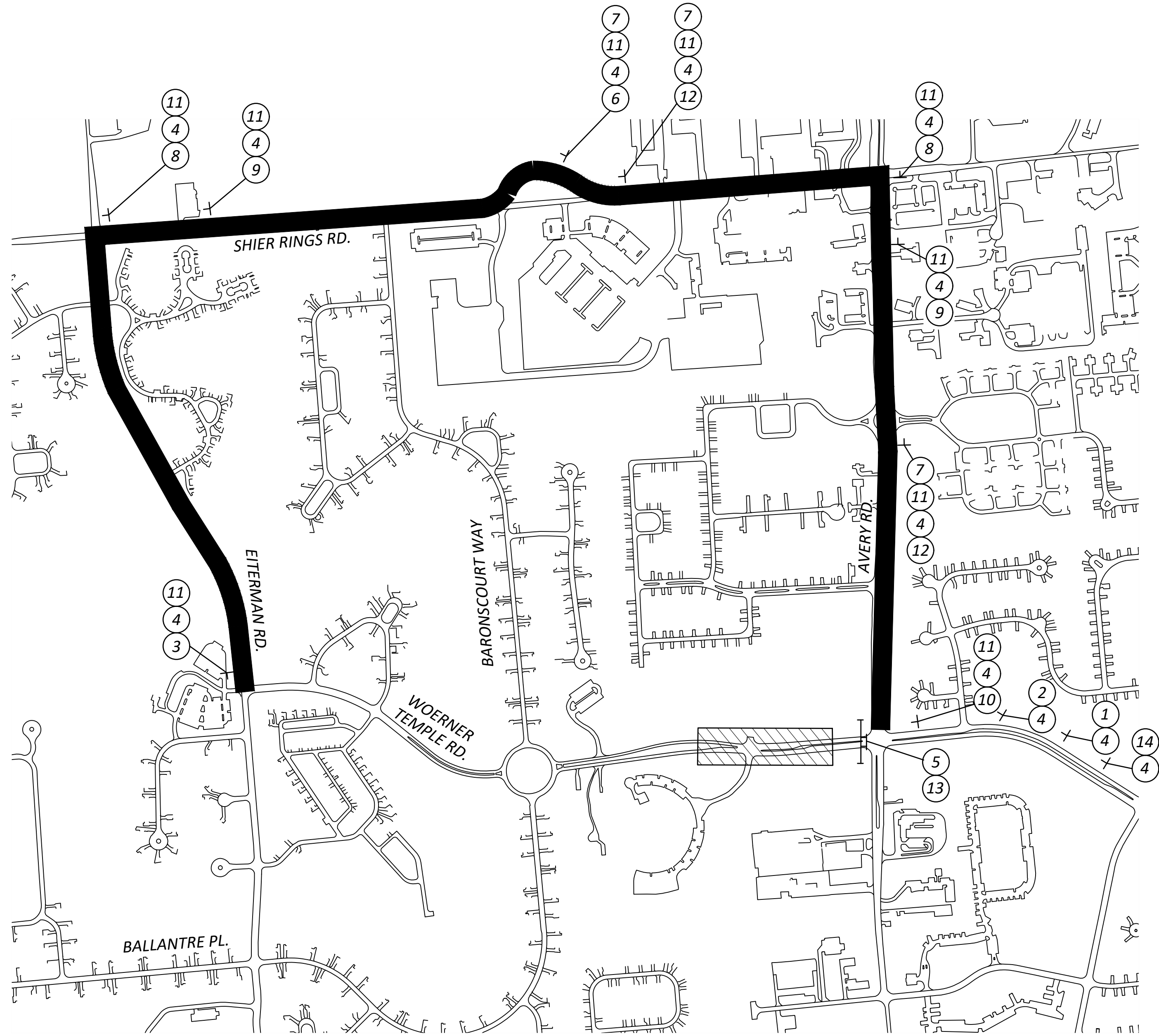




Δ BARRICADE SHALL BE
PLACED ADJACENT TO ROAD.



DETOUR LEGEND	
	SIGN
	TYPE 3 BARRICADE
	WORK AREA
	WOERNER TEMPLE RD. DETOUR ROUTE
	BARONSCOURT WAY DETOUR ROUTE

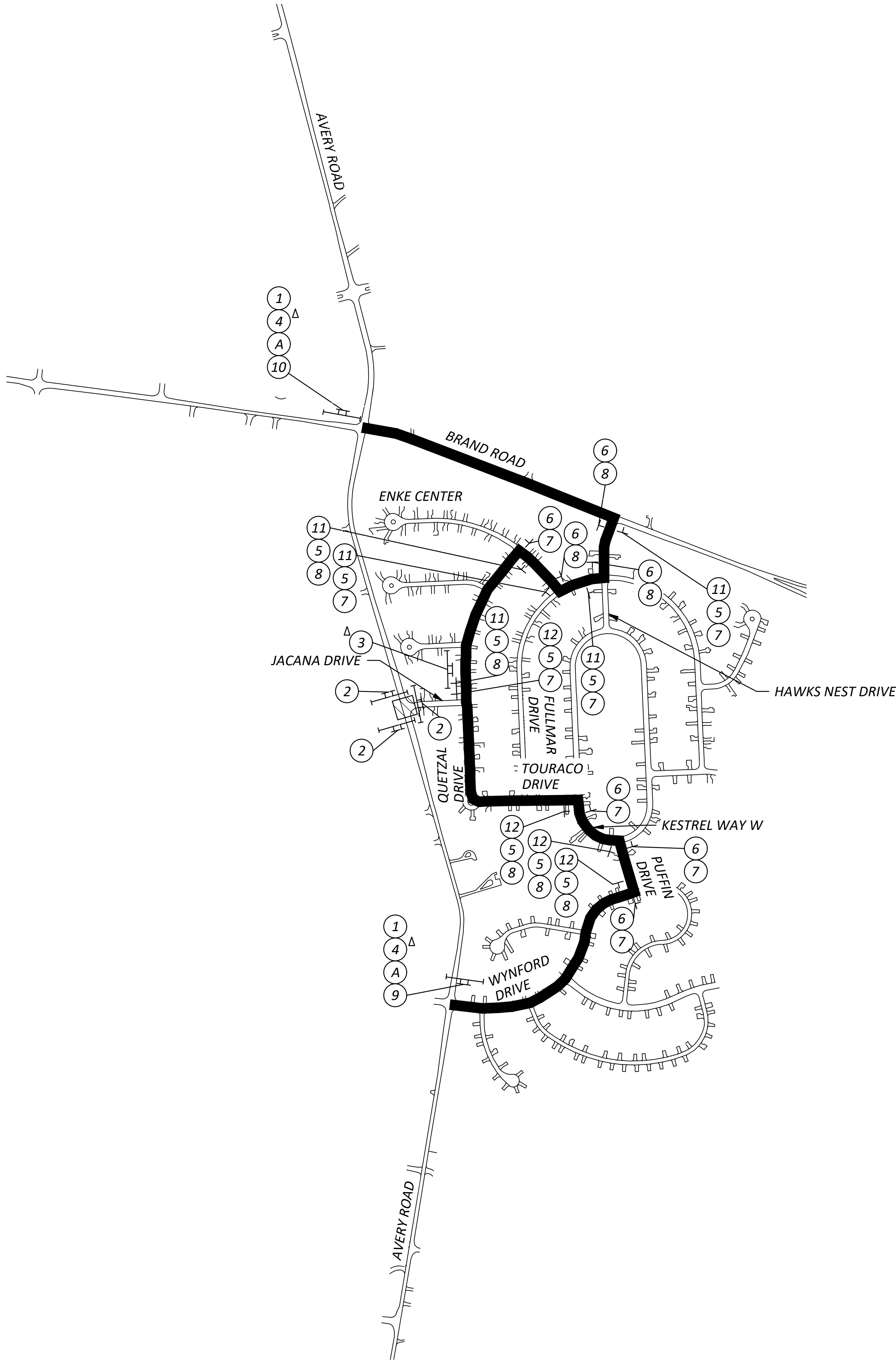


Δ BARRICADE SHALL BE PLACED ADJACENT TO ROAD.

DETOUR LEGEND	
	SIGN
	TYPE 3 BARRICADE
	WORK AREA
	WOERNER TEMPLE RD. DETOUR ROUTE



DESIGN AGENCY 	
DESIGNER SRT	
REVIEWER AKF 2/18/25	
PROJECT ID 25-002-CIP	
SHEET P.16	TOTAL 60



JACANA DR
CLOSED AT
AVERY RD
FOLLOW
DETOUR

SPECIAL

1

ROAD
CLOSED

R11-2-48
MOUNTED ON
TYPE 3 BARRICADE

2

ROAD CLOSED
LOCAL TRAFFIC ONLY

R11-3-66
MOUNTED ON
TYPE 3 BARRICADE

3

ROAD CLOSED
... MILES AHEAD
LOCAL TRAFFIC ONLY

R11-3a-60
MOUNTED ON
TYPE 3 BARRICADE

4

0.3
PLAQUE
A

AVERY RD
D3-1-30

5

JACANA DR
D3-1-30

6

DETOUR
←

M4-9L-30

7

DETOUR
→

M4-9R-30

8

DETOUR →

M4-10R-48

9

← DETOUR

M4-10L-48

10

NORTH

M3-1-24

11

SOUTH

M3-3-24

12

JACANA DR
CLOSED AT
AVERY RD
FOLLOW
DETOUR

4.6 21.28 5 6.52 4.6
20.53 5 5.91
5.28 17.34 5 6.52 5.28
6.57 10.27 21.46 10.27
10.67 20.66 10.67
42

2.25" Radius, 0.88" Border, 0.63" Indent, Black on Orange;

"JACANA DR", C;

"CLOSED AT", C;

"AVERY RD", C; "FOLLOW", C;

"DETOUR", C;

Table of letter and object lefts

J	A	C	A	N	A
4.60	7.94	11.91	15.21	19.18	22.76

D	R
30.88	34.67

C	L	O	S	E	D
5.28	8.86	12.21	15.94	19.73	23.07

A	T
30.81	34.22

A	V	E	R	Y
6.57	9.97	13.86	17.21	20.79

R	D
28.91	32.70

F	O	L	L	O	W
10.27	13.61	17.56	20.90	24.25	27.98

D	E	T	O	U	R
10.67	14.46	17.52	20.87	24.81	28.60

△ BARRICADE SHALL BE
PLACED ADJACENT TO ROAD.

DETOUR LEGEND

T	SIGN
TYPE 3 BARRICADE	
WORK AREA	
DETOUR ROUTE	

DETOUR PLAN - JACANA DRIVE

DESIGN AGENCY



DESIGNER

SRT

REVIEWER

AKF 2/18/25

PROJECT ID

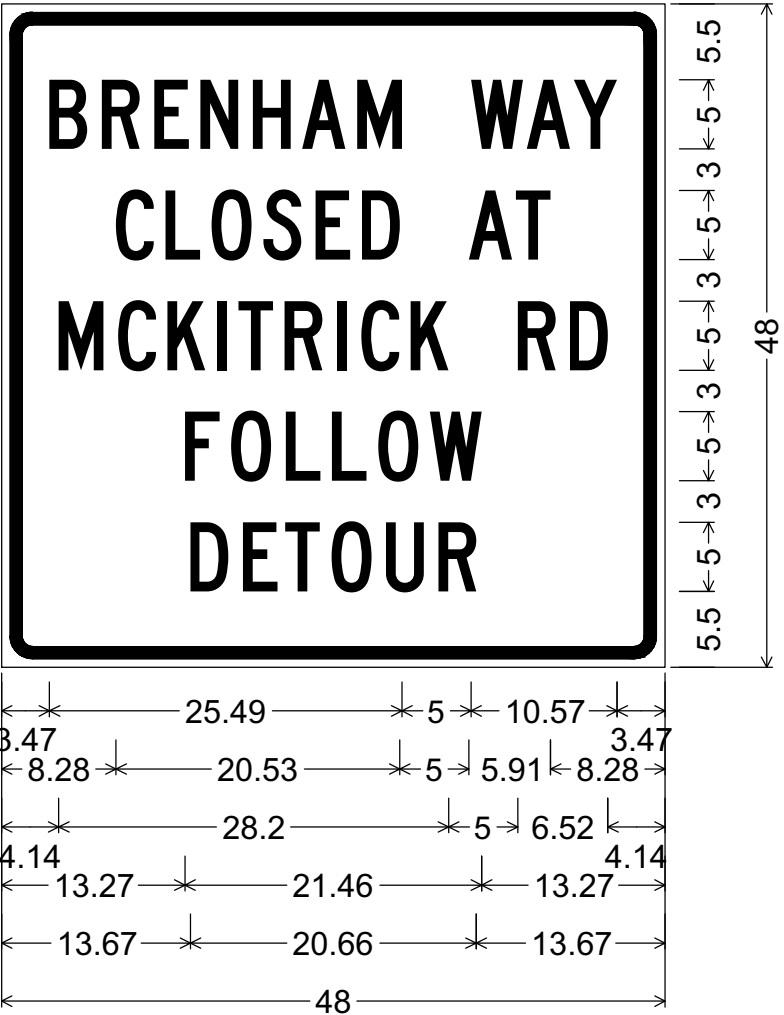
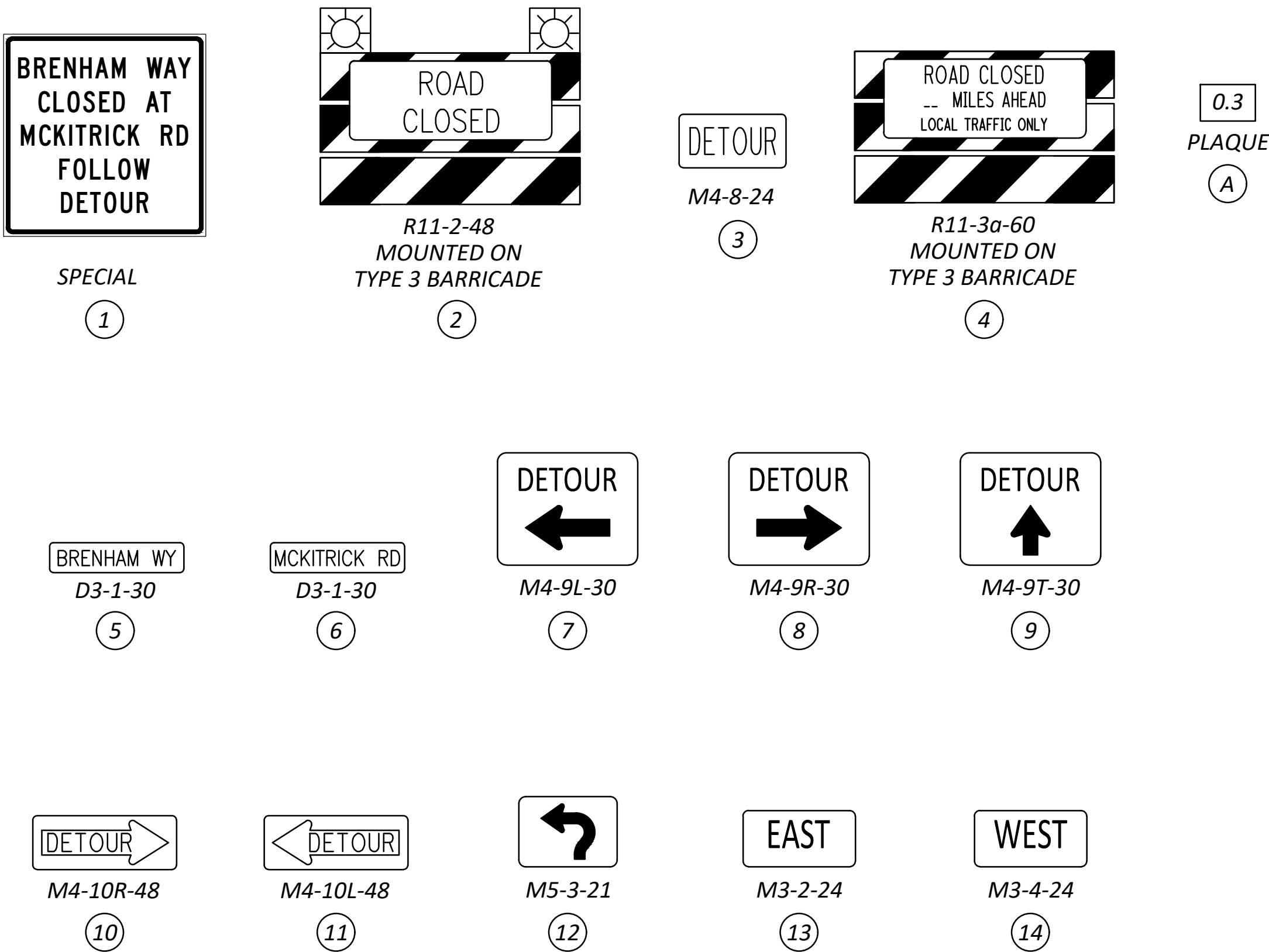
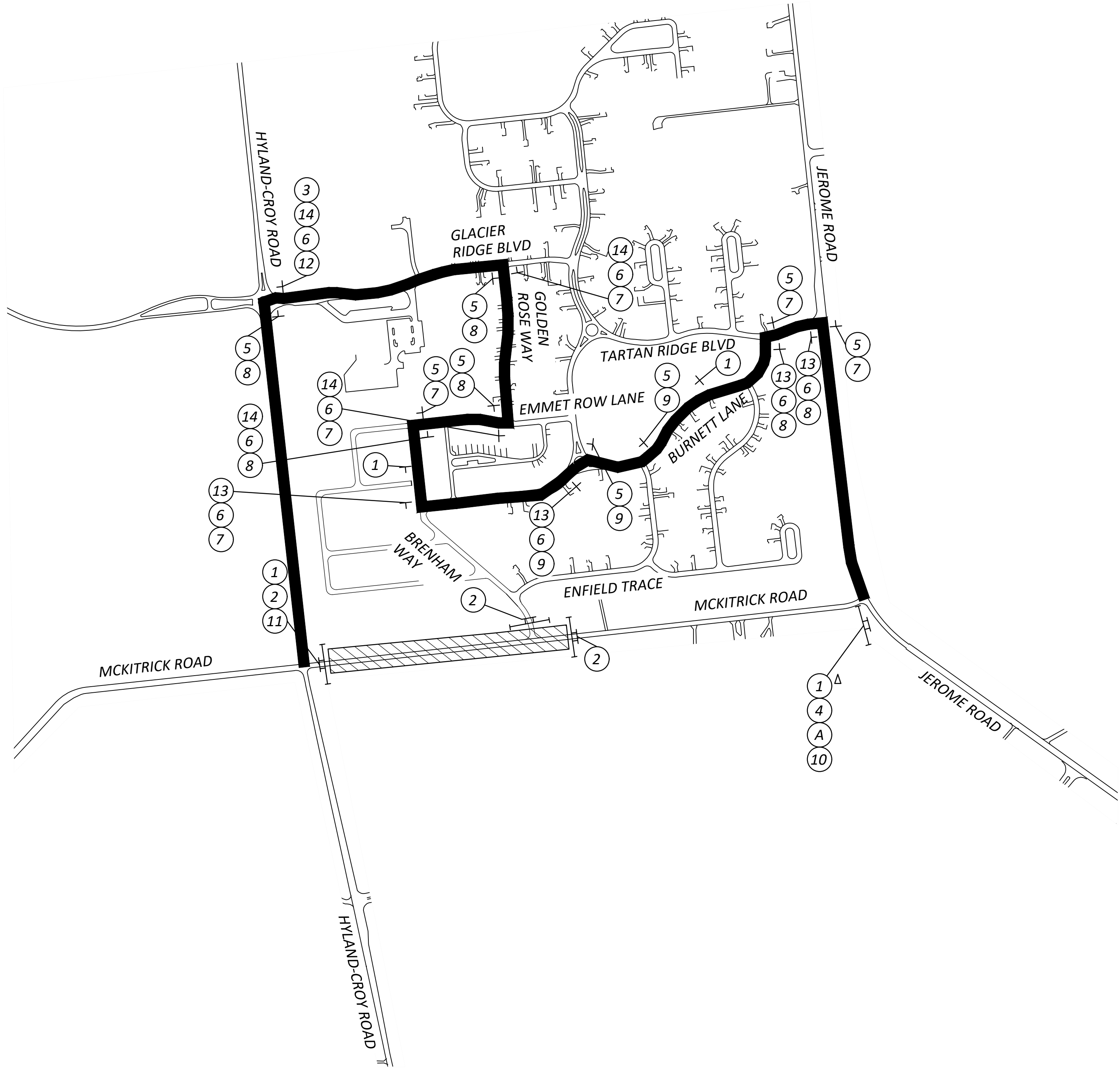
25-002-CIP

SHEET

P.17

TOTAL

60



2.25" Radius, 0.88" Border, 0.63" Indent, Black on Orange;
"BRENHAM WAY", C;
"CLOSED AT", C;
"MCKITRICK RD", C; "FOLLOW", C;
"DETOUR", C;
Table of letter and object lefts

B	R	E	N	H	A	M
3.47	7.26	11.04	14.39	18.17	21.75	25.72
	W	A	Y			
	33.96	38.00	41.41			
C	L	O	S	E	D	
8.28	11.86	15.21	18.94	22.73	26.07	
	A	T				
	33.81	37.22				
M	C	K	L	I	T	R
4.14	8.43	12.01	15.59	17.14	20.48	24.27
	R	D				
	37.34	41.13				
F	O	L	O	W		
13.27	16.61	20.56	23.90	27.25	30.98	
D	E	T	O	U	R	
13.67	17.46	20.52	23.87	27.81	31.60	

NOTE: SEE APPENDIX A FOR MCKITRICK ROAD DETOUR PLAN.

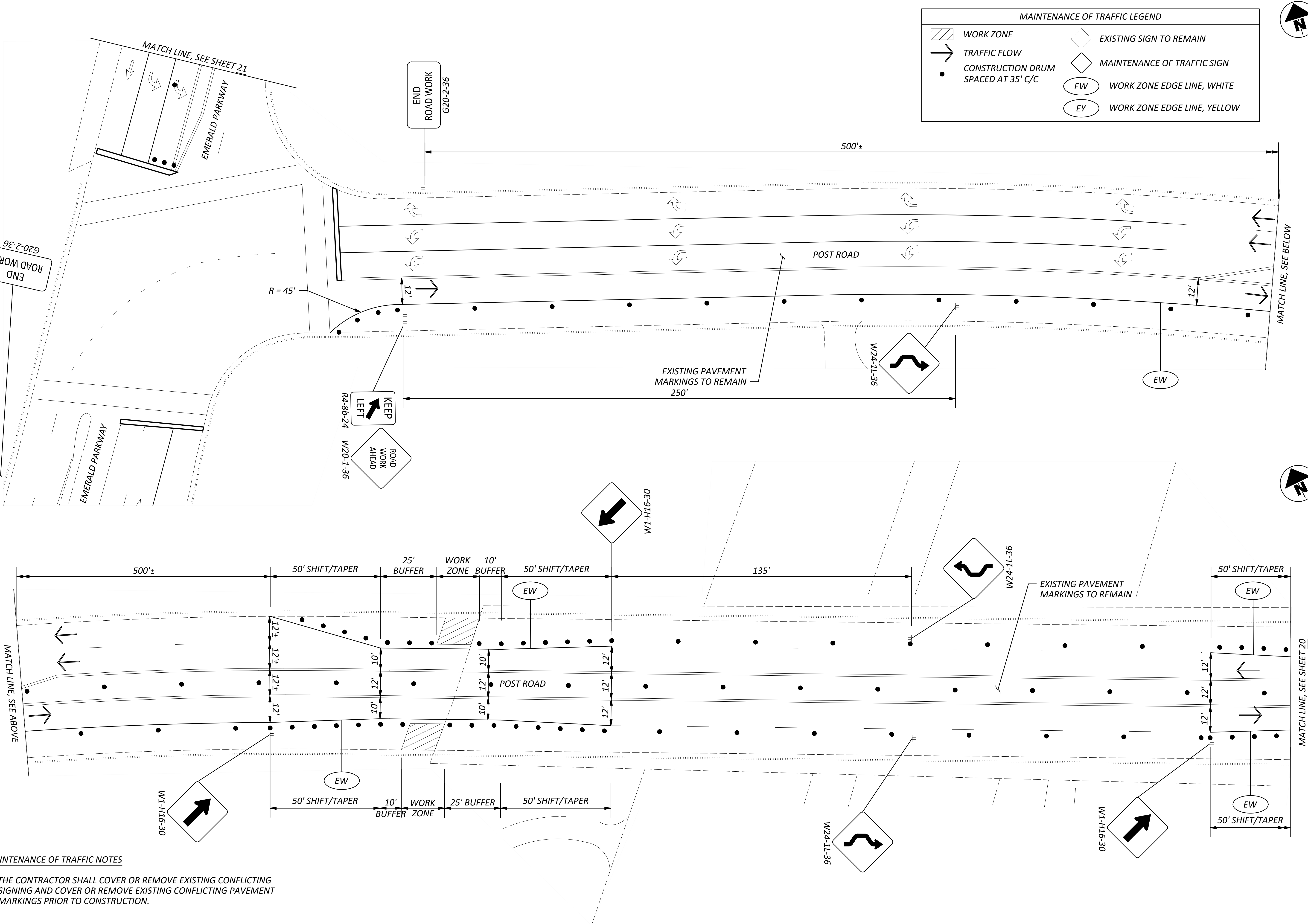
Δ BARRICADE SHALL BE PLACED ADJACENT TO ROAD.

DETOUR LEGEND	
	SIGN
	TYPE 3 BARRICADE
	WORK AREA
	DETOUR ROUTE

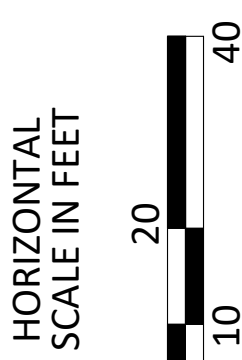


MAINTENANCE OF TRAFFIC NOTES

1. THE CONTRACTOR SHALL COVER OR REMOVE EXISTING CONFLICTING SIGNING AND COVER OR REMOVE EXISTING CONFLICTING PAVEMENT MARKINGS PRIOR TO CONSTRUCTION.



MAINTENANCE OF TRAFFIC LEGEND			
	WORK ZONE		EXISTING SIGN TO REMAIN
	TRAFFIC FLOW		MAINTENANCE OF TRAFFIC SIGN
	CONSTRUCTION DRUM SPACED AT 35' C/C		WORK ZONE EDGE LINE, WHITE
			WORK ZONE EDGE LINE, YELLOW



MAINTENANCE OF TRAFFIC PLAN - PHASE 1 - POST ROAD

DESIGN AGENCY



DESIGNER

SRT

REVIEWER

AKF 2/18/25

PROJECT ID

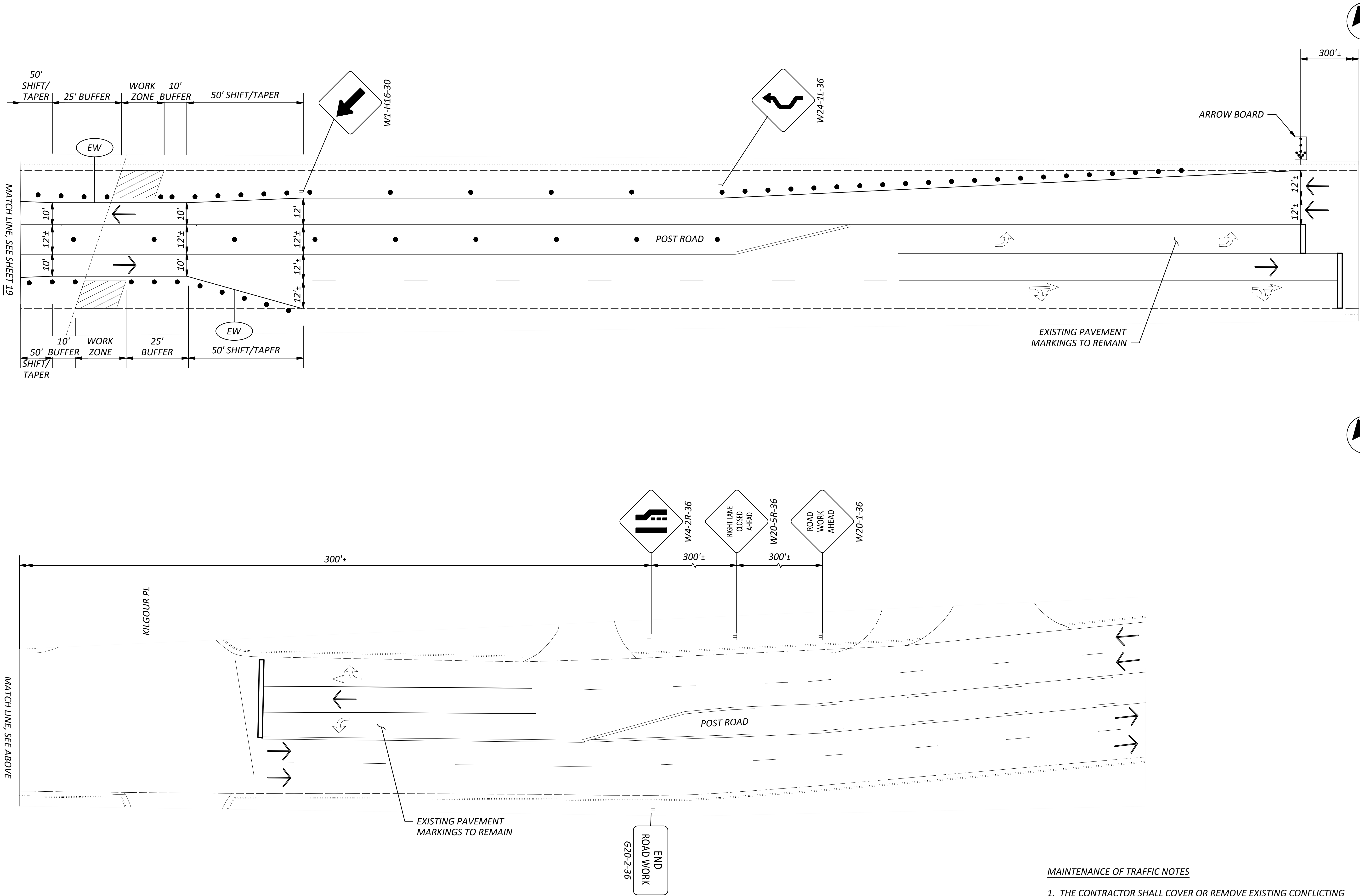
25-002-CIP

SHEET

P.19

TOTAL

60



MAINTENANCE OF TRAFFIC NOTES

1. THE CONTRACTOR SHALL COVER OR REMOVE EXISTING CONFLICTING SIGNING AND COVER OR REMOVE EXISTING CONFLICTING PAVEMENT MARKINGS PRIOR TO CONSTRUCTION.
2. FOR MAINTENANCE OF TRAFFIC LEGEND, SEE SHEET 19.

MAINTENANCE OF TRAFFIC PLAN - PHASE 1 - POST ROAD

DESIGN AGENCY

DESIGNER

SRT

REVIEWER

AKF 2/18/25

PROJECT ID

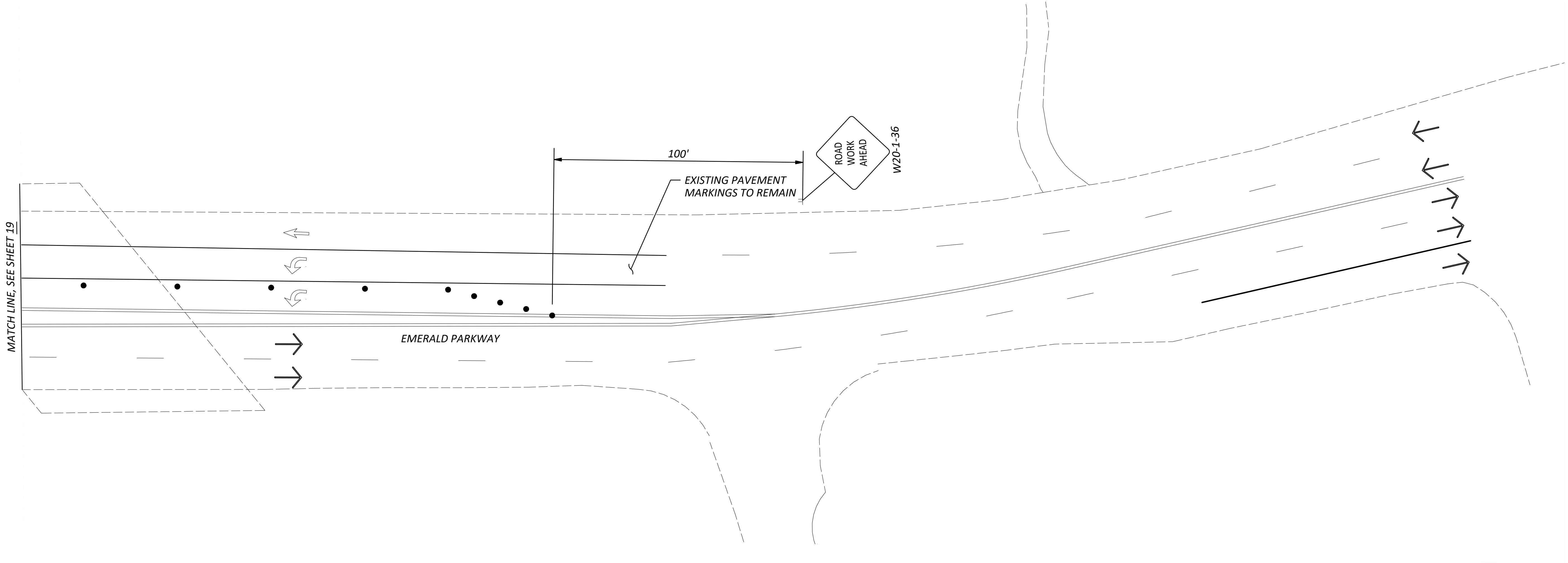
25-002-CIP

SHEET

P.20

TOTAL

60



MAINTENANCE OF TRAFFIC NOTES

1. THE CONTRACTOR SHALL COVER OR REMOVE EXISTING CONFLICTING SIGNING AND COVER OR REMOVE EXISTING CONFLICTING PAVEMENT MARKINGS PRIOR TO CONSTRUCTION.
2. FOR MAINTENANCE OF TRAFFIC LEGEND, SEE SHEET 19.



DESIGN AGENCY

DESIGNER

SRT

REVIEWER

LMO 2/18/25

PROJECT ID

25-002-CIP

SHEET

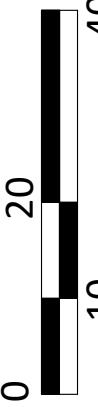
P.21

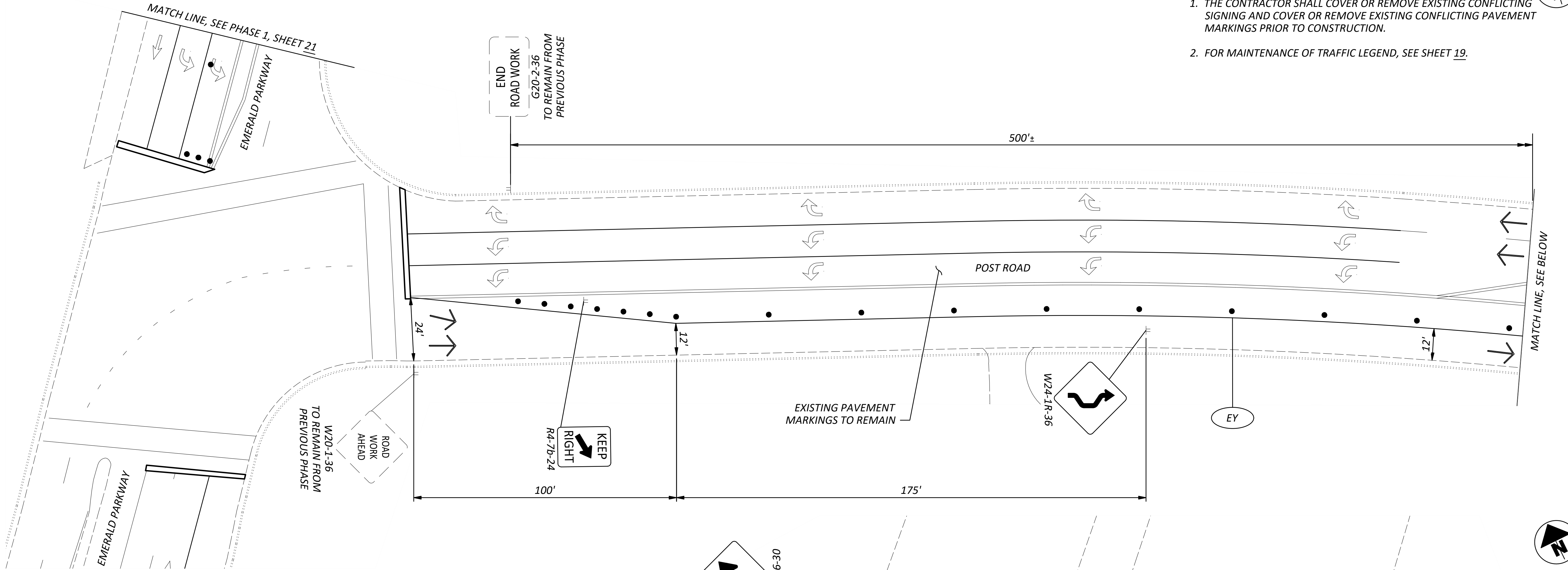
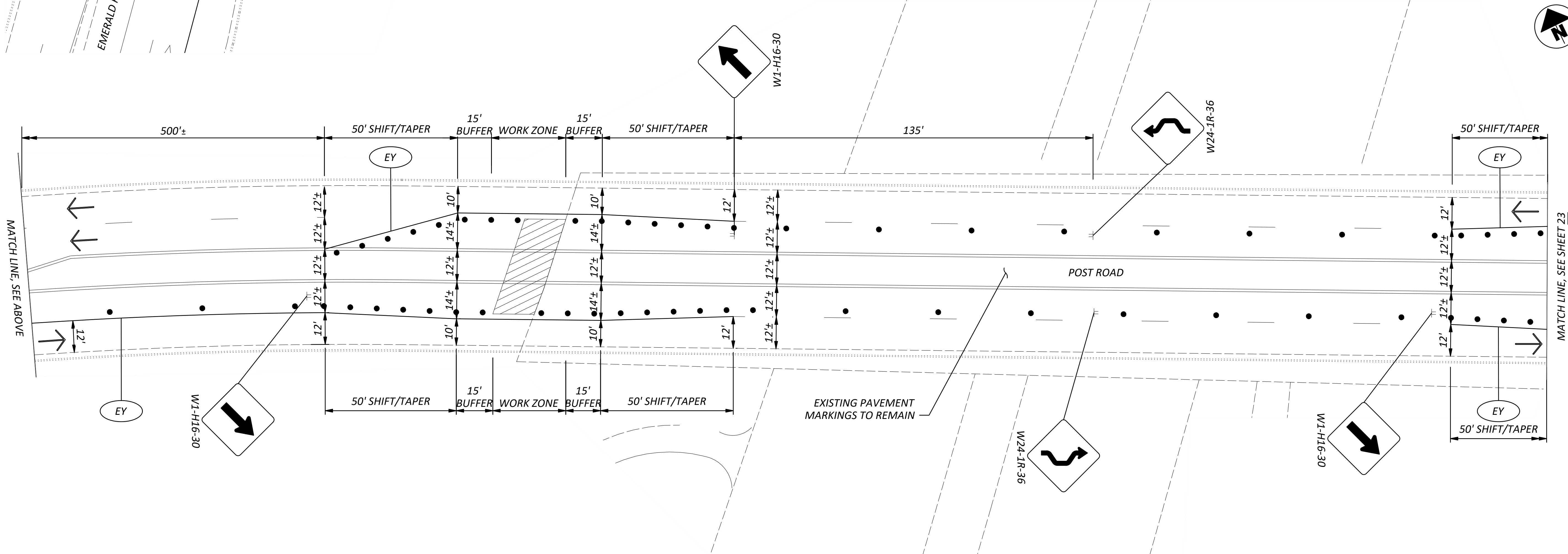
TOTAL

60

MAINTENANCE OF TRAFFIC PLAN - PHASE 1 - POST ROAD

HORIZONTAL
SCALE IN FEET



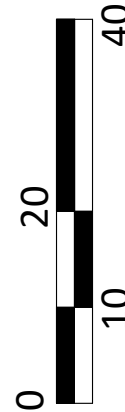


MAINTENANCE OF TRAFFIC NOTES

1. THE CONTRACTOR SHALL COVER OR REMOVE EXISTING CONFLICTING SIGNING AND COVER OR REMOVE EXISTING CONFLICTING PAVEMENT MARKINGS PRIOR TO CONSTRUCTION.
2. FOR MAINTENANCE OF TRAFFIC LEGEND, SEE SHEET 19.



HORIZONTAL
SCALE IN FEET



MAINTENANCE OF TRAFFIC PLAN - PHASE 2 - POST ROAD

DESIGN AGENCY



DESIGNER

SRT

REVIEWER

AKF 2/18/25

PROJECT ID

25-002-CIP

SHEET

P.22

TOTAL

60

O:\2024\2024085\02 Dublin 2025 Street Maintenance\400-Engineering\MOT\Sheets\2024085_MP402.dwg
9/18/2024



**HORIZONTAL
SCALE IN FEET**

10 20 40



GPD GROUP*
Glaus, Pyle, Schorner, Burns & Dehaven, LLP
Chicago, IL

DESIGNER	
SRT	
REVIEWER	
AKF	2/18/25
PROJECT ID	
25-002-CIP	
SHEET	TOTAL
P.23	60

2. FOR MAINTENANCE OF TRAFFIC LEGEND, SEE SHEET 19.

SHEET NUMBER														PARTICIPATION		ITEM	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	
4	9			29	30	31	32	34	37	38	40	41		ST25M06	ST25M08						
												96		96			202	96	SY	PAVEMENT REMOVED	
	625			1,588	4,928	1,658		1,460		150				10,409			202	10,409	FT	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	7
	50						171	1,498						1,719			202	1,719	FT	CURB REMOVED AND DISPOSED OF, AS PER PLAN	7
	500			1,437				2,267	229	218				4,651			202	4,651	SF	CONCRETE REMOVED AND DISPOSED OF, AS PER PLAN	7
	20							292		261	1,585			2,158			203	2,158	CY	EXCAVATION	
	100													100			203	100	CY	EXCAVATION, ROADWAY UNDERCUT, AS PER PLAN	4
	30													30			203	30	CY	EXCAVATION AND ROOT PRUNING, AS PER PLAN	4
								404						404			203	404	CY	EMBANKMENT	
140														140			204	140	CY	EXCAVATION OF SUBGRADE	
105														105			204	105	CY	GRANULAR EMBANKMENT, NO. 2 STONE	
35														35			204	35	CY	GRANULAR EMBANKMENT, NO. 304 STONE	
420														420			204	420	SY	GEOTEXTILE FABRIC, TYPE D	
								332		627	4,184			5,143			204	5,143	SY	SUBGRADE COMPACTION	
3														3			204	3	hour	PROOF ROLLING	
	250			1,375				1,081	93	64				2,863			608	2,863	SF	4" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	7
	250			486				1,761	136	154				2,787			608	2,787	SF	6" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	7
	2			10				25	2	3				42			608	42	EACH	CURB RAMPS	
	16			112				382	32	30				572			608	572	SF	DETECTABLE WARNING, TYPE A, AS PER PLAN	6
	50							335						385			608	385	SF	BRICK PAVER REMOVE & RESET, TYPE 2, AS PER PLAN	6
	400													400			653	400	CY	TOPSOIL FURNISHED AND PLACED, AS PER PLAN	5
	5													5			SPECIAL	5	EACH	MAILBOX REPLACED	
	500			180				1,130	1,841		1,081			4,732			659	4,732	SY	SEEDING AND MULCHING, AS PER PLAN	5
	100													100			659	100	MGAL	WATER FOR IRRIGATION, AS PER PLAN	5
	1.00													1.00			659	1.00	TON	COMMERCIAL FERTILIZER	
											10,000			10,000			832*	10,000	EACH	EROSION CONTROL	
	100													100			SPECIAL	100	CY	HARDWOOD MULCH	5
	5			1				3						9			604	9	EACH	MANHOLE, CATCH BASIN, OR INLET ADJUSTED TO GRADE, AS PER PLAN	6
	5													5			604	5	EACH	MANHOLE, CATCH BASIN, OR INLET RECONSTRUCTED TO GRADE, AS PER PLAN	6
								1						1			611	1	EACH	INLET NO. 2-A-10, AS PER PLAN	6
				1	1		2	13						17			SPECIAL	17	EACH	INLET REPAIR AND PARGE	6
	300													300			SPECIAL	300	FT	DRAIN LEADER PIPE	6

[illegible]

SHEET NUMBER														PARTICIPATION		ITEM	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	
5	9			29	30	32	34	37	38	40		58	59	ST25M06	ST25M08						
														2	2	625	2	EACH	CONNECTOR KIT, TYPE II, FUSED		
														4	4	625	4	EACH	CONNECTOR KIT, TYPE III, UNFUSED		
														2	2	625	2	EACH	LIGHT POLE, 35'-0", GENERAL STREET LIGHT	58	
														2	2	625	2	EACH	TRANSFORMER BASE		
														2	2	625	2	EACH	LIGHT POLE FOUNDATION, 24" X 7', AS PER PLAN	58	
														2,679	2,679	625	2,679	FT	NO. 6 AWG 2400 VOLT DISTRIBUTION CABLE		
														240	240	625	240	FT	NO. 12 AWG POLE AND BRACKET CABLE		
														816	816	625	816	FT	CONDUIT, 2", 725.051		
														63	63	625	63	FT	CONDUIT SLEEVE UNDER PAVEMENT, 3" SLEEVE, 725.051 (SCH 80)		
														2	2	625	2	EACH	LUMINAIRE: 80 LED, TYPE III MEDIUM, 480 V	58	
														740	740	625	740	FT	TRENCH, AS PER PLAN	58	
														13	13	625	13	FT	TRENCH IN PAVED AREA, AS PER PLAN	58	
														5	5	625	5	EACH	PULL BOX, 725.06 (18" X 11" X 18")		
														2	2	625	2	EACH	GROUND ROD, AS PER PLAN	58	
														816	816	625	816	FT	UNDERGROUND WARNING/MARKING TAPE, AS PER PLAN	58	
													1		1	625*	1	EACH	LIGHTING, MISC.: CONTROL CENTER MODIFICATIONS	58	
													1		1	632*	1	EACH	TEST HOLE PERFORMED	58	
							135.5							135.5		630*	135.5	FT	GROUND MOUNTED SUPPORT, NO. 3 POST, AS PER PLAN	5	
							63.3							63.3		630*	63.3	SF	SIGN, FLAT SHEET		
							5							5		630*	5	EACH	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL		
				3										3		630*	3	EACH	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION		
				3										3		630*	3	EACH	REMOVAL OF GROUND MOUNTED POST SUPPORT AND REERECTION		
							4							4		630*	4	EACH	REMOVAL OF GROUND MOUNTED SIGN AND STORAGE, AS PER PLAN	5	
				3			8							11		630*	11	EACH	SIGNING, MISC.: 2 1/4" SQUARE ANCHOR POST	5	
						10								10		630*	10	EACH	SIGNING, MISC.: REMOVAL AND DISPOSAL OF REBOUNDABLE DELINEATOR POST ASSEMBLY	5	
						10								10		630*	10	EACH	SIGNING, MISC.: REBOUNDABLE DELINEATOR POST ASSEMBLY	5	
						0.13	2.27	1.58	0.03	0.56				4.57		644*	4.57	MILE	EDGE LINE, 4"		
0.26							1.75							2.01		644*	2.01	MILE	LANE LINE, 4"		
0.02						0.31	0.02	0.94	0.03	0.41				1.73		644*	1.73	MILE	CENTER LINE		
						572	1,996	120	22	125				2,835		644*	2,835	FT	CHANNELIZING LINE, 8"		
	100			41	26	67	199		15	12				460		644*	460	FT	STOP LINE		
	200			292	135	118	1,826		175					2,746		644*	2,746	FT	CROSSWALK LINE, 12"		
							78							78		644*	78	FT	CHEVRON MARKING		
								70	57	57				184		644*	184	SF	ISLAND MARKING		
						8	33	4		3				48		644*	48	EACH	LANE ARROW		
						113								113		644*	113	FT	CURB MARKING		
						56								56		644*	56	FT	PARKING LOT STALL MARKING		
						160	141	450		204				955		644*	955	FT	TRANSVERSE/DIAGONAL LINE		
						100	288							388		644*	388	FT	DOTTED LINE, 4"		
						1								1		644*	1	EACH	HANDICAP SYMBOL MARKING		
0.05														0.05		645	0.05	MILE	LANE LINE, 4", TYPE A1, WITH CONTRAST		
														</							

[illegible]

SHEET NUMBER													PARTICIPATION		ITEM	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
													ST25M06	ST25M08					
											44	45							
																		ADDITIVE ALTERNATE 1: COSGRAY ROAD	
											20			20	202	20	FT	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	7
											10			10	202	10	FT	CURB REMOVED AND DISPOSED OF, AS PER PLAN	7
											10			10	251	10	SY	PARTIAL DEPTH PAVEMENT REPAIR (441)	
											8,712			8,712	254	8,712	SY	PAVEMENT PLANING, ASPHALT CONCRETE (2")	
											871			871	407	871	GAL	NON-TRACKING TACK COAT	
											968			968	441	968	TON	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	
											136			136	608	136	SF	4" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	7
											25			25	608	25	SF	BRICK PAVER REMOVE & RESET, TYPE 2, AS PER PLAN	6
											20			20	609	20	FT	COMBINATION CURB AND GUTTER, 8" THICK, AS PER PLAN	7
											10			10	609	10	FT	STRAIGHT 18" CURB, AS PER PLAN	7
											100			100	617*	100	CY	RECONDITION SHOULDER, 4" COMPACTED AGGREGATE, AS PER PLAN	7
											37			37	617*	37	CY	RECONDITION SHOULDER, 1.5" ITEM 653 PULVERIZED TOPSOIL, AS PER PLAN	7
											1.02			1.02	644*	1.02	MILE	EDGE LINE, 4"	
											0.51			0.51	644*	0.51	MILE	CENTER LINE	
											376			376	644*	376	FT	CHANNELIZING LINE, 8"	
											352			352	644*	352	FT	CROSSWALK LINE, 24"	
											2			2	644*	2	EACH	LANE ARROW	
											110			110	644*	110	FT	TRANSVERSE/DIAGONAL LINE	
											79			79	644*	79	FT	YIELD LINE	
											152			152	644*	152	FT	PAVEMENT MARKING, MISC.: YIELD BAR, WHITE, 24", (2' LONG, 2' GAP)	5
											897			897	659	897	SY	SEEDING AND MULCHING, AS PER PLAN	5
											3,638			3,638	SPECIAL	3,638	SY	ASPHALT REJUVENATING AGENT	8
																		ADDITIVE ALTERNATE 2: LIGGETT ROAD	
											13			13	251	13	SY	PARTIAL DEPTH PAVEMENT REPAIR (441)	
											25			25	253	25	SY	PAVEMENT REPAIR	
											3389			3,389	254	3,389	SY	PAVEMENT PLANING, ASPHALT CONCRETE (1.5")	
											1868			1,868	254	1,868	SY	PAVEMENT PLANING, ASPHALT CONCRETE (5")	
											666			666	407	666	GAL	NON-TRACKING TACK COAT	
											438			438	441	438	TON	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	
											363			363	441	363	TON	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	
											116			116	617*	116	CY	RECONDITION SHOULDER, 4" COMPACTED AGGREGATE, AS PER PLAN	7
											43			43	617*	43	CY	RECONDITION SHOULDER, 1.5" ITEM 653 PULVERIZED TOPSOIL, AS PER PLAN	7
											0.37			0.37	644*	0.37	MILE	EDGE LINE, 4"	
											0.19			0.19	644*	0.19	MILE	CENTER LINE	
											22			22	644*	22	FT	STOP LINE	
											1,041			1,041	659	1,041	SY	SEEDING AND MULCHING, AS PER PLAN	5
											5,257			5,257	SPECIAL	5,257	SY	ASPHALT REJUVENATING AGENT	8
																		* DENOTES ODOT CONSTRUCTION AND MATERIALS SPECIFICATIONS (2023 EDITION); ALL OTHER ITEMS REFERENCE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS (2018 EDITION)	

GENERAL SUMMARY

DESIGN AGENCY

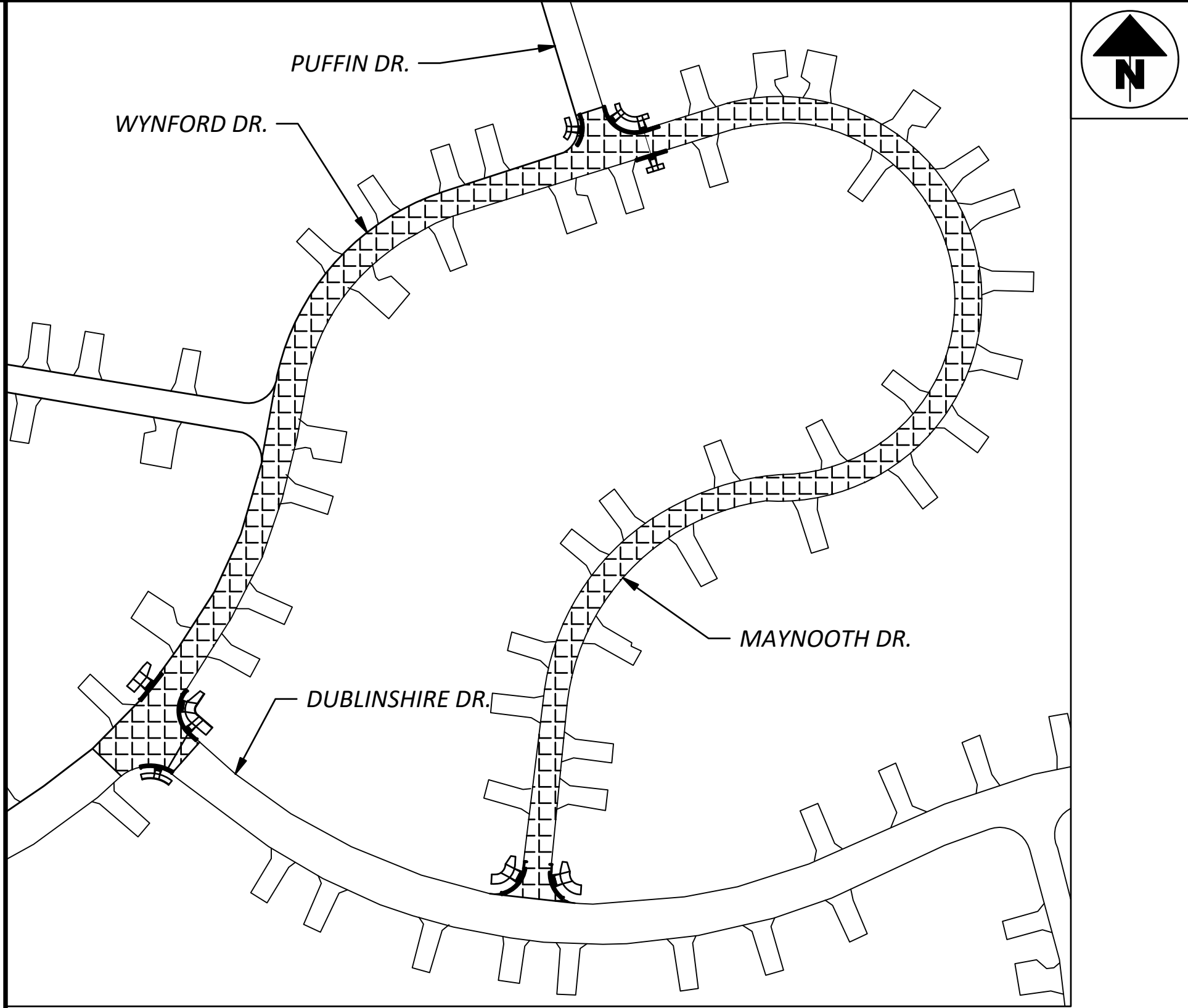


DESIGNER
MSS

REVIEWER
LMO 2/18/25

PROJECT ID
25-002-CIP

SHEET TOTAL
P.28 60

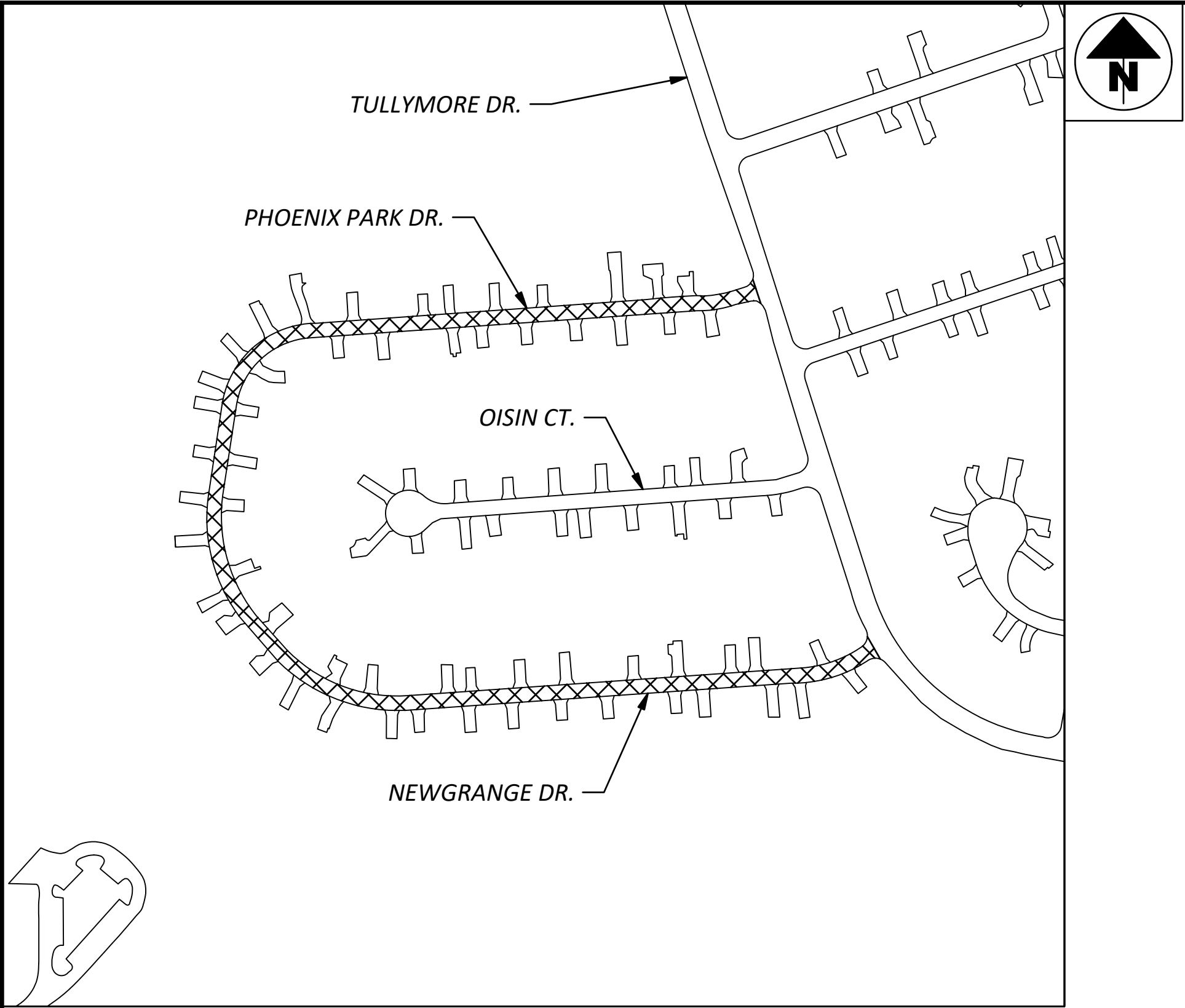


3" MILL AND FILL:
ITEM 254 - 3" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 1.75" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
ITEM 407 - NON-TRACKING TACK COAT, 0.075 GAL/SY
ITEM 441 - 1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT

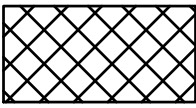
NOTE:
1. FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/dsf6erwm)).
2. FOR CURB RAMP DETAILS, SEE SHEET 46.

ESTIMATED QUANTITIES WYNFORD DR.			
(3" MILL AND FILL - DUBLINSHIRE DR. TO MAYNOOTH DR.)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	FT	641
202	CONCRETE REMOVED AND DISPOSED OF, AS PER PLAN	SF	1,006
254	PAVEMENT PLANING, ASPHALT CONCRETE (3")	SY	2,308
407	NON-TRACKING TACK COAT	GAL	404
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	160
441	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	TON	224
608	4" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	906
608	6" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	345
608	CURB RAMPS	EACH	8
608	DETECTABLE WARNING, TYPE A, AS PER PLAN	SF	80
609	COMBINATION CURB AND GUTTER, 6" THICK, AS PER PLAN	FT	641
630	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION	EACH	2
630	REMOVAL OF GROUND MOUNTED POST SUPPORT AND REERECTION	EACH	2
630	SIGNING, MISC.: 2 1/4" SQUARE ANCHOR POST	EACH	2
644	STOP LINE	FT	27
644	CROSSWALK LINE, 12"	FT	208
659	SEEDING AND MULCHING, AS PER PLAN	SY	130
SPECIAL	CURB CAN HOLES	EACH	15
SPECIAL	ASPHALT REJUVENATING AGENT	SY	2,308
SPECIAL	CHEEK WALL	FT	62

ESTIMATED QUANTITIES MAYNOOTH DR.			
(3" MILL AND FILL)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	FT	947
202	CONCRETE REMOVED AND DISPOSED OF, AS PER PLAN	SF	431
254	PAVEMENT PLANING, ASPHALT CONCRETE (3")	SY	2,642
407	NON-TRACKING TACK COAT	GAL	462
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	183
441	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	TON	257
604	MANHOLE, CATCH BASIN, OR INLET ADJUSTED TO GRADE, AS PER PLAN	EACH	1
608	4" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	469
608	6" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	141
608	CURB RAMPS	EACH	2
608	DETECTABLE WARNING, TYPE A, AS PER PLAN	SF	32
609	COMBINATION CURB AND GUTTER, 6" THICK, AS PER PLAN	FT	947
630	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION	EACH	1
630	REMOVAL OF GROUND MOUNTED POST SUPPORT AND REERECTION	EACH	1
630	SIGNING, MISC.: 2 1/4" SQUARE ANCHOR POST	EACH	1
644	CROSSWALK LINE, 12"	FT	84
644	STOP LINE	FT	14
659	SEEDING AND MULCHING, AS PER PLAN	SY	50
SPECIAL	INLET REPAIR AND PARGE	EACH	1
SPECIAL	CURB CAN HOLES	EACH	30
SPECIAL	ASPHALT REJUVENATING AGENT	SY	2,642



LOCATION MAP: NTS
PHOENIX PARK DR.
NEWGRANGE DR.

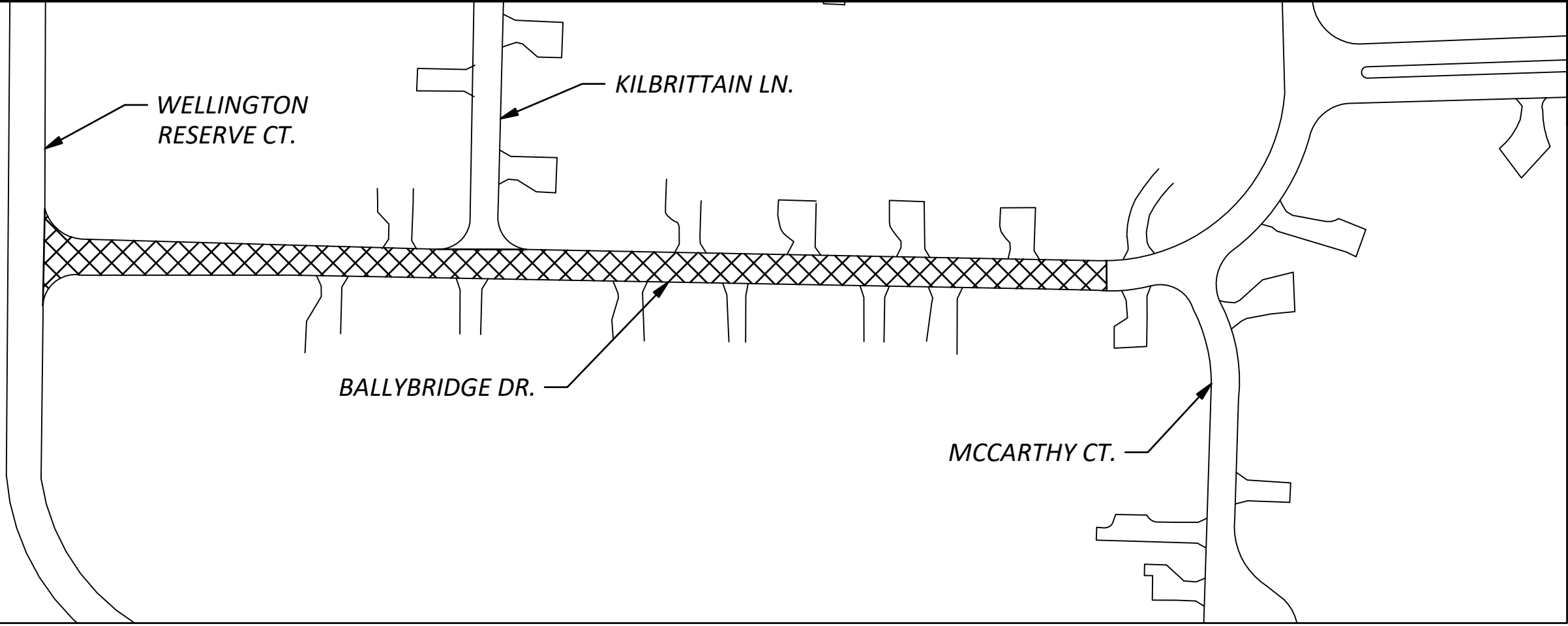


1.5" MILL AND FILL:
ITEM 254 - 1.5" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT

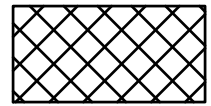
- NOTE:
- FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/dsf6erwm)).
 - ADD A STOP LINE ON NEWGRANGE DRIVE AT THE TULLYMORE DRIVE INTERSECTION.

ESTIMATED QUANTITIES PHOENIX PARK DR.			
(1.5" MILL AND FILL)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	FT	1,809
254	PAVEMENT PLANING, ASPHALT CONCRETE (1.5")	SY	2,453
407	NON-TRACKING TACK COAT	GAL	245
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	204
609	COMBINATION CURB AND GUTTER, 6" THICK, AS PER PLAN	FT	1,809
644	STOP LINE	FT	15
644	CROSSWALK LINE, 12"	FT	72
SPECIAL	CURB CAN HOLES	EACH	42
SPECIAL	ASPHALT REJUVENATING AGENT	SY	2,453

ESTIMATED QUANTITIES NEWGRANGE DR.			
(1.5" MILL AND FILL)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	FT	3,119
254	PAVEMENT PLANING, ASPHALT CONCRETE (1.5")	SY	4,236
407	NON-TRACKING TACK COAT	GAL	424
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	353
609	COMBINATION CURB AND GUTTER, 6" THICK, AS PER PLAN	FT	3,119
644	STOP LINE	FT	11
644	CROSSWALK LINE, 12"	FT	63
SPECIAL	INLET REPAIR AND PARGE	EACH	1
SPECIAL	CURB CAN HOLES	EACH	74
SPECIAL	ASPHALT REJUVENATING AGENT	SY	4,236



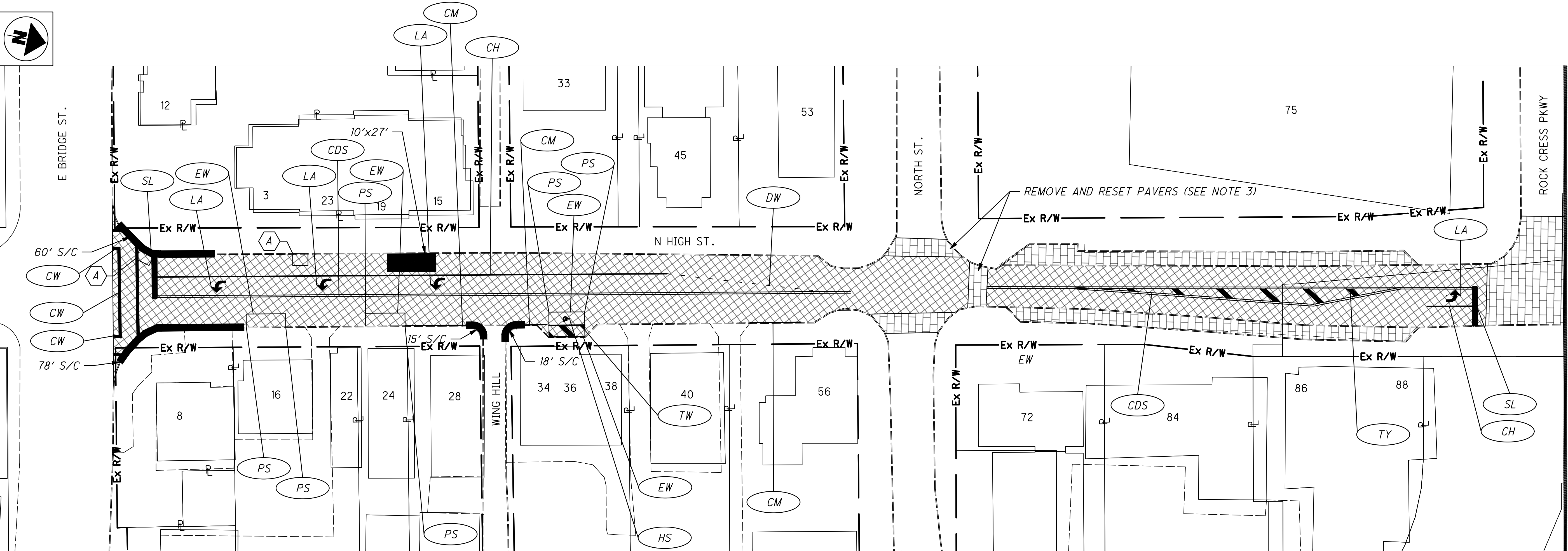
LOCATION MAP: NTS
BALLYBRIDGE DR.



1.5" MILL AND FILL:
ITEM 254 - 1.5" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT

NOTE:
1. FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/dsf6erwm)).

ESTIMATED QUANTITIES BALLYBRIDGE DR.			
(1.5" MILL AND FILL - WELLINGTON RESERVE CT. TO MCCARTHY CT.)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	FT	1,658
254	PAVEMENT PLANING, ASPHALT CONCRETE (1.5")	SY	2,330
407	NON-TRACKING TACK COAT	GAL	233
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	194
609	COMBINATION CURB AND GUTTER, 6" THICK, AS PER PLAN	FT	1,658
SPECIAL	CURB CAN HOLES	EACH	28
SPECIAL	ASPHALT REJUVENATING AGENT	SY	2,330



ESTIMATED QUANTITIES N. HIGH ST.			
(1.5" MILL AND FILL - E. BRIDGE ST. TO RIVERVIEW ST.)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB REMOVED AND DISPOSED OF, AS PER PLAN	FT	171
251	PARTIAL DEPTH PAVEMENT REPAIR (441)	SY	30
254	PAVEMENT PLANING, ASPHALT CONCRETE (1.5")	SY	5,476
407	NON-TRACKING TACK COAT	GAL	548
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	456
609	STRAIGHT 18" CURB, AS PER PLAN	FT	171
630	SIGNING, MISC.: REMOVAL AND DISPOSAL OF REBOUNDALE DELINEATOR POST ASSEMBLY	EACH	10
630	SIGNING, MISC.: REBOUNDALE DELINEATOR POST ASSEMBLY	EACH	10
644	EDGE LINE, 4", WHITE	MILE	0.05
644	EDGE LINE, 4", YELLOW	MILE	0.08
644	CENTER LINE	MILE	0.31
644	CHANNELIZING LINE, 8"	FT	572
644	STOP LINE	FT	67
644	CROSSWALK LINE, 12"	FT	118
644	CURB MARKING	FT	113
644	LANE ARROW	EACH	8
644	PARKING LOT STALL MARKING	FT	56
644	TRANSVERSE/DIAGONAL LINE	FT	160
644	DOTTED LINE, 4", WHITE	FT	100
644	HANDICAP SYMBOL MARKING	EACH	1
SPECIAL	INLET REPAIR AND PARGE	EACH	2
SPECIAL	ASPHALT REJUVENATING AGENT	SY	5,476
SPECIAL	PAVERS, REMOVED AND RESET	SF	588
SPECIAL	BRICK PAVERS	SF	588

PAVEMENT MARKING LEGEND	
●	DELINEATOR POST
EW	EDGE LINE, WHITE
EY	EDGE LINE, YELLOW
CDS	CENTER LINE, DOUBLE SOLID
CH	CHANNELIZING LINE
SL	STOP LINE
CW	CROSSWALK LINE
TW	TRANSVERSE/DIAGONAL LINE, WHITE
TY	TRANSVERSE/DIAGONAL LINE, YELLOW
LA	LANE ARROW
DW	DOTTED LINE, WHITE
PS	PARKING LOT STALL MARKING
CM	CURB MARKING, YELLOW
HS	HANDICAP SYMBOL MARKING

1.5" MILL AND FILL:
ITEM 254 - 1.5" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)

EX. BRICK PAVERS WITH GRANITE BANDS (DO NOT DISTURB EXCEPT WHERE NOTED IN THE PLANS)

REMOVE AND REPLACE EXISTING CURB

EXISTING CURB

XX' S/C

FEET OF PROPOSED STRAIGHT CURB

A

ITEM SPECIAL - INLET REPAIR AND PARGE

NOTE:

- FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/DSF6ERWM)).
- CONTRACTOR SHALL NOTIFY THE CITY IN WRITING A MINIMUM OF 14 DAYS PRIOR TO WORKING WITHIN THE VICINITY OF SENSORS TO COORDINATE THEIR REMOVAL BY THE CITY PRIOR TO MILLING OPERATIONS. IF THE CONTRACTOR FAILS TO PROVIDE ADEQUATE NOTICE TO THE CITY, WITH OPERATIONS RESULTING IN ANY DAMAGE TO SENSORS, CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR THE REPAIR AND/OR REPLACEMENT AT THE DISCRETION OF THE CITY ENGINEER.
- THE CONTRACTOR SHALL REMOVE AND RESET PAVERS AS DESCRIBED IN THE NOTE FOR ITEM SPECIAL - PAVERS, REMOVED AND RESET ON SHEET 9 AND ACCORDING TO THE PAVER DETAIL ON SHEET 33. DO NOT DISTURB EXISTING GRANITE BANDS. RESET PAVERS SHALL MATCH THE EXISTING PAVER LAYOUT PATTERN.

CONTACT: MARC DILSAVER, TRANSPORTATION PROGRAM MANAGER,
614.410.4744, MDILSAVER@DUBLIN.OH.US

HORIZONTAL
SCALE IN FEET

0

15

30

60

2025 DUBLIN STREET MAINTENANCE PHASE 2

1/3/2025 O:\2024\2024085\02 Dublin 2025 Street Maintenance\400-Engineering\Roadway\Phase 2\Sheets\2024085_GA200.dwg

DESIGN AGENCY

DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

25-002-CIP

SHEET

P.32

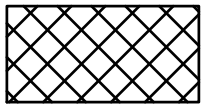
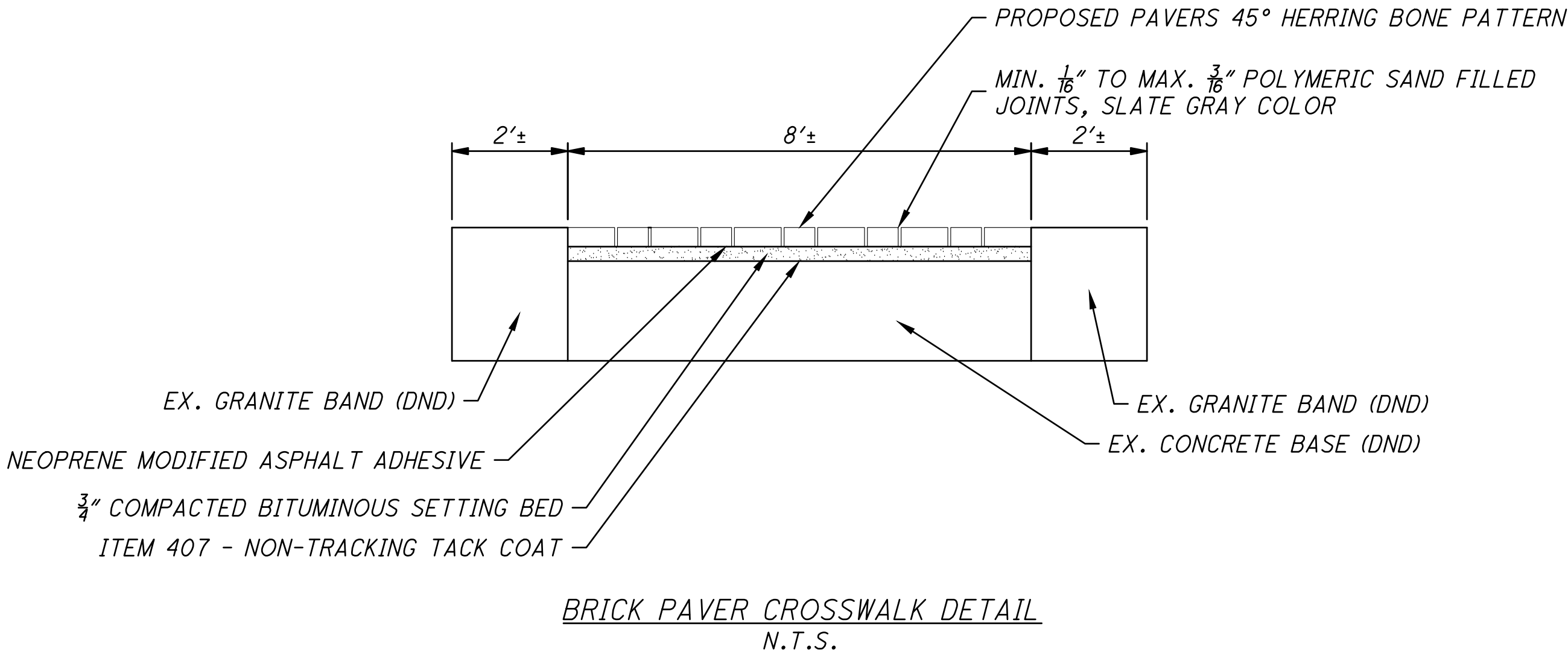
TOTAL

60

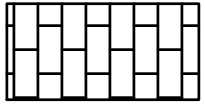
N. HIGH ST. IMPROVEMENTS

MATCH LINE - SEE SHEET 33

PAVEMENT MARKING LEGEND	
●	DELINEATOR POST
EW	EDGE LINE, WHITE
EY	EDGE LINE, YELLOW
CDS	CENTER LINE, DOUBLE SOLID
CH	CHANNELIZING LINE
SL	STOP LINE
CW	CROSSWALK LINE
TW	TRANSVERSE/DIAGONAL LINE, WHITE
TY	TRANSVERSE/DIAGONAL LINE, YELLOW
LA	LANE ARROW
DW	DOTTED LINE, WHITE
PS	PARKING LOT STALL MARKING
CM	CURB MARKING, YELLOW



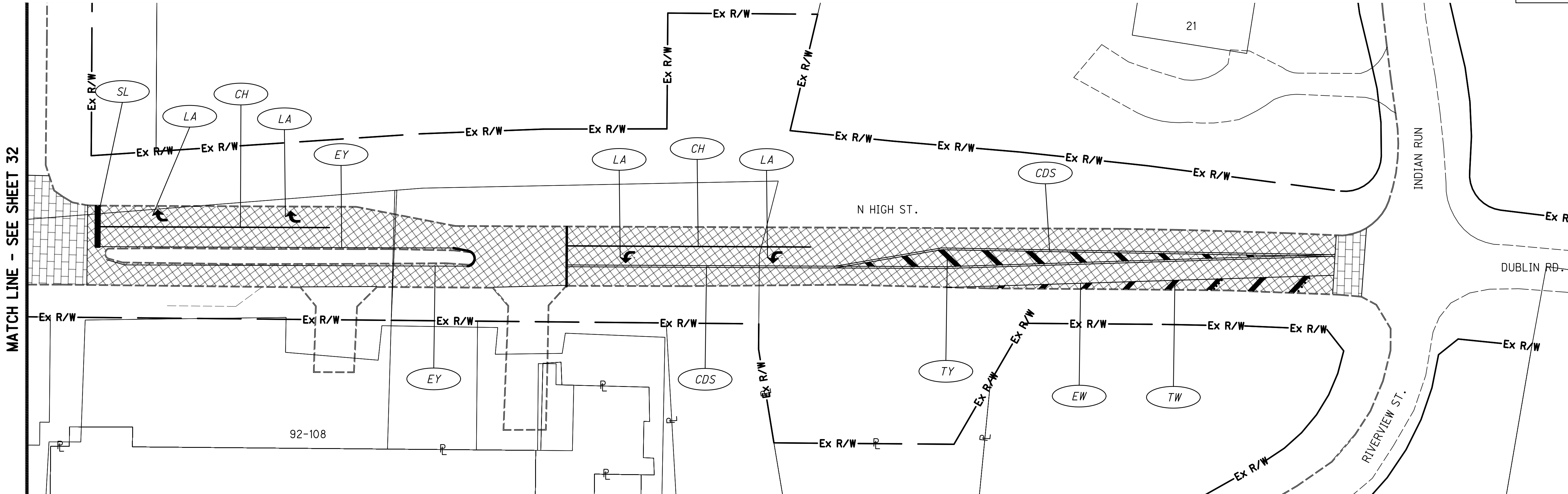
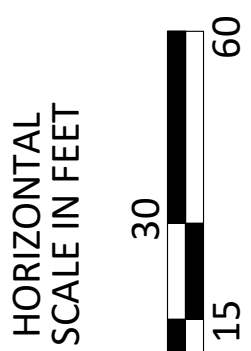
1.5" MILL AND FILL:
ITEM 254 - 1.5" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT



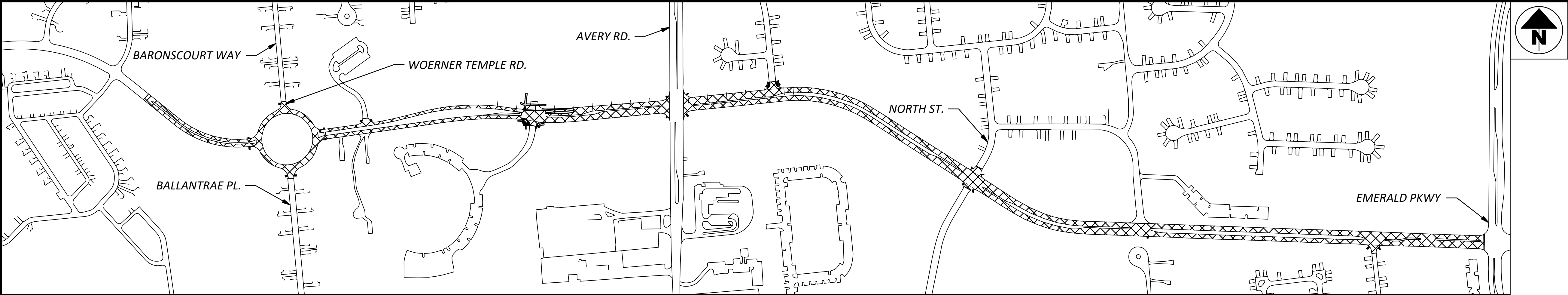
EX. BRICK PAVERS WITH GRANITE BANDS (DO NOT DISTURB EXCEPT WHERE NOTED IN THE PLANS)

- NOTE:
- FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/dsf6erwm)).
 - CONTRACTOR SHALL NOTIFY THE CITY IN WRITING A MINIMUM OF 14 DAYS PRIOR TO WORKING WITHIN THE VICINITY OF SENSORS TO COORDINATE THEIR REMOVAL BY THE CITY PRIOR TO MILLING OPERATIONS. IF THE CONTRACTOR FAILS TO PROVIDE ADEQUATE NOTICE TO THE CITY, WITH OPERATIONS RESULTING IN ANY DAMAGE TO SENSORS, CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR THE REPAIR AND/OR REPLACEMENT AT THE DISCRETION OF THE CITY ENGINEER.
- CONTACT: MARC DILSAVER, TRANSPORTATION PROGRAM MANAGER,
614.410.4744, MDILSAVER@DUBLIN.OH.US

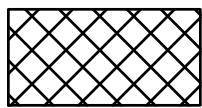
N. HIGH ST. IMPROVEMENTS



DESIGN AGENCY	
 GPD GROUP Claus, Pyle, Schorner, Burris & Dehaven, Inc. Copyright 2025. All rights reserved. Version 1.0, 2025	
DESIGNER	MSS
REVIEWER	LMO 2/18/25
PROJECT ID	25-002-CIP
SHEET	TOTAL
P.33	60



LOCATION MAP: NTS
WOERNER TEMPLE RD.



1.5" MILL AND FILL:
ITEM 254 - 1.5" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT (SEE NOTE 2)

- NOTE:
- FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/dsf6erwm)).
 - DO NOT APPLY ASPHALT REJUVENATING AGENT WITHIN THE LIMITS OF THE ROUNDABOUT.
 - FOR WOERNER TEMPLE RD. DESIGN DETAILS, SEE SHEETS 35-36.
 - FOR CURB RAMP DETAILS, SEE SHEET 47.
 - FOR WOERNER TEMPLE RD. TRAFFIC CONTROL PLAN, SEE SHEET 48.

ESTIMATED QUANTITIES WOERNER TEMPLE RD.			
(1.5" MILL AND FILL - BALLANTRAE PL. WEST ROUNDABOUT RETURN TO EMERALD PKWY.)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	FT	1,460
202	CURB REMOVED AND DISPOSED OF, AS PER PLAN	FT	1,498
202	CONCRETE REMOVED AND DISPOSED OF, AS PER PLAN	SF	2,267
203	EXCAVATION	CY	292
203	EMBANKMENT	CY	404
204	SUBGRADE COMPACTION	SY	332
251	PARTIAL DEPTH PAVEMENT REPAIR (441) (4")	SY	53
254	PAVEMENT PLANING, ASPHALT CONCRETE (1.5")	SY	42,372
301	ASPHALT CONCRETE BASE COURSE	TON	46
304	AGGREGATE BASE	CY	50
407	NON-TRACKING TACK COAT	GAL	4,220
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	3,516
604	MANHOLE, CATCH BASIN, OR INLET ADJUSTED TO GRADE	EACH	3
608	4" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	1,081
608	6" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	1,761
608	CURB RAMPS	EACH	25
608	DETECTABLE WARNING, TYPE A, AS PER PLAN	SF	382
608	BRICK PAVER REMOVE AND RESET, TYPE 2, AS PER PLAN	SF	335
609	STRAIGHT 18" CURB, AS PER PLAN	FT	1,844
609	COMBINATION CURB AND GUTTER, 8" THICK, AS PER PLAN	FT	1,447
611	INLET NO. 2-A-10, AS PER PLAN	EACH	1
630	REMOVAL OF GROUND MOUNTED SIGN AND STORAGE, AS PER PLAN	EACH	4
630	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	EACH	5
630	SIGN, FLAT SHEET	SF	63.3
630	GROUND MOUNTED SUPPORT, NO. 3 POST, AS PER PLAN	FT	135.5
630	SIGNING, MISC.: 2 1/4" SQUARE ANCHOR POST	EACH	8
644	EDGE LINE, 4"	MILE	2.27
644	LANE LINE, 4"	MILE	1.75
644	CENTER LINE	MILE	0.02
644	CHANNELIZING LINE, 8"	FT	1,996
644	STOP LINE	FT	199
644	CROSSWALK LINE, 12"	FT	1,826
644	LANE ARROW	EACH	33
644	TRANSVERSE/DIAGONAL LINE	FT	141
644	CHEVRON MARKING	FT	78
644	DOTTED LINE, 4", WHITE	FT	288
659	SEEDING AND MULCHING, AS PER PLAN	SY	1,130
SPECIAL	INLET REPAIR AND PARGE	EACH	13
SPECIAL	ASPHALT REJUVENATING AGENT	SY	38,665

DESIGN AGENCY



DESIGNER
MSS

REVIEWER
LMO 2/18/25

PROJECT ID
25-002-CIP

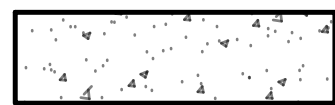
SHEET TOTAL
P.34 60

① **CURVE DATA**
P.I. = Sta. 0+76.56
D = 6° 48' 17"
Dc = 5° 43' 46"
R = 1000.000
T = 59.45'
L = 118.76'
E = 1.77
C = 118.69
C. B. = S 85° 34' 55" E

② **CURVE DATA**
P.I. = Sta. 3+43.91
D = 12° 29' 35"
Dc = 3° 49' 11"
R = 1500.000
T = 164.18'
L = 327.06'
E = 8.96
C = 326.42
C. B. = S 88° 31' 46" E

③ **CURVE DATA**
P.I. = Sta. 2+92.58
D = 10° 59' 57"
Dc = 9° 32' 56"
R = 600.000
T = 57.77'
L = 115.19'
E = 2.77
C = 115.01
C. B. = S 88° 31' 30" E

LEGEND



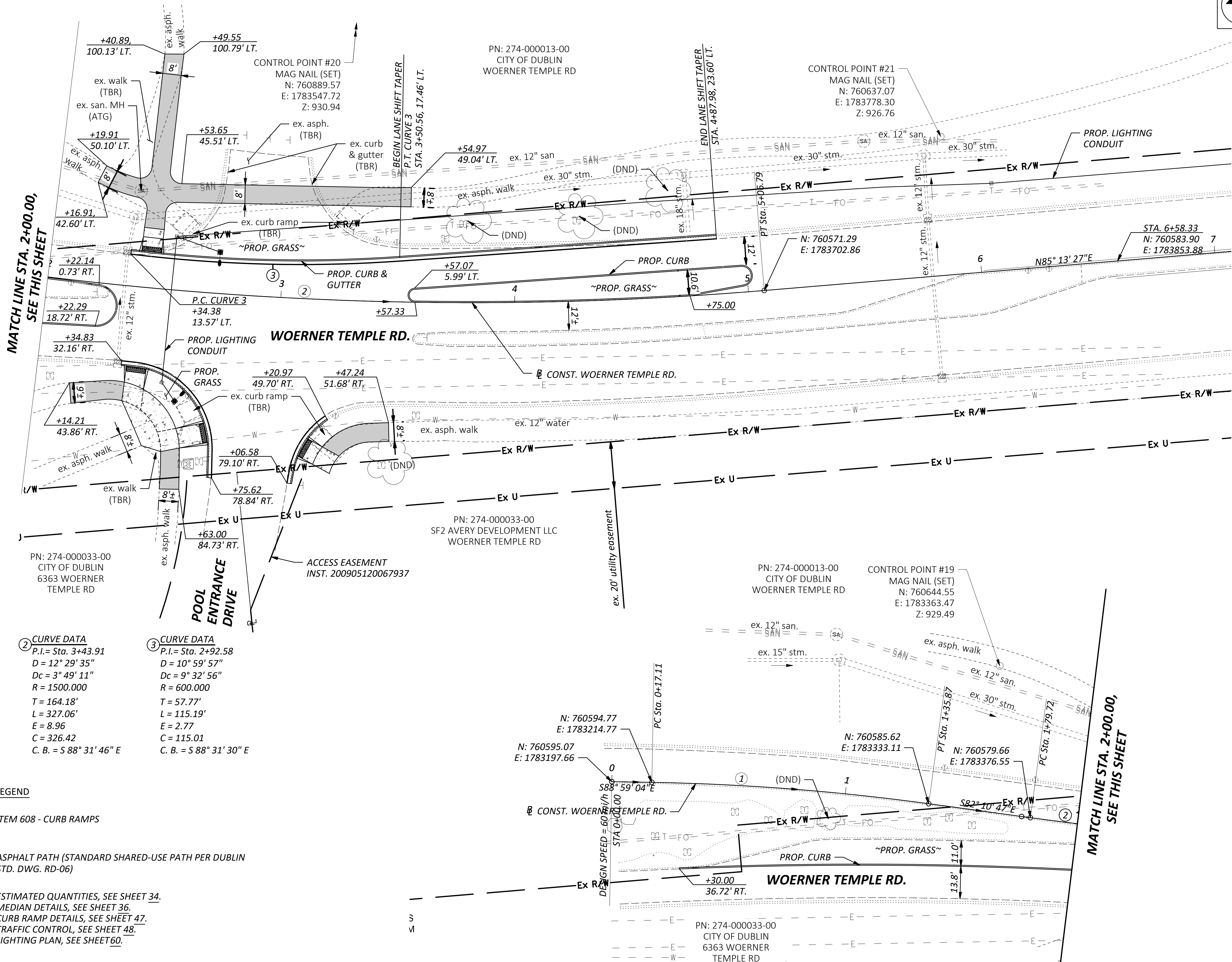
ITEM 608 - CURB RAMPS



ASPHALT PATH (STANDARD SHARED-USE PATH PER DUBLIN STD. DWG. RD-06)

NOTE:

1. FOR ESTIMATED QUANTITIES, SEE SHEET 34.
2. FOR MEDIAN DETAILS, SEE SHEET 36.
3. FOR CURB RAMP DETAILS, SEE SHEET 47.
4. FOR TRAFFIC CONTROL, SEE SHEET 48.
5. FOR LIGHTING PLAN, SEE SHEET 60.



WOERNER TEMPLE RD IMPROVEMENTS

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

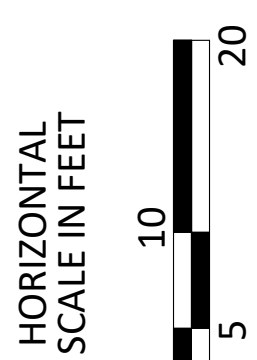
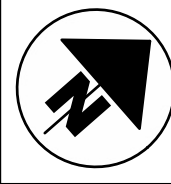
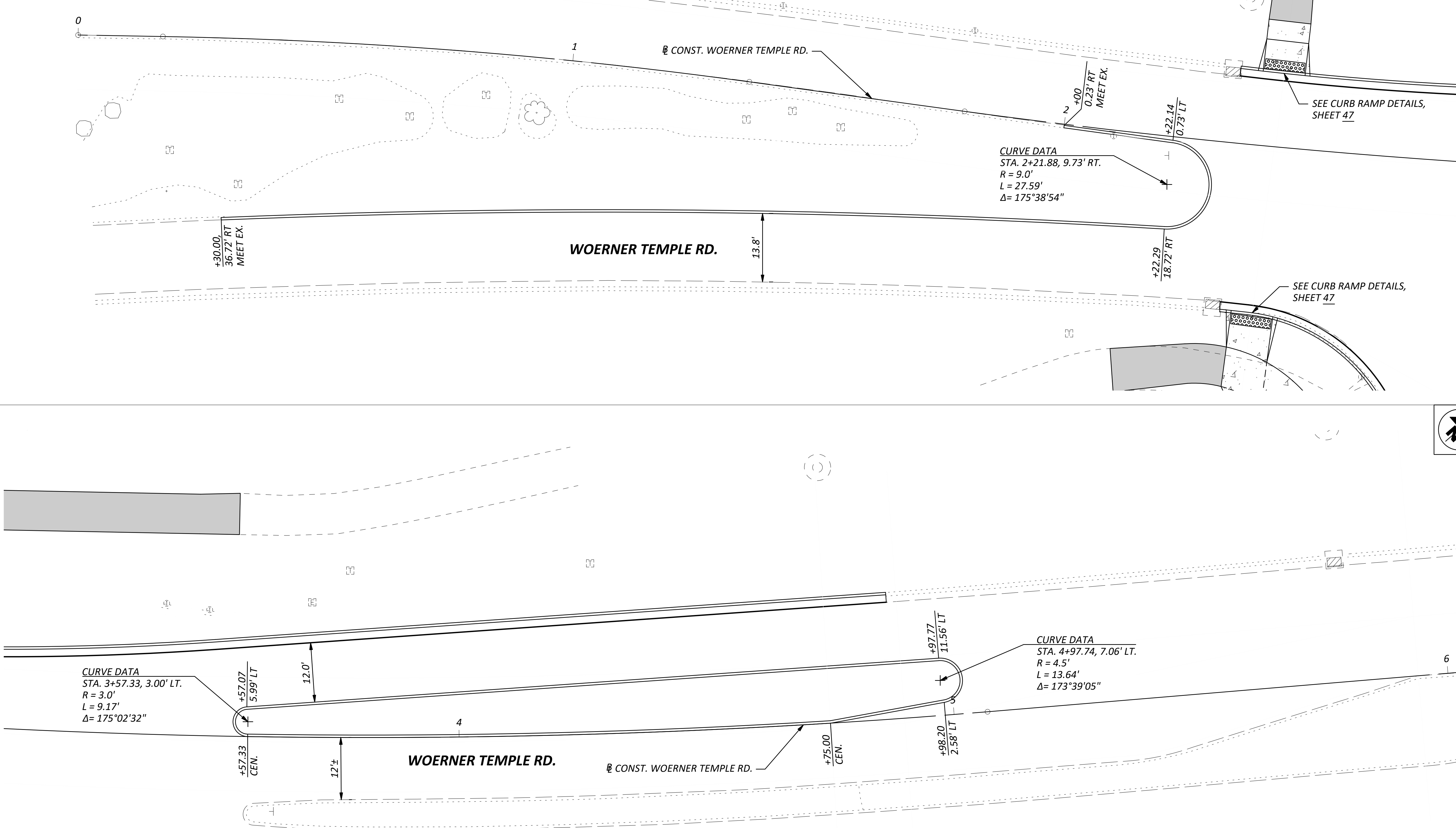
PROJECT ID

25-002-CIP

SHEET

TOTAL

P.35 60



WOERNER TEMPLE RD. MEDIAN DETAILS

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

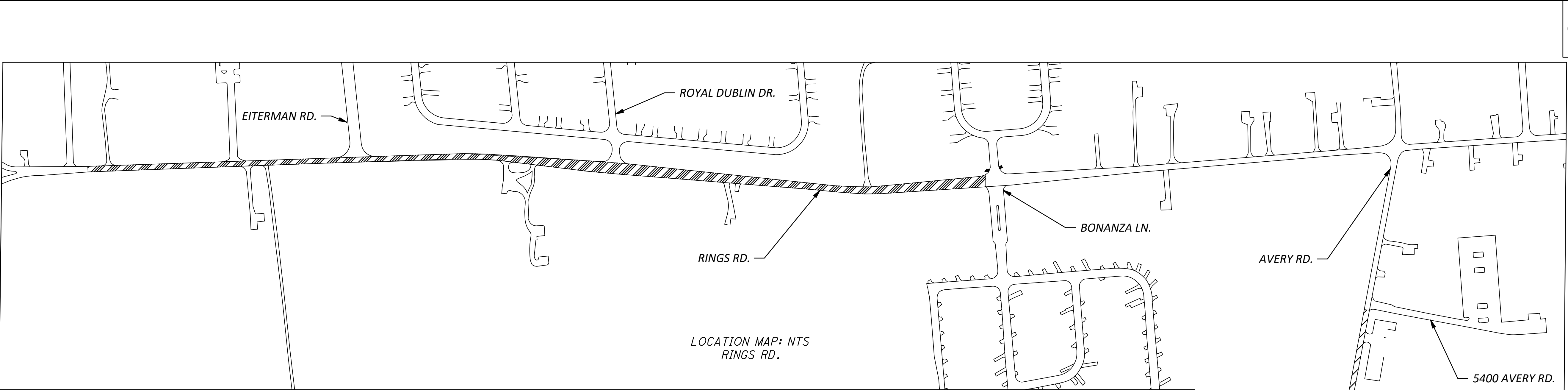
25-002-CIP

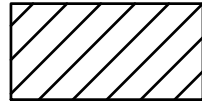
SHEET

P.36

TOTAL

60




- 

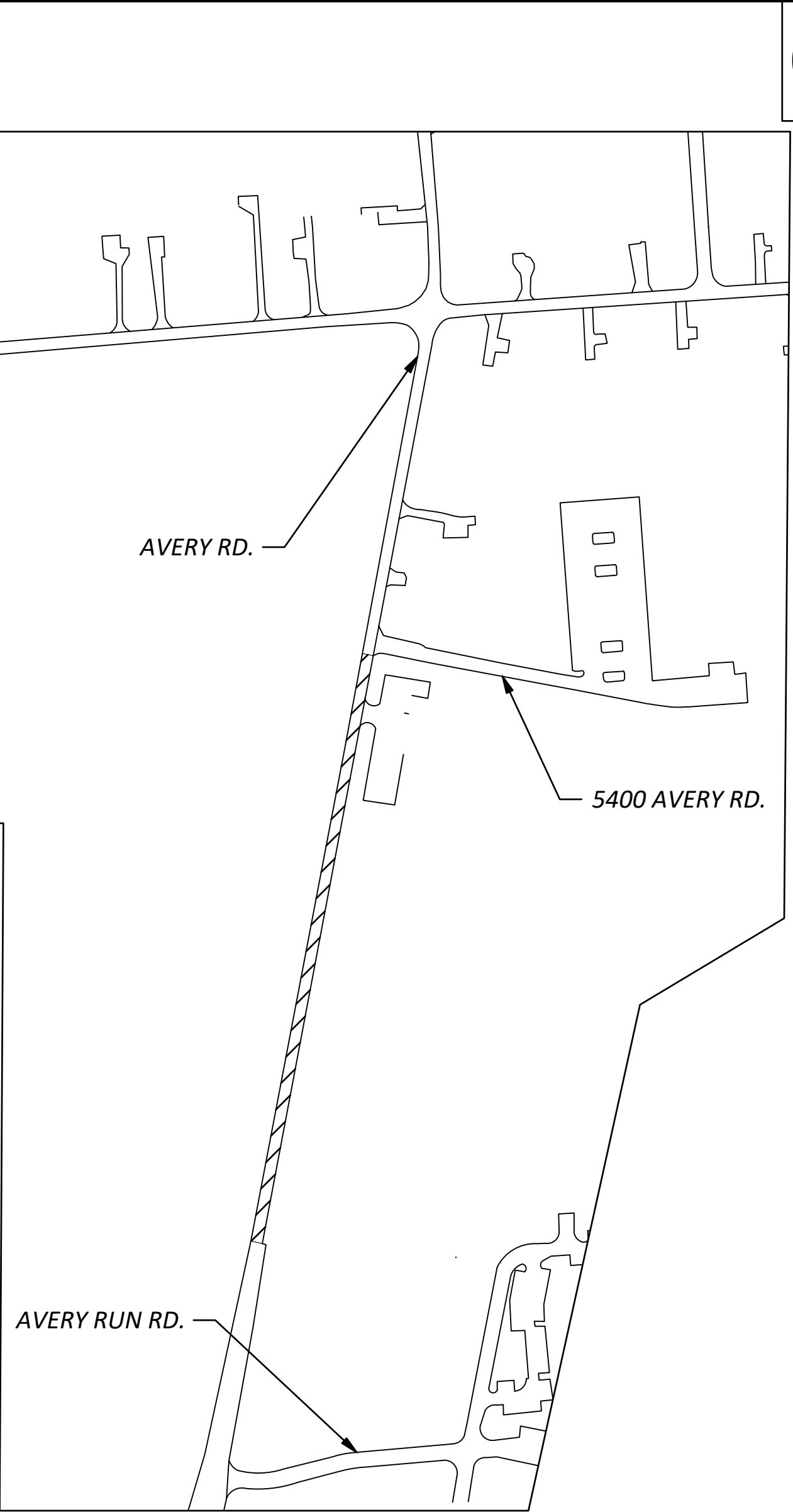
3" MILL AND FILL:
ITEM 254 - 3" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 1.75" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
ITEM 407 - NON-TRACKING TACK COAT, 0.075 GAL/SY
ITEM 441 - 1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT
- SHOULDER TREATMENT (2' WIDE):**
ITEM 617 - RECONDITION SHOULDER, 4" COMPACTED AGGREGATE, AS PER PLAN
ITEM 617 - RECONDITION SHOULDERS, 1.5" ITEM 653 PULVERIZED TOPSOIL, AS PER PLAN
ITEM 659 - SEEDING AND MULCHING, AS PER PLAN

NOTE:
1. FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/dsf6erwm)).

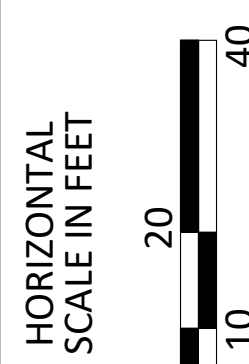
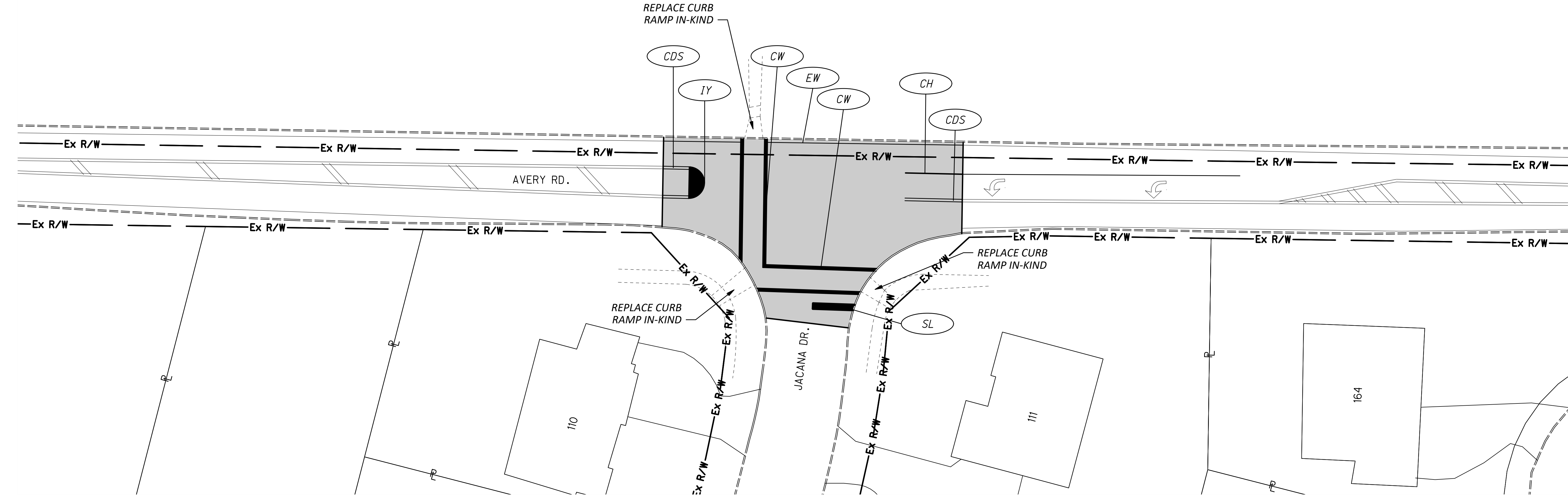
ESTIMATED QUANTITIES RINGS RD.			
(4" MILL AND FILL - 940' WEST OF EITERMAN RD. TO BONANZA LN.)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CONCRETE REMOVED AND DISPOSED OF, AS PER PLAN	SF	229
254	PAVEMENT PLANING, ASPHALT CONCRETE (4")	SY	8,583
407	NON-TRACKING TACK COAT	GAL	1,502
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	715
441	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	TON	1,192
608	4" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	93
608	6" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	136
608	CURB RAMPS	EACH	2
608	DETECTABLE WARNING, TYPE A, AS PER PLAN	SF	32
617	RECONDITION SHOULDER, 4" COMPACTED AGGREGATE, AS PER PLAN	CY	156
617	RECONDITION SHOULDER, 1.5" ITEM 653 PULVERIZED TOPSOIL, AS PER PLAN	CY	58
644	EDGE LINE, 4", WHITE	MILE	1.19
644	CENTER LINE	MILE	0.75
644	CHANNELIZING LINE, 8"	FT	120
644	LANE ARROW	EACH	4
644	TRANSVERSE/DIAGONAL LINE	FT	450
644	ISLAND MARKING, YELLOW	SF	70
659	SEEDING AND MULCHING, AS PER PLAN	SY	1,401
SPECIAL	ASPHALT REJUVENATING AGENT	SY	8,583

- 

4" MILL AND FILL:
ITEM 254 - 4" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 2.5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
ITEM 407 - NON-TRACKING TACK COAT, 0.075 GAL/SY
ITEM 441 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT



ESTIMATED QUANTITIES AVERY RD.			
(3" MILL AND FILL - 400' NORTH OF AVERY RUN RD. TO 5400 AVERY RD.)			
ITEM	DESCRIPTION	UNIT	QUANTITY
254	PAVEMENT PLANING, ASPHALT CONCRETE (3")	SY	2,041
407	NON-TRACKING TACK COAT	GAL	357
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	142
441	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	TON	198
617	RECONDITION SHOULDER, 4" COMPACTED AGGREGATE, AS PER PLAN	CY	49
617	RECONDITION SHOULDER, 1.5" ITEM 653 PULVERIZED TOPSOIL, AS PER PLAN	CY	18
644	EDGE LINE, 4", WHITE	MILE	0.39
644	CENTER LINE	MILE	0.19
659	SEEDING AND MULCHING, AS PER PLAN	SY	440
SPECIAL	ASPHALT REJUVENATING AGENT	SY	2,041



AVERY RD. & JACANA DR. INTERSECTION IMPROVEMENTS



FULL DEPTH REPLACEMENT:
ITEM 204 - SUBGRADE COMPACTION
ITEM 203 - EXCAVATION
ITEM 304 - 6" AGGREGATE BASE
ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22, (449) (TWO LIFTS OF 3" WITH ITEM 407 - NON-TRACKING TACK COAT, 0.075 GAL/SY)
ITEM 407 - NON-TRACKING TACK COAT, 0.075 GAL/SY
ITEM 441 - 1.75" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
ITEM 407 - NON-TRACKING TACK COAT, 0.075 GAL/SY
ITEM 441 - 1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT

NOTE:
1. FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/dsf6erwm)).

PAVEMENT MARKING LEGEND	
	EDGE LINE, WHITE
	ISLAND MARKING, YELLOW
	CENTER LINE, DOUBLE SOLID
	CHANNELIZING LINE, 8"
	STOP LINE
	CROSSWALK LINE, 12"

ESTIMATED QUANTITIES AVERY RD.			
(FULL DEPTH REPLACEMENT - JACANA INTERSECTION)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	FT	150
202	CONCRETE REMOVED AND DISPOSED OF, AS PER PLAN	SF	218
203	EXCAVATION	CY	261
204	SUBGRADE COMPACTION	SY	627
301	ASPHALT CONCRETE BASE, PG64-22	TON	209
304	AGGREGATE BASE	CY	105
407	NON-TRACKING TACK COAT	GAL	141
441	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	TON	61
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	44
608	4" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	64
608	6" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	154
608	CURB RAMPS	EACH	3
608	DETECTABLE WARNING, TYPE A, AS PER PLAN	SF	30
609	COMBINATION CURB AND GUTTER, 6" THICK, AS PER PLAN	FT	150
644	CENTER LINE	MILE	0.03
644	CHANNELIZING LINE, 8"	FT	22
644	CROSSWALK, LINE, 12"	FT	175
644	EDGE LINE, 4"	MILE	0.03
644	STOP LINE	FT	15
644	ISLAND MARKING	SF	57
SPECIAL	ASPHALT REJUVENATING AGENT	SY	627

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

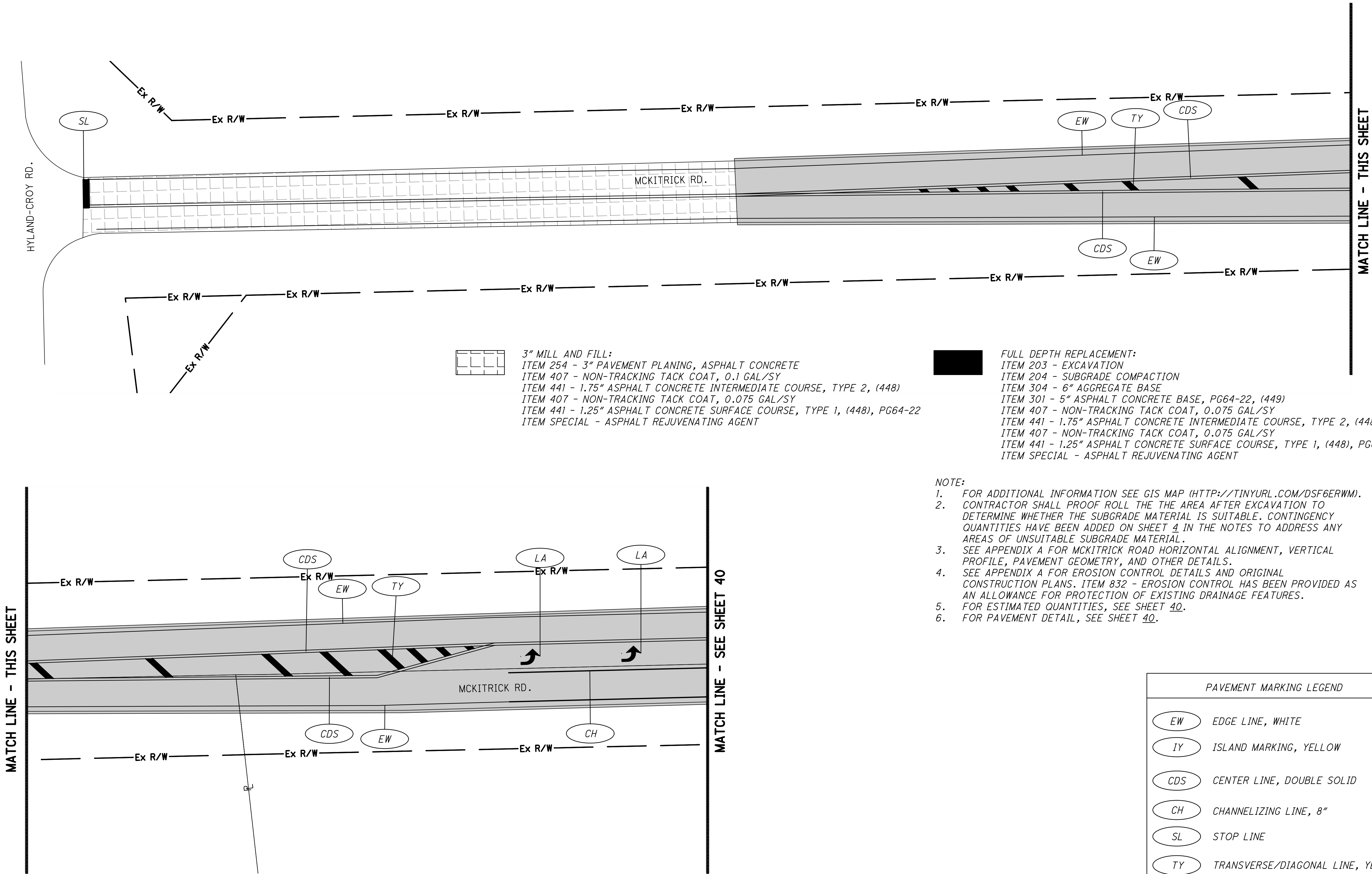
25-002-CIP

SHEET

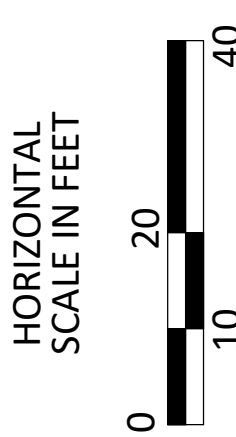
P.38

TOTAL

60



PAVEMENT MARKING LEGEND	
EW	EDGE LINE, WHITE
IY	ISLAND MARKING, YELLOW
CDS	CENTER LINE, DOUBLE SOLID
CH	CHANNELIZING LINE, 8"
SL	STOP LINE
TY	TRANSVERSE/DIAGONAL LINE, YELLOW
LA	LANE ARROW



MCKITRICK RD. IMPROVEMENTS

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

25-002-CIP

SHEET

P.39

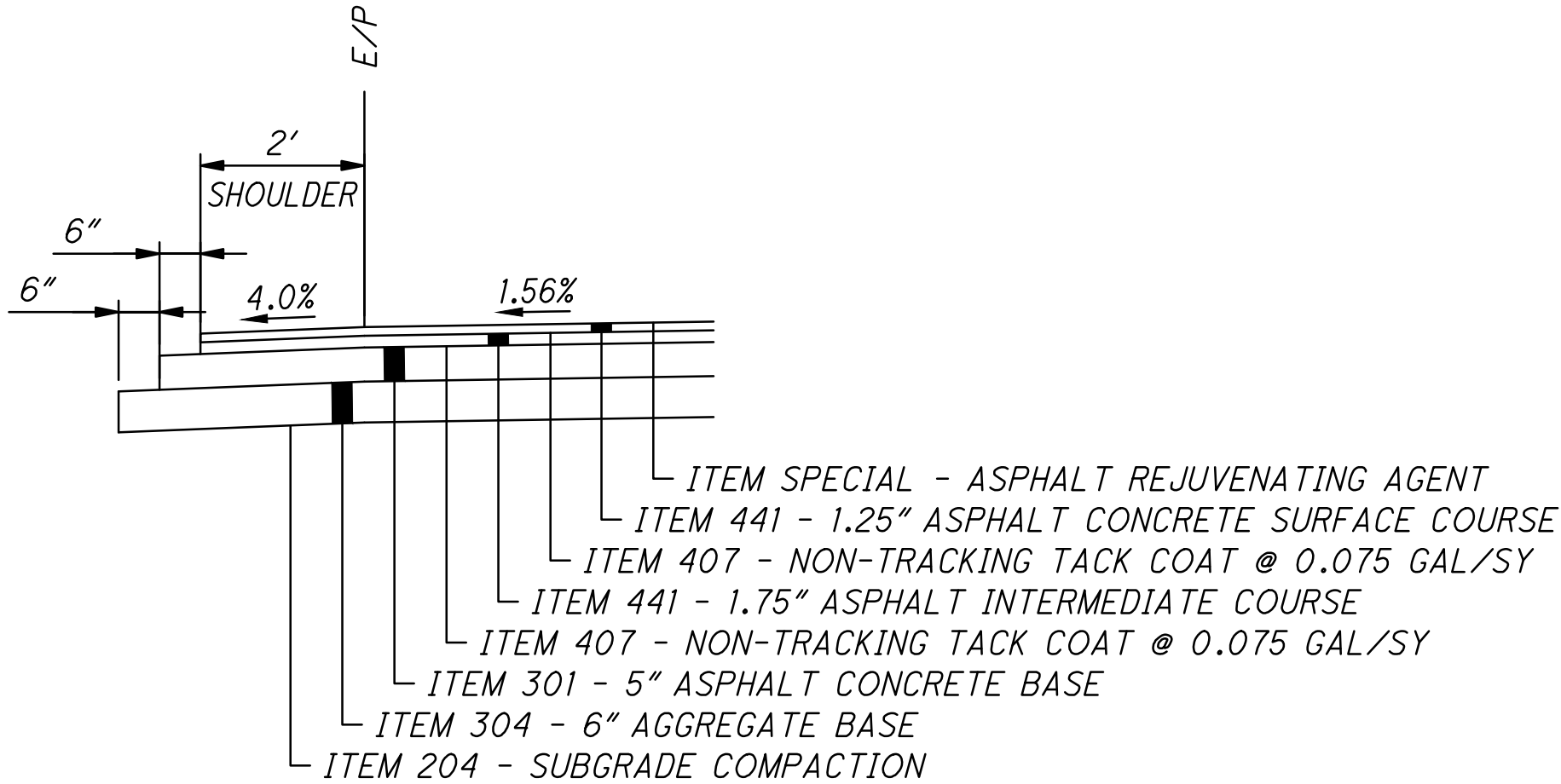
TOTAL

60

ESTIMATED QUANTITIES MckITRICK RD.			
(FULL DEPTH REPLACEMENT)			
ITEM	DESCRIPTION	UNIT	QUANTITY
203	EXCAVATION	CY	1,585
204	SUBGRADE COMPACTION	SY	4,184
254	PAVEMENT PLANING, ASPHALT CONCRETE (3")	SY	730
301	ASPHALT CONCRETE BASE, PG64-22, (449)	TON	1,102
304	AGGREGATE BASE	CY	679
407	NON-TRACKING TACK COAT	GAL	707
441	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	TON	446
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	319
644	CENTER LINE	MILE	0.41
644	CHANNELIZING LINE, 8"	FT	125
644	TRANSVERSE/DIAGONAL MARKING	FT	204
644	EDGE LINE, 4"	MILE	0.56
644	STOP LINE	FT	12
644	ISLAND MARKING	SF	57
644	LANE ARROW	EACH	3
659	SEEDING AND MULCHING, AS PER PLAN	SY	1,081
832	EROSION CONTROL	EACH	10,000
SPECIAL	ASPHALT REJUVENATING AGENT	SY	4,590

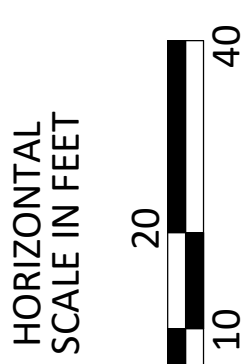
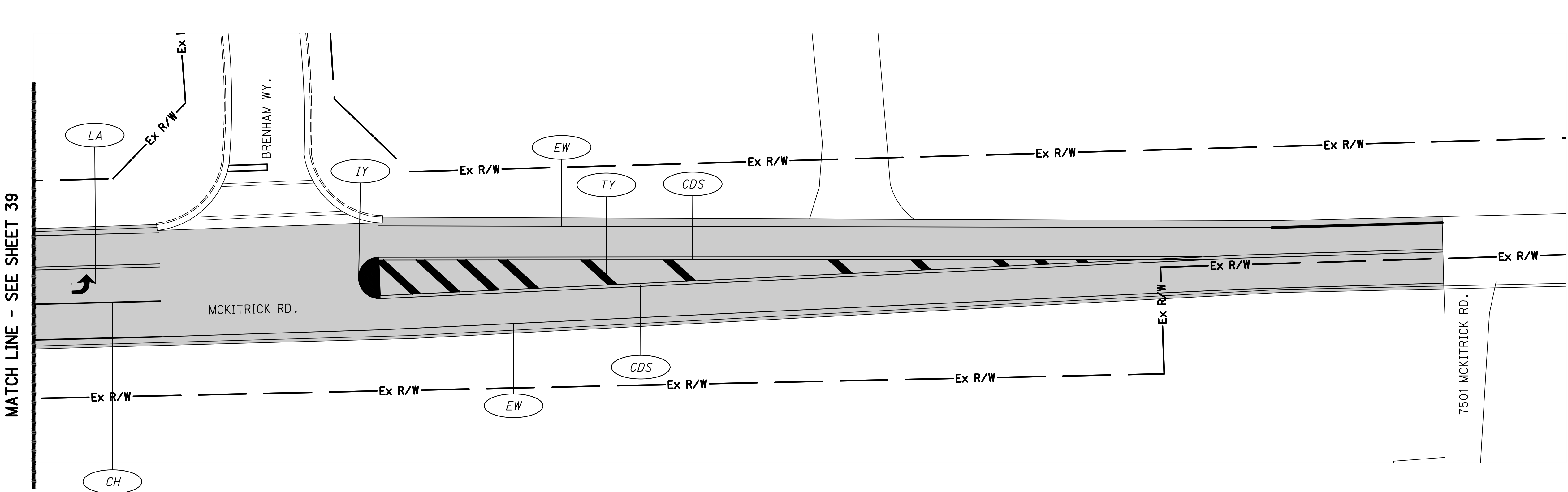
NOTE:

- FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/DSF6ERWM)).
- CONTRACTOR SHALL PROOF ROLL THE THE AREA AFTER EXCAVATION TO DETERMINE WHETHER THE SUBGRADE MATERIAL IS SUITABLE. CONTINGENCY QUANTITIES HAVE BEEN ADDED ON SHEET 4 IN THE NOTES TO ADDRESS ANY AREAS OF UNSUITABLE SUBGRADE MATERIAL.
- SEE APPENDIX A FOR MCKITRICK ROAD HORIZONTAL ALIGNMENT, VERTICAL PROFILE, PAVEMENT GEOMETRY, AND OTHER DETAILS.
- SEE APPENDIX A FOR EROSION CONTROL DETAILS AND ORIGINAL CONSTRUCTION PLANS. ITEM 832 - EROSION CONTROL HAS BEEN PROVIDED AS AN ALLOWANCE FOR PROTECTION OF EXISTING DRAINAGE FEATURES.



PAVEMENT DETAIL

PAVEMENT MARKING LEGEND	
EW	EDGE LINE, WHITE
IY	ISLAND MARKING, YELLOW
CDS	CENTER LINE, DOUBLE SOLID
CH	CHANNELIZING LINE, 8"
SL	STOP LINE
TY	TRANSVERSE/DIAGONAL LINE, YELLOW
LA	LANE ARROW



MCKITRICK RD. IMPROVEMENTS

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

25-002-CIP

SHEET

P.40

TOTAL

60

GENERAL NOTES:

SCOPE OF WORK

THIS WORK CONSISTS OF RETROFITTING THE ROADWAY END OF EACH EXISTING APPROACH SLAB FOR THE POST ROAD BRIDGE BY CONSTRUCTING AN APPROACH SLAB INSTALLATION (TYPE C) IN ACCORDANCE WITH ODOT STANDARD DRAWING AS-2-15 AND AS SHOWN ON THE PLANS. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE ODOT CONSTRUCTION AND MATERIAL SPECIFICATIONS. THIS INSTALLATION WILL CREATE AN EXPANSION JOINT TO ACCOMMODATE MOVEMENT OF THE APPROACH SLAB WITHOUT CAUSING DISTRESS IN THE APPROACH ROADWAY ASPHALT PAVEMENT. WORK SHALL BE PERFORMED PART-WIDTH IN PHASES WHILE MAINTAINING TRAFFIC. SEE MAINTENANCE OF TRAFFIC PLANS FOR FURTHER DETAILS.

ITEM 516 - ARMORLESS PREFORMED JOINT SEAL, AS PER PLAN

THIS WORK SHALL INCLUDE INSTALLATION OF A PREFORMED ARMORLESS JOINT SEAL WITHIN THE PROPOSED APPROACH SLAB INSTALLATION TYPE C AS SHOWN IN THE PLAN DETAILS. THE SEAL SHALL BE A 2¾" SIKA EMSEAL BEJS SILICONE EXPANSION JOINT SYSTEM. THE SEAL SHALL BE INSTALLED AND SPLICED IN ACCORDANCE WITH THE MANUFACTURER'S REQUIREMENTS. THE SEAL SHALL BE TRIMMED FLUSH AND INSTALLED TIGHTLY AGAINST THE EXISTING CONCRETE SLAB SUPPORTING THE BARRIER WALL AT EACH END.

ITEM 519 - PATCHING CONCRETE STRUCTURE

THIS WORK SHALL BE PERFORMED AS DIRECTED BY THE ENGINEER TO REPAIR DETERIORATED EDGES AT THE ENDS OF THE EXISTING APPROACH SLABS. ALL WORK SHALL COMPLY WITH ODOT CMS ITEM 519.

ITEM 526 - REINFORCED CONCRETE APPROACH SLABS (T=15"), AS PER PLAN

THIS WORK SHALL INCLUDE A 5 FOOT EXTENSION OF THE EXISTING APPROACH SLAB PAVEMENT TO BE CONSTRUCTED ON TOP OF THE NEW SLEEPER SLAB OF TYPE C INSTALLATION. ALL REINFORCING STEEL SHOWN ON THE PLANS SHALL BE INCLUDED IN THE COST OF THE APPROACH SLAB PAVEMENT.

ITEM 526 - TYPE C INSTALLATION, AS PER PLAN

THIS WORK INCLUDES CONSTRUCTING THE TYPE C INSTALLATION AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH ODOT STANDARD DRAWING AS-2-15. THIS INSTALLATION WILL BE PLACED IN PHASES BETWEEN THE EXISTING CONCRETE BASE PAVEMENT SLABS WHICH SUPPORT THE ADJACENT ROADWAY BARRIERS, WHICH ARE TO REMAIN UNDISTURBED.

ESTIMATED QUANTITIES:

ITEM	DESCRIPTION	UNIT	QUANTITY
202	PAVEMENT REMOVED	SY	96
254	PAVEMENT PLANING, ASPHALT CONCRETE (1.5")	SY	55
304	AGGREGATE BASE	CY	20
407	NON-TRACKING TACK COAT	GAL	6
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	5
503	UNCLASSIFIED EXCAVATION	CY	48
510	DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT	EACH	124
516	ARMORLESS PREFORMED JOINT SEAL, AS PER PLAN	FT	123
516	1" PREFORMED EXPANSION JOINT FILLER	SF	21
519	PATCHING CONCRETE STRUCTURE	SF	123
526	REINFORCED CONCRETE APPROACH SLABS (T=15"), AS PER PLAN	SY	69
526	TYPE C INSTALLATION, AS PER PLAN	FT	123

POST ROAD APPROACH SLAB NOTES

DESIGN AGENCY



DESIGNER

JKM

REVIEWER

TJW 2/18/25

PROJECT ID

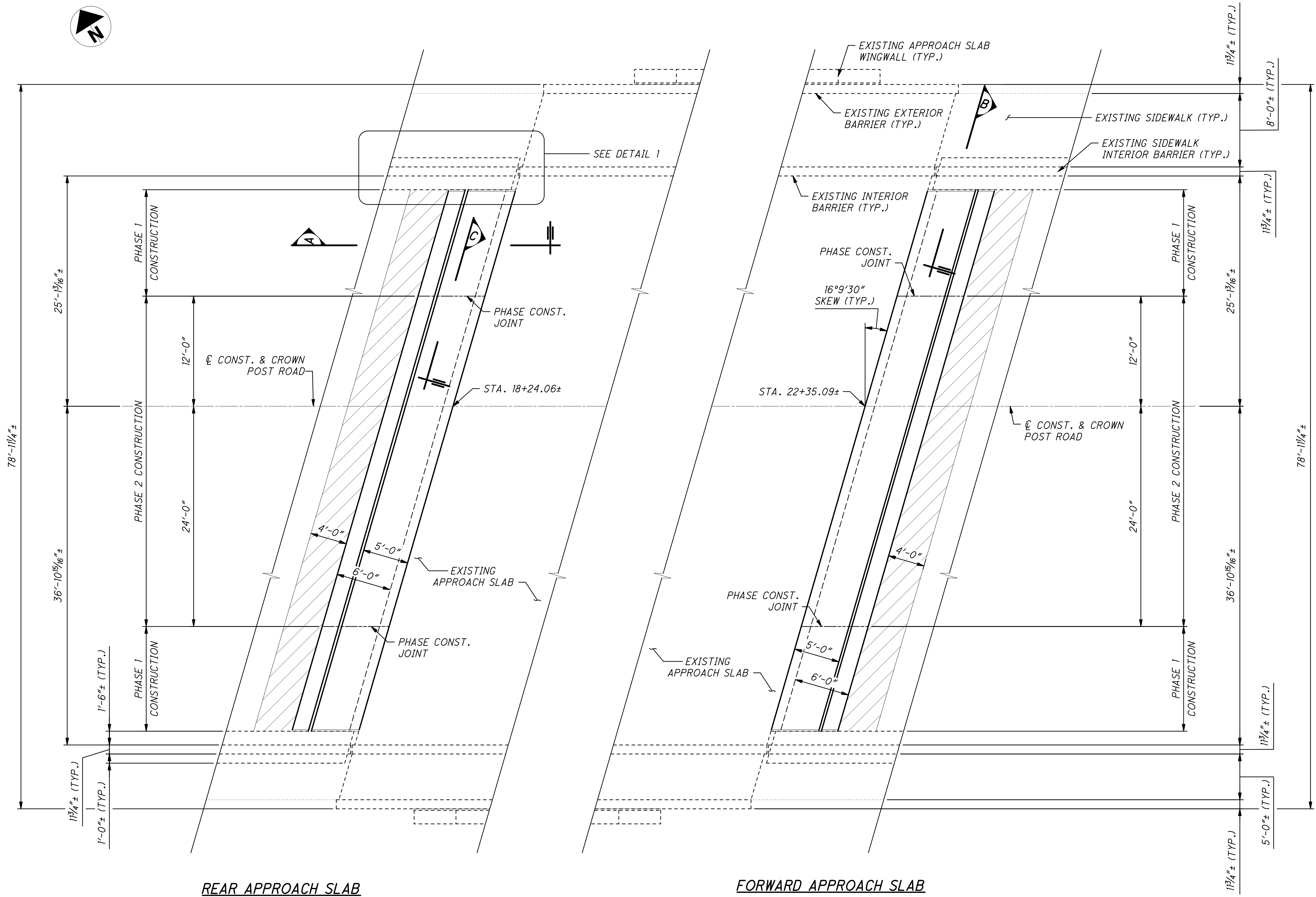
25-002-CIP

SHEET

P.41

TOTAL

60



POST ROAD APPROACH SLAB PLAN

SCALE: $\frac{3}{16}" = 1'-0"$

DESIGN AGENCY



DESIGNER

JKM

REVIEWER

TJW 2/18/25

PROJECT ID

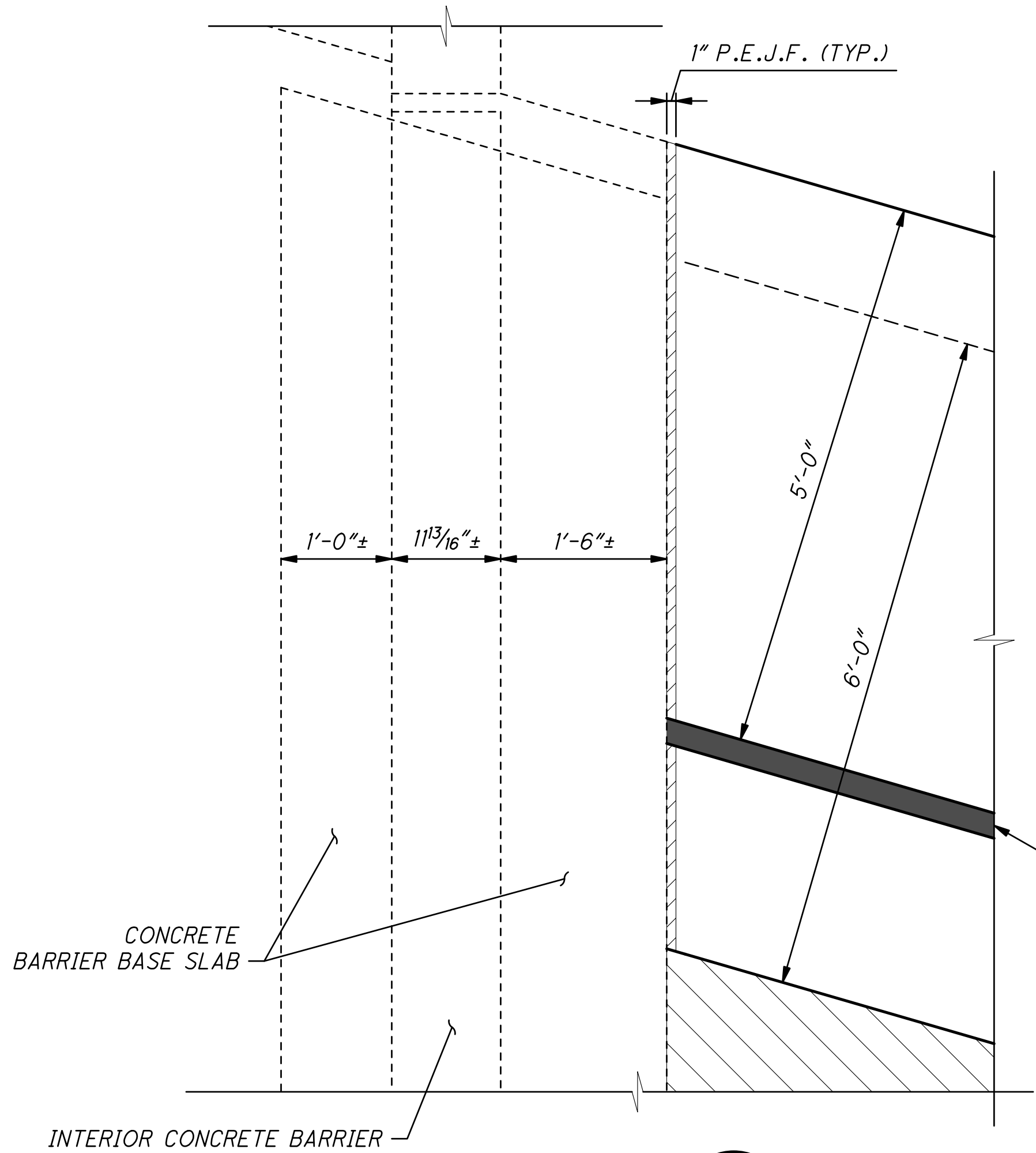
25-002-CIP

SHEET

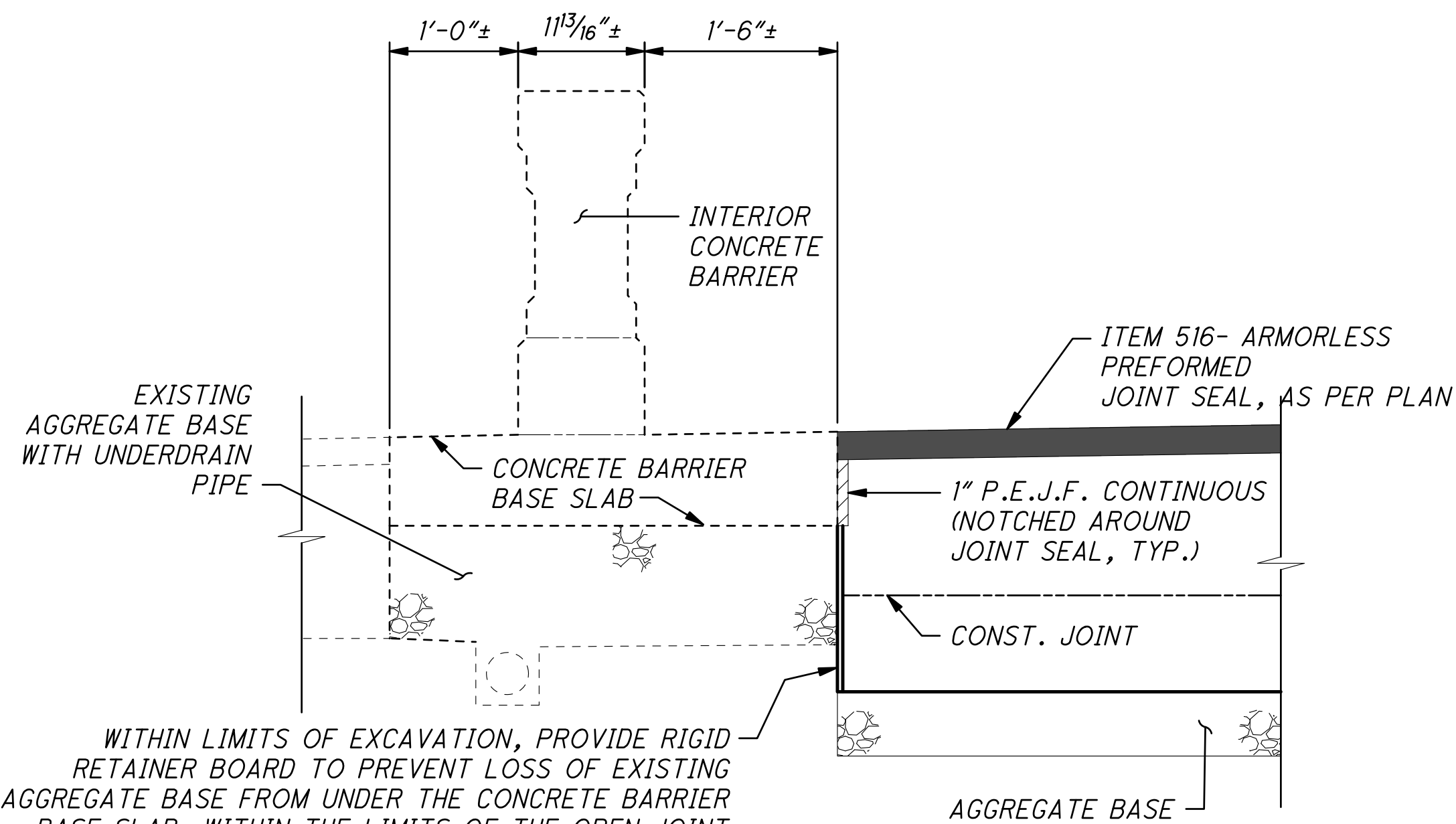
P.42

TOTAL

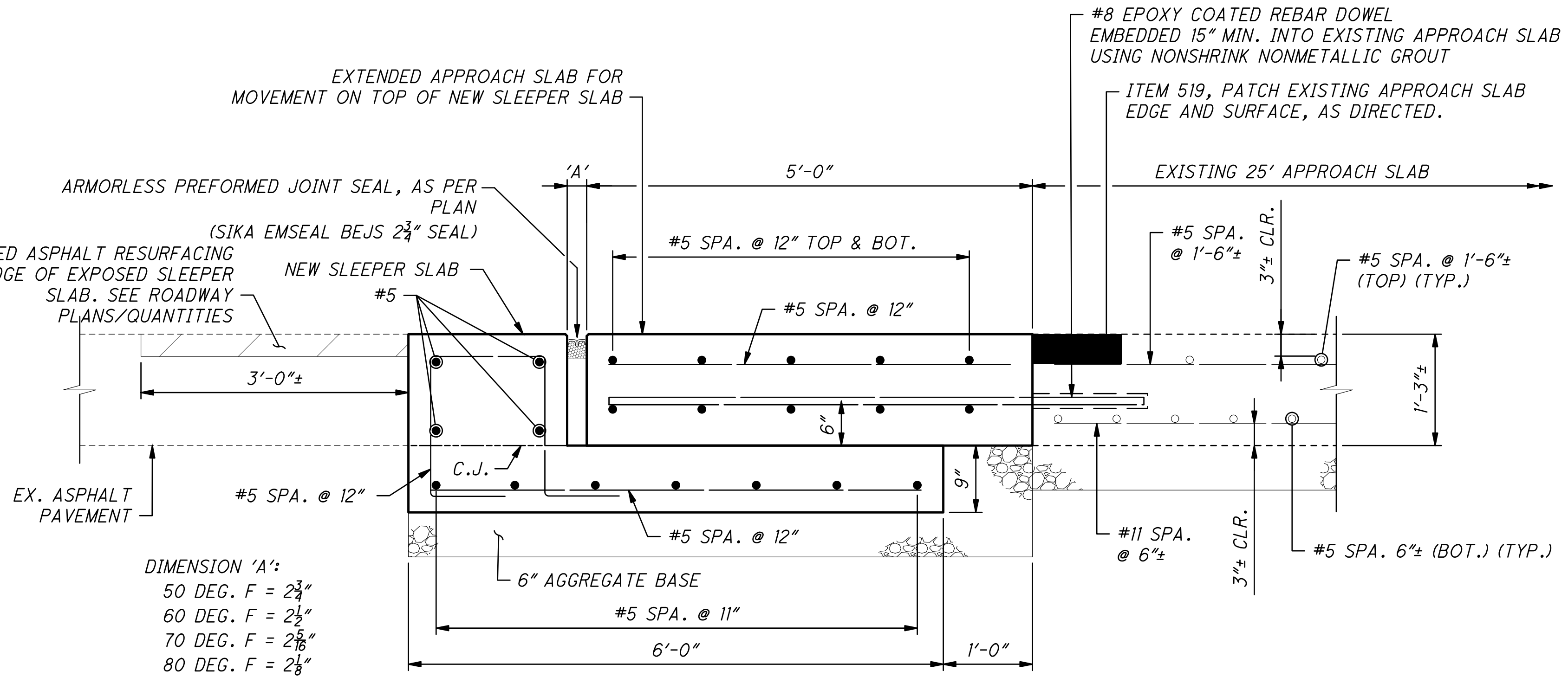
60



DETAIL 1

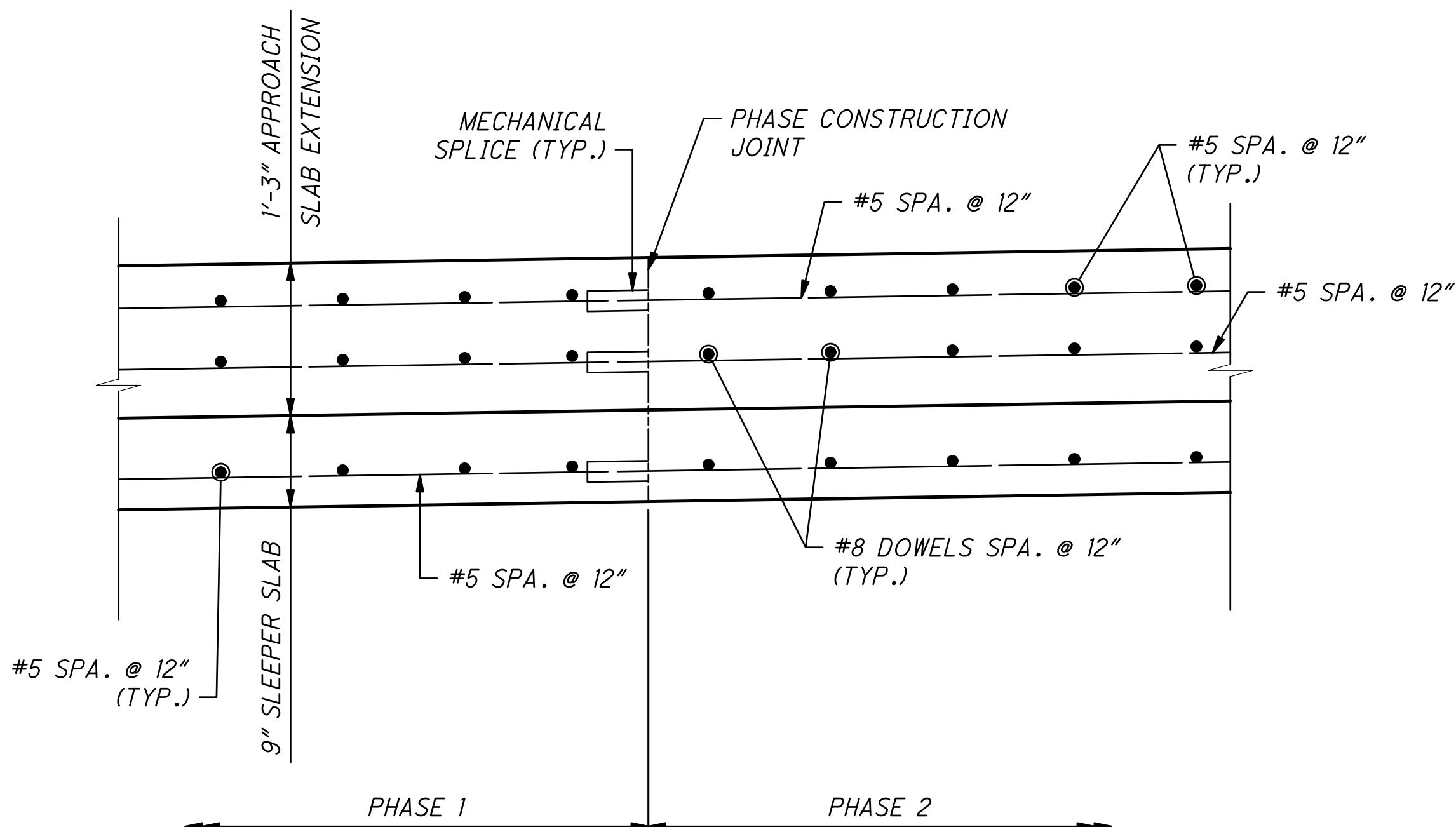


SECTION B



SECTION A

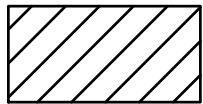
ALL LONGITUDINAL REINFORCING STEEL SHALL BE PLACED PARALLEL TO THE CENTERLINE OF BRIDGE. ALL TRANSVERSE REINFORCING STEEL SHALL BE PLACED ALONG THE SKEW.



SECTION C

NOTES:

- FOR LOCATION OF SECTIONS, SEE SHEET 42.
- FOR GENERAL NOTES, SEE SHEET 41.
- FOR DETAILS AND REQUIREMENTS NOT SHOWN, SEE STANDARD DRAWING AS-2-15 TYPE C INSTALLATION.

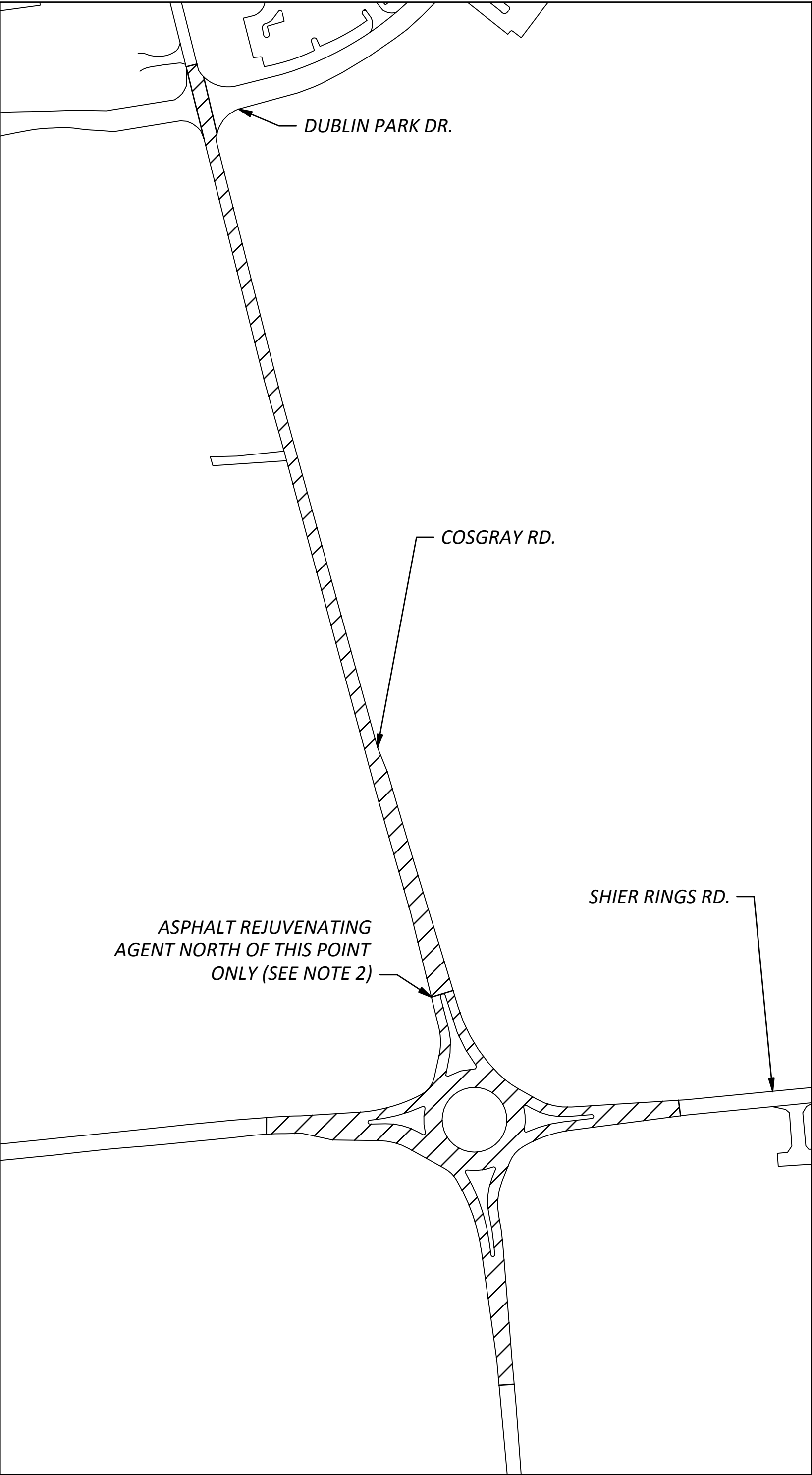


2" MILL AND FILL:
ITEM 254 - 2" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 2" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT (SEE NOTE 2)

SHOULDER TREATMENT (2' WIDE):
ITEM 617 - RECONDITION SHOULDER, 4" COMPACTED AGGREGATE, AS PER PLAN
ITEM 617 - RECONDITION SHOULDERS, 1.5" ITEM 653 PULVERIZED TOPSOIL, AS PER PLAN
ITEM 659 - SEEDING AND MULCHING, AS PER PLAN

- NOTE:
- FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/DSF6ERWM)).
 - ASPHALT REJUVENATING AGENT SHALL ONLY BE USED IN THE AREA NORTH OF THE ROUNDABOUT, AS INDICATED IN THE LOCATION MAP ON THIS SHEET.

ESIMATED QUANTITIES COSGRAY RD.			
(2" MILL AND FILL - SHIER RINGS RD. TO DUBLIN PARK DR.)			
ITEM	DESCRIPTION	UNIT	QUANTITY
202	CURB AND GUTTER REMOVED AND DISPOSED OF, AS PER PLAN	FT	20
202	CURB REMOVED AND DISPOSED OF, AS PER PLAN	FT	10
251	PARTIAL DEPTH PAVEMENT REPAIR (441)	SY	10
254	PAVEMENT PLANING, ASPHALT CONCRETE (2")	SY	8,712
407	NON-TRACKING TACK COAT	GAL	871
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	968
608	4" CONCRETE WALK WITH AGGREGATE BASE, AS PER PLAN	SF	136
608	BRICK PAVER REMOVE & RESET, TYPE 2, AS PER PLAN	SF	25
609	COMBINATION CURB AND GUTTER, 8" THICK, AS PER PLAN	FT	20
609	STRAIGHT 18" CURB, AS PER PLAN	FT	10
617	RECONDITION SHOULDER, 4" COMPACTED AGGREGATE, AS PER PLAN	CY	100
617	RECONDITION SHOULDER, 1.5" ITEM 653 PULVERIZED TOPSOIL, AS PER PLAN	CY	37
644	EDGE LINE, 4", WHITE	MILE	0.84
644	EDGE LINE, 4", YELLOW	MILE	0.18
644	CENTER LINE	MILE	0.51
644	CHANNELIZING LINE, 8"	FT	376
644	CROSSWALK LINE, 24"	FT	352
644	LANE ARROW	EACH	2
644	TRANSVERSE/DIAGONAL LINE	FT	110
644	YIELD LINE	FT	79
644	PAVEMENT MARKING, MISC.: YIELD BAR, WHITE, 24", (2' LONG, 2' GAP)	FT	152
659	SEEDING AND MULCHING, AS PER PLAN	SY	897
SPECIAL	ASPHALT REJUVENATING AGENT	SY	3,638



LOCATION MAP: NTS
COSGRAY RD.
SHIER RINGS RD.

ALTERNATE 1 - COSGRAY RD. IMPROVEMENTS

DESIGN AGENCY



DESIGNER

MSS

REVIEWER

LMO 2/18/25

PROJECT ID

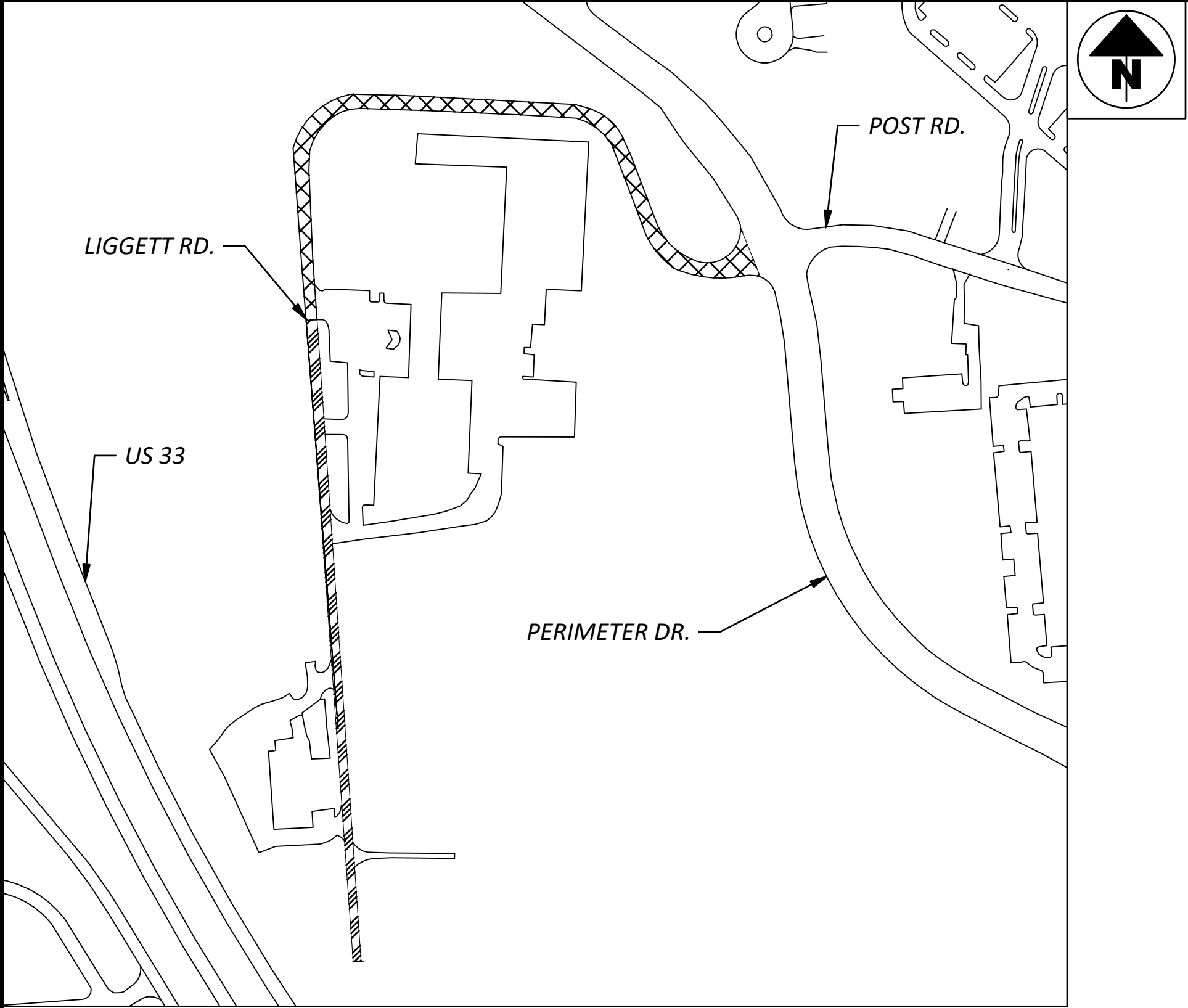
25-002-CIP

SHEET

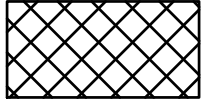
P.44

TOTAL

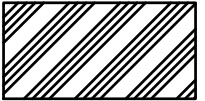
60



LOCATION MAP: NTS
LIGGETT RD.



1.5" MILL AND FILL:
ITEM 254 - 1.5" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT

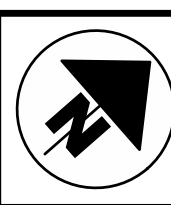
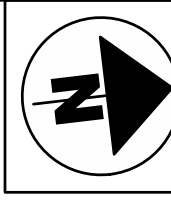
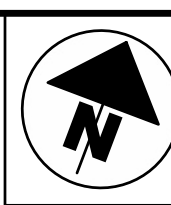


5" MILL AND FILL:
ITEM 254 - 5" PAVEMENT PLANING, ASPHALT CONCRETE
ITEM 407 - NON-TRACKING TACK COAT, 0.1 GAL/SY
ITEM 441 - 3.5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
ITEM 407 - NON-TRACKING TACK COAT, 0.075 GAL/SY
ITEM 441 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
ITEM SPECIAL - ASPHALT REJUVENATING AGENT

SHOULDER TREATMENT (2' WIDE):
ITEM 617 - RECONDITION SHOULDER, 4" COMPACTED
AGGREGATE, AS PER PLAN
ITEM 617 - RECONDITION SHOULDERS, 1.5" ITEM 653
PULVERIZED TOPSOIL, AS PER PLAN
ITEM 659 - SEEDING AND MULCHING, AS PER PLAN

NOTE:
1. FOR ADDITIONAL INFORMATION SEE GIS MAP ([HTTP://TINYURL.COM/DSF6ERWM](http://tinyurl.com/DSF6ERWM)).

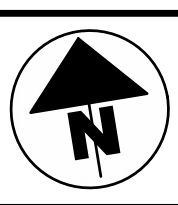
ESTIMATED QUANTITIES LIGGETT RD.			
(5" MILL AND FILL - SOUTHERN TERMINUS TO 1085' NORTH) (1.5" MILL AND FILL - 1085' NORTH OF SOUTHERN TERMINUS TO PERIMETER DR.)			
ITEM	DESCRIPTION	UNIT	QUANTITY
251	PARTIAL DEPTH PAVEMENT REPAIR (441)	SY	13
253	PAVEMENT REPAIR	SY	25
254	PAVEMENT PLANING, ASPHALT CONCRETE (1.5")	SY	3,389
254	PAVEMENT PLANING, ASPHALT CONCRETE (5")	SY	1,868
407	NON-TRACKING TACK COAT	GAL	666
441	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	TON	438
441	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	TON	363
617	RECONDITION SHOULDER, 4" COMPACTED AGGREGATE, AS PER PLAN	CY	116
617	RECONDITION SHOULDER, 1.5" ITEM 653 PULVERIZED TOPSOIL, AS PER PLAN	CY	43
644	EDGE LINE, 4", WHITE	MILE	0.37
644	CENTER LINE	MILE	0.19
644	STOP LINE	FT	22
659	SEEDING AND MULCHING, AS PER PLAN	SY	1,041
SPECIAL	ASPHALT REJUVENATING AGENT	SY	5,257




CURB RAMP DETAILS

DESIGNER	MNR
REVIEWER	LMO 2/18
PROJECT ID	25-002-C
SHEET	TOTAL
P.46	6

O:\2024\2024085\02 Dublin 2025 Street Maintenance\400-Engineering\Roadway\Phase 2\Sheets\2024085 GP200.dwg
2/4/2025



**HORIZONTAL
SCALE IN FEET**



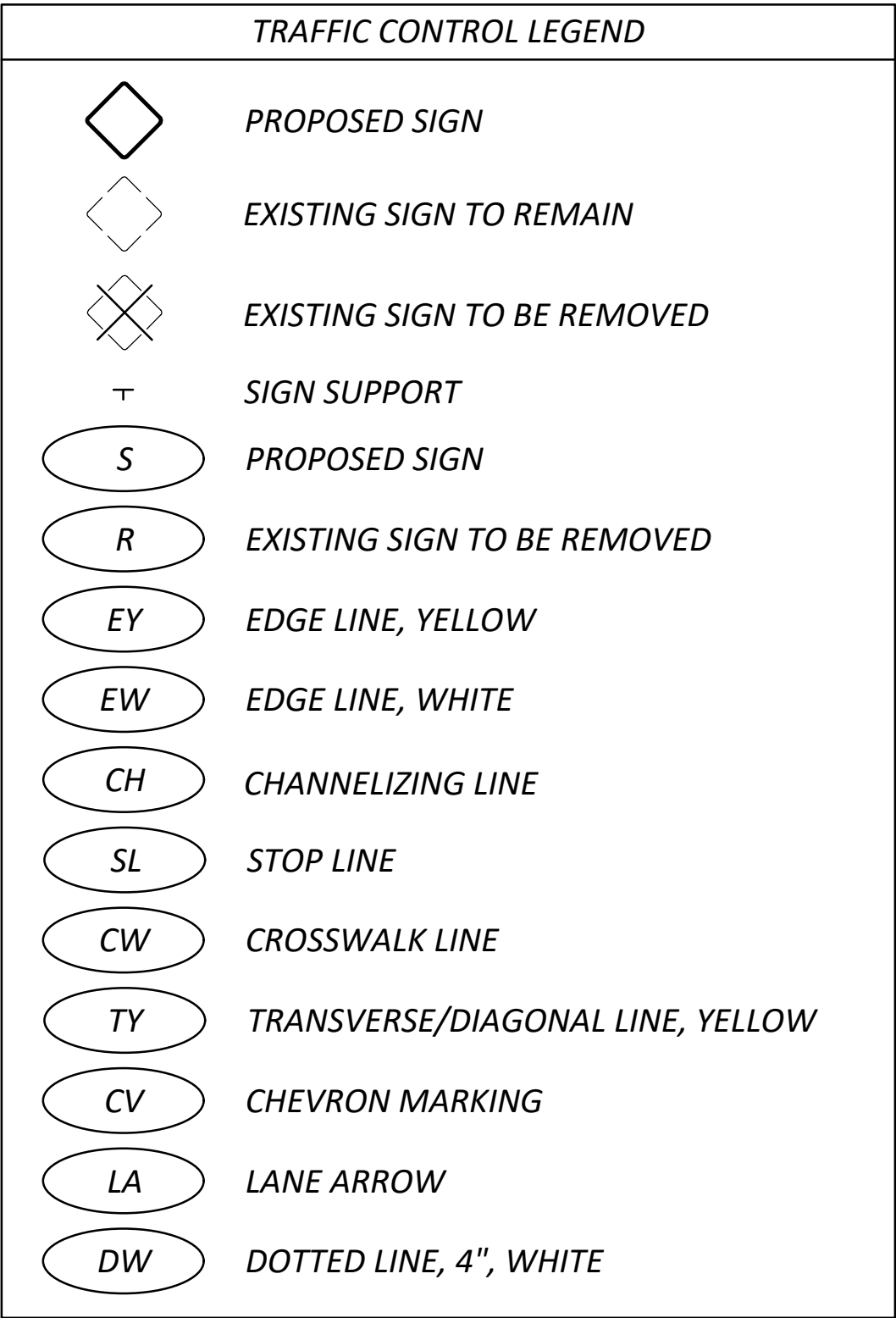
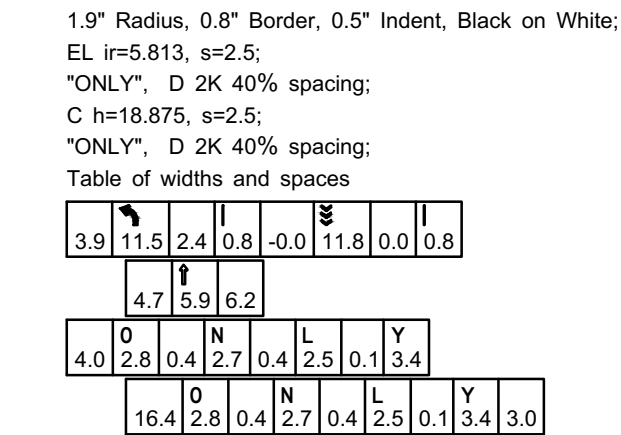
A horizontal scale bar with a black and white checkerboard pattern. The bar is divided into four equal segments. The first segment is labeled '0', the second '5', the third '10', and the fourth '20'.

DESIGN AGENCY

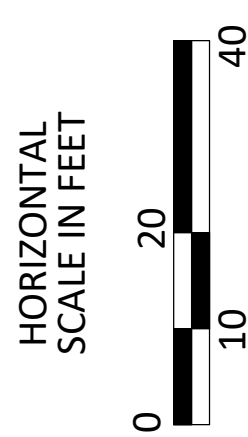


DESIGNER	
MNR	
REVIEWER	
LMO 2/18/25	
PROJECT ID	
25-002-CIP	
SHEET	TOTAL
P.47	60

NOTE: CURB ELEVATIONS ARE
AT FACE OF CURB / FLOW LINE
OF GUTTER



NOTE:
FOR ESTIMATED QUANTITIES, SEE SHEET 34.



DESIGN AGENCY



DLS

AKF 2/18/25

25-002-CIP

P.48	60
------	----

GENERAL TRAFFIC

THE CONTRACTOR SHALL FURNISH AND INSTALL TRAFFIC SIGNAL EQUIPMENT IN CONFORMANCE WITH THE CITY OF DUBLIN SPECIFICATIONS, INCLUDING THE SPECIFICATIONS OF THESE PLANS AS WELL AS IN CONFORMANCE WITH THE LATEST EDITION OF THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS AND ALL SUPPLEMENTAL SPECIFICATIONS.

GUARANTEE

THE CONTRACTOR SHALL GUARANTEE THAT THE TRAFFIC CONTROL SYSTEM INSTALLED AS PART OF THIS CONTRACT SHALL OPERATE SATISFACTORILY FOR A PERIOD OF 90 DAYS FOLLOWING COMPLETION OF THE 10–DAY PERFORMANCE TEST. IN THE EVENT OF UNSATISFACTORY OPERATIONS, THE CONTRACTOR SHALL CORRECT FAULTY INSTALLATIONS, MAKE REPAIRS AND REPLACE DEFECTIVE PARTS WITH NEW PARTS FROM THE SAME MANUFACTURER.

EQUIPMENT, MATERIAL AND LABOR COSTS INCURRED IN CORRECTING THE UNSATISFACTORY OPERATIONS SHALL BE BORNE BY THE CONTRACTOR.

THE GUARANTEE SHALL COVER THE FOLLOWING ITEMS OF THE TRAFFIC CONTROL SYSTEM: VIDEO DETECTION EQUIPMENT, PEDESTRIAN SIGNAL HEADS AND PUSHBUTTONS.

CUSTOMARY MANUFACTURER’S GUARANTEES FOR THE FOREGOING ITEMS SHALL BE TURNED OVER TO THE CITY OF DUBLIN FOLLOWING ACCEPTANCE OF THE EQUIPMENT.

THE COST OF GUARANTEEING THE TRAFFIC CONTROL SYSTEM WILL BE INCIDENTAL TO AND INCLUDED IN THE CONTRACT UNIT PRICE OF THE VARIOUS ITEMS MAKING UP THE SYSTEM.

WIRING DIAGRAM

TWO (2) EACH WIRING DIAGRAMS AND TWO (2) EACH SERVICE/OPERATION MANUALS FOR EACH DIFFERENT PIECE OF EQUIPMENT SHALL BE PROVIDED. A HEAVY CLEAR PLASTIC ENVELOPE ATTACHED TO THE INSIDE OF THE CABINET DOOR SHALL BE PROVIDED FOR STORING WIRING DIAGRAMS. (MINIMUM OF 12” BY 18” IN SIZE)

SIGNAL ACTIVATION

PRIOR TO ACTIVATING THE NEW TRAFFIC SIGNAL TO STOP–AND–GO MODE AND/OR REMOVING THE EXISTING TRAFFIC SIGNAL FROM SERVICE, ALL ITEMS IN THE PROPOSED SIGNAL PLAN SHALL BE FULLY COMPLETED, (I.E., VEHICLE DETECTION, PEDESTRIAN SIGNAL HEADS, ETC.). IF THERE ARE CONSTRUCTABILITY ISSUES (I.E., ROADWAY WIDENING, ETC.) THAT PREVENT THE SIGNAL FROM BEING COMPLETED PRIOR TO ACTIVATION, IT SHALL BE BROUGHT TO THE ATTENTION OF THE PROJECT ENGINEER AND CITY OF DUBLIN. THE PROJECT ENGINEER AND THE CITY OF DUBLIN WILL THEN REVIEW, APPROVE OR REJECT PROPOSALS TO ACTIVATE THE TRAFFIC SIGNAL PRIOR TO COMPLETION.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AND CITY OF DUBLIN AT LEAST 10 WORKING DAYS PRIOR TO SCHEDULING THE FINAL INSPECTION OF THE SIGNAL INSTALLATION. FINAL INSPECTION IS NOT CONSIDERED COMPLETE UNTIL DESIGNATED PERSONNEL INSPECT THE TRAFFIC SIGNAL AND ISSUE WRITTEN APPROVAL. IF ISSUES ARE FOUND DURING THE FINAL INSPECTION THAT EFFECT THE SAFETY OF THE TRAVELING PUBLIC AND/OR THE EFFICIENCY OF THE INTERSECTION, THE SIGNAL SHALL NOT BE ACTIVATED ON THE PROPOSED DATE. ANY PUNCH LIST ITEMS THAT ARE FOUND SHALL BE CORRECTED AND REINSPECTED BY DESIGNATED PERSONNEL PRIOR TO FINAL ACCEPTANCE. THE CITY OF DUBLIN SHALL ONLY ASSUME DAY TO DAY MAINTENANCE OF THE TRAFFIC SIGNAL AFTER FINAL WRITTEN ACCEPTANCE HAS BEEN ISSUED.

WORK INSPECTION

THE CONTRACTOR SHALL PROVIDE THE PROJECT ENGINEER AND CITY OF DUBLIN WITH 72–HOUR NOTICE OF ANY SIGNAL WORK TO BE PERFORMED AT THE INTERSECTION SITE(S) SO THAT INSPECTION SERVICES CAN BE SUPPLIED.

DETECTION MAINTENANCE

IF VEHICLE DETECTION BECOMES UNEXPECTEDLY DISABLED, REQUIRES MODIFICATION, OR IS SCHEDULED TO BE TEMPORARILY REMOVED DURING THE CONSTRUCTION PROJECT, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT ENGINEER AND THE CITY OF DUBLIN.

IF THE LOSS OF VEHICLE DETECTION IS KNOWN PRIOR TO THE START OF CONSTRUCTION, IT SHALL BE DISCUSSED AT THE PRECONSTRUCTION MEETING. AT SUCH TIME, THE CITY OF DUBLIN SHALL ADVISE THE PROJECT ENGINEER AND CONTRACTOR ON THE APPROPRIATE ACTION TO RECTIFY ANY LOSS OF VEHICLE DETECTION. THIS MAY INCLUDE PLACING THE TRAFFIC SIGNAL ON MINIMUM OR MAXIMUM RECALL, MODIFYING THE MINIMUM GREEN TIMES, AND REMOVING THE MALFUNCTIONING DETECTION FROM SERVICE. WHERE NON–INTRUSIVE DETECTION (I.E. VIDEO, RADAR) ALREADY EXISTS, THE CONTRACTOR SHALL INSURE THAT DETECTION IS OPERATING AND MAINTAINED BY RECONFIGURING THE DETECTION UNITS ACCORDINGLY DURING ALL CONSTRUCTION PHASES. THIS IS TO AVOID THE SIGNAL FROM MAXING OUT THE EFFECTED SIGNAL PHASE AND CREATING UNNECESSARY DELAYS.

LOCATIONS WHERE NON–INTRUSIVE DETECTION IS PROPOSED AND THE EXISTING VEHICLE DETECTION IS TO BE ABANDONED, THE NON–INTRUSIVE VEHICLE DETECTION SHALL BE INSTALLED, CONFIGURED AND MADE FULLY FUNCTIONAL PRIOR TO THE EXISTING DETECTION BEING DISABLED. THE CONTRACTOR SHALL CONTINUE TO MAINTAIN AND MODIFY THE DETECTION UNTIL FINAL ACCEPTANCE OF THE TRAFFIC SIGNAL. THIS IS TO ENSURE VEHICLE DETECTION REMAINS FULLY FUNCTIONAL THROUGHOUT CONSTRUCTION.

CABLE AND WIRE

IN ADDITION TO ODOT STANDARD SPECIFICATION 632 AND 725 THE FOLLOWING SHALL ALSO APPLY:

ALL WIRE SPLICES SHALL BE WATERPROOF SPLICES, BUTT SPLICES ARE NOT ACCEPTABLE.

SIGNAL CABLE JACKETS SHALL NOT BE REMOVED OR PULLED BACK LEAVING EXPOSED WIRES OR COLORED JACKETS OUTSIDE OF SIGNAL HEADS. JACKETS ARE TO BE ONLY PULLED BACK ONCE CABLE HAS BEEN PULLED INTO SIGNAL HEAD. THREE FEET OF SLACK IS REQUIRED (DRIP LOOP) IN CABLE BETWEEN SIGNAL HEADS.

ALL CABLES ENTERING AND EXITING A PULL BOX SHALL BE UNIQUELY COLOR CODED TO TRACE THE WIRE THROUGH THE INTERSECTION; COLORS SHALL BE NOTED ON SIGNAL PLANS IN THE SIGNAL CABINET.

GROUNDING AND BONDING

THE REQUIREMENTS OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS (C&MS) AND THE TC SERIES OF STANDARD CONSTRUCTION DRAWINGS ARE MODIFIED AS FOLLOWS:

1. ALL METALLIC PARTS CONTAINING ELECTRICAL CONDUCTORS SHALL BE PERMANENTLY JOINED TO FORM AN EFFECTIVE GROUND FAULT CURRENT PATH BACK TO THE GROUNDED CONDUCTOR IN THE POWER SERVICE DISCONNECT SWITCH.
- A. PROVIDE AN EQUIPMENT GROUNDING CONDUCTOR IN METALLIC CONDUITS (725.04) IN ADDITION TO THE CONDUCTORS SPECIFIED AND BOND THE CONDUIT TO THIS GROUNDING CONDUCTOR.
- B. WHEN AN EQUIPMENT GROUNDING CONDUCTOR IS REQUIRED IN PLASTIC CONDUIT (725.05), THE INSTALLATION SHALL INCLUDE A SEPARATE EQUIPMENT GROUNDING CONDUCTOR IN ADDITION TO THE CONDUCTORS SPECIFIED.
- C. METALLIC CONDUIT CARRYING THE LOOP WIRES FROM IN THE PAVEMENT TO THE PULL BOX SPLICE LOCATION WILL ONLY BE BONDED AT THE PULL BOX END, AND WILL NOT CONTAIN AN EQUIPMENT GROUNDING CONDUCTOR.
- D. IF MULTIPLE CONDUIT RUNS BEGIN AND END AT THE SAME POINTS, ONLY ONE EQUIPMENT GROUNDING CONDUCTOR IS REQUIRED.
- E. IF AN EQUIPMENT GROUNDING CONDUCTOR IS NEEDED IN CONDUIT BETWEEN SIGNALIZED INTERSECTIONS FOR UNDERGROUND INTERCONNECT CABLE, THE GROUNDING SYSTEM FOR EACH SIGNALIZED INTERSECTION WILL BE SEPARATED ABOUT MIDWAY BETWEEN THE INTERSECTIONS.
- F. THE MESSENGER WIRE AT SIGNALIZED INTERSECTIONS WILL BE USED AS THE CONDUCTIVE PATH FROM CORNER TO CORNER IF CONDUIT IS NOT PROVIDED UNDER THE ROADWAY. WHEN CONDUIT CONNECTS THE CORNERS OF AN INTERSECTION, AN EQUIPMENT GROUNDING CONDUCTOR SHALL BE USED IN THE CONDUIT.
2. CONDUITS.
- A. THE 725.04 CONDUIT SHALL HAVE GROUNDING BUSHINGS INSTALLED AT ALL TERMINATION POINTS.THE BUSHING MATERIAL SHALL BE COMPATIBLE WITH GALVANIZED STEEL CONDUIT AND THE GROUNDING LUG MATERIAL SHALL BE COMPATIBLE FOR USE WITH COPPER WIRE. THREADED OR COMPRESSION TYPE BUSHINGS MAY BE USED.
- B. THE 725.05 CONDUIT SHALL HAVE THE INSIDE AND OUTSIDE DIAMETERS OF THE CONDUIT DEBURRED AT ALL TERMINATION POINTS.
- C. BOTH ENDS OF METALLIC CONDUIT SHALL BE BONDED TO THE EQUIPMENT GROUNDING CONDUCTOR.
- D. METALLIC CONDUIT MAY BE BONDED TO METALLIC BOXES THROUGH THE USE OF CONDUIT FITTINGS UL APPROVED FOR THIS TYPE OF CONNECTION, WITH THE BOX BONDED TO THE EQUIPMENT GROUNDING CONDUCTOR.
3. WIRE FOR GROUNDING AND BONDING.
- A. USE INSULATED, COPPER WIRE FOR THE EQUIPMENT GROUNDING CONDUCTOR. BONDING JUMPERS IN BOXES AND ENCLOSURES MAY BE BARE OR INSULATED COPPER WIRE. WIRE SIZE SHALL BE AS FOLLOWS:
- i. USE 4 AWG BETWEEN THE POWER SERVICE AND SUPPORTS, POLES, PEDESTALS, CONTROLLER OR FLASHER CABINETS.
- ii. USE A MINIMUM 8 AWG BETWEEN LOOP DETECTOR PULL BOXES AND THE FIRST CONDUIT THAT REQUIRES A LARGER SIZE AS SPECIFIED IN 3.A.I ABOVE.
- iii. USE A MINIMUM 8 AWG BETWEEN THE "PREPARE TO STOP WHEN FLASHING" INSTALLATION (INCLUDING SUPPORT) AND THE FIRST CONDUIT THAT REQUIRES A LARGER SIZE AS SPECIFIED IN 3.A.I ABOVE.
- iv. THE INSULATION SHALL BE GREEN OR GREEN WITH YELLOW STRIPE(S). FOR 4 AWG OR LARGER, INSULATION MAY ALSO BE BLACK WITH GREEN TAPE/LABELS INSTALLED AT ALL ACCESS POINTS.
- B. IN A HIGHWAY LIGHTING SYSTEM, THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE THE SAME WIRE SIZE AS THE DUCT CABLE OR DISTRIBUTION CABLECIRCUIT CONDUCTORS, WITH THE MINIMUM CONDUCTOR SIZE OF 4 AWG. BONDING JUMPERS WILL BE MINIMUM SIZE 4 AWG.

GROUNDING AND BONDING (CONT.)

4. GROUND ROD.
- A. A 3/4–INCH SCHEDULE 40 PVC CONDUIT WILL BE USED IN FOUNDATIONS AND CONCRETE WALLS FOR THE GROUNDING CONDUCTOR (GROUND WIRE) RACEWAY TO THE GROUND ROD. SHOULD METALLIC CONDUIT BE USED, BOTH ENDS OF THE CONDUIT SHALL BE BONDED TO THE GROUNDING CONDUCTOR.
- B. THE TYPICAL GROUNDING CONDUCTOR (GROUND WIRE) SHALL BE 4 AWG INSULATED, COPPER.
5. THE GREEN CONDUCTOR IN SIGNAL CABLES (CONDUCTOR #4) SHALL NOT BE USED TO SUPPLY POWER TO A SIGNAL INDICATION. IT WILL BE CONNECTED TO THE SIGNAL BODY AS AN EQUIPMENT GROUND IN ALUMINUM HEADS AND IT WILL BE UNUSED IN PLASTIC HEADS. UNUSED CONDUCTORS SHALL BE GROUNDED IN THE CABINET. TYPICAL USE OF CONDUCTORS IS AS FOLLOWS:
- | COND. NO. | COLOR | VEHICLE SIGNAL | PEDESTRIAN SIGNAL |
|-----------|--------------------|------------------|-------------------|
| 1 | BLACK | GREEN BALL | #1 WALK |
| 2 | WHITE | AC NEUTRAL | AC NEUTRAL |
| 3 | RED | RED BALL | #1 DW/FDW |
| 4 | GREEN | EQUIPMENT GROUND | EQUIPMENT GROUND |
| 5 | ORANGE | YELLOW BALL | #2 DW/FDW |
| 6 | BLUE | GREEN ARROW | #2 WALK |
| 7 | WHITE/BLACK STRIPE | YELLOW ARROW | NOT USED |
6. POWER SERVICE AND DISCONNECT SWITCH.
- A. AT THE POWER SERVICE LOCATION, THE GROUNDING CONDUCTOR (GROUND WIRE) FROM THE DISCONNECT SWITCH NEUTRAL (AC–) BAR TO THE GROUND ROD SHALL BE A CONTINUOUS, UNSPLICED CONDUCTOR. IF SPLICED, IT SHALL BE AN EXOTHERMIC WELD BUTT SPLICE.
- B. THE SERVICE NEUTRAL (AC–) SHALL ONLY BE CONNECTED TO GROUND AT THE PRIMARY POWER SERVICE DISCONNECT SWITCH.
- i. NEMA CONTROLLER CABINETS: IF A POWER SERVICE DISCONNECT SWITCH IS LOCATED BEFORE THE CONTROLLER CABINET, THE NEUTRAL (AC–) AND THE GROUNDING BARS IN THE CONTROLLER CABINET SHALL NOT BE CONNECTED TOGETHER AS SHOWN IN NEMA TS–2, FIGURE 5–4.
- ii. IF SECONDARY DISCONNECT SWITCHES ARE CONNECTED AFTER THE PRIMARY DISCONNECT SWITCH, THE NEUTRAL (AC–) SHALL ONLY BE GROUNDED AT THE PRIMARY SWITCH. EQUIPMENT GROUNDING CONDUCTORS SHALL BE BROUGHT TO THE PRIMARY SWITCH, BUT SHALL BE GROUNDED AT BOTH SECONDARY AND PRIMARY SWITCHES.
7. PAYMENT – ALL MATERIALS AND WORK REQUIRED TO COMPLETE THE EFFECTIVE GROUND FAULT CURRENT PATH SYSTEM ARE INCIDENTAL TO THE CONDUCTORS INSTALLED BY CONTRACT.

DESIGN AGENCY



DESIGNER

JAH

REVIEWER

KMG 2/18/25

PROJECT ID

25-002-CIP

SHEET

P.49

TOTAL

60

ITEM 625 – UNDERGROUND WARNING/MARKING TAPE, AS PER PLAN

IN ADDITION TO ITEM 625 AND 725 OF THE ODOT STANDARD SPECIFICATIONS, THE FOLLOWING SHALL ALSO APPLY:

THE TAPE SHALL BE THREE (3”) INCH YELLOW PLASTIC CAUTION TAPE. THE TAPE SHALL BE INSTALLED OVER ALL 725.05 CONDUIT, OUTSIDE OF DRIVEWAYS AND ROADWAYS. AS PER CITY SPECIFICATIONS.

PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE FOR THIS ITEM, COMPLETE AND IN PLACE.

ITEM 625 – TRENCH, AS PER PLAN

IN ADDITION TO ODOT ITEM 625, THE TRENCH SIZE, DEPTH, AND BACKFILL SHALL MEET CITY SPECIFICATIONS. IT IS ALSO THE INTENT OF THIS ITEM THAT CONDUIT TO BE LOCATED UNDER NEW ASPHALT/CONCRETE SHALL BE INSTALLED PRIOR TO PLACEMENT OF THE ASPHALT/CONCRETE, AND THAT COMPACTED ITEM 304 – BACKFILL, OR ITEM 636 – TYPE 2 OR 3 SHALL BE USED AT THOSE LOCATIONS (AS PER CITY SPECIFICATIONS). NO CONCRETE ENCASEMENT SHALL BE PROVIDED. THE COST OF THE BACKFILL SHALL BE INCLUDED IN THE UNIT COST BID FOR THIS ITEM. THE COST OF THE UNDERGROUND WARNING/MARKING TAPE SHALL BE PAID AS A SEPARATE ITEM.

PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER LINEAR FOOT.

ITEM 625 – TRENCH IN PAVED AREA, AS PER PLAN

IN ADDITION TO ODOT ITEM 625, THE TRENCH SIZE, DEPTH, AND BACKFILL SHALL MEET CITY SPECIFICATIONS.

THE CONTRACTOR SHALL PERFORM ALL RESTORATION WITH MATERIALS IDENTICAL TO THE EXISTING SURFACE, INCLUDING, BUT NOT LIMITED TO ASPHALT, CONCRETE AND BRICK SIDEWALK, INTEGRAL CURB, AND SPECIAL SURFACES (SUCH AS COLORED OR TEXTURED) AS ENCOUNTERED. PATCHING OF CONCRETE SIDEWALKS AND DRIVEWAYS SHALL NOT BE PERMITTED. THE CONTRACTOR SHALL REPLACE SIDEWALKS AND DRIVEWAYS IN ENTIRE ORIGINAL SLAB SECTIONS.

THIS ITEM SHALL INCLUDE ALL LABOR AND MATERIAL COSTS ASSOCIATED WITH THE PROVISION AND RESTORATION OF THE EXISTING SURFACE AS OUTLINED ABOVE. PAYMENT FOR THIS WORK SHALL BE AT THE CONTRACT UNIT PRICE AND WILL BE FULL COMPENSATION FOR ALL RESTORATION WORK, INCLUDING MATERIALS, EQUIPMENT, LABOR, INCIDENTALS, AND DISPOSAL OF ALL SURPLUS MATERIALS. THE COST OF THE UNDERGROUND WARNING/MARKING TAPE SHALL BE PAID AS A SEPARATE ITEM. PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER LINEAR FOOT.

ITEM 625 – CONDUIT, JACKED OR DRILLED, 725.051, AS PER PLAN, 3”

ALL UNDERGROUND CONDUIT UTILIZED FOR STREET CROSSINGS SHALL MEET THE REQUIREMENTS OF ODOT 725.051, EXCEPT THE CONDUIT SHALL BE SCHEDULE 80 IN LIEU OF SCHEDULE 40 PVC CONDUIT.

PAYMENT SHALL BE MADE AT THE UNIT BID PRICE AND WILL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS, TOOLS, EQUIPMENT, TEST HOLES, SURFACE REPAIR AND RESTORATION AND OTHER INCIDENTALS NECESSARY FOR EACH STREET CROSSING.

ITEM 632 – ACCESSIBLE PEDESTRIAN PUSHBUTTON, AS PER PLAN

ALL PUSHBUTTONS SHALL BE AN ACCESSIBLE PEDESTRIAN SIGNAL TYPE, POLARA BRAND iNS – NEXT GENERATION iNAVIGATOR 2–WIRE PUSH BUTTON STATION WITH iDETECT OPTION, IN YELLOW WITH A YELLOW BUTTON COVER (MODEL iDS23TNO–Y), OR APPROVED EQUAL. THE BODY HOUSING SHALL BE YELLOW IN COLOR. LOCATION AND PLACEMENT OF PUSHBUTTONS TO BE FIELD LOCATED BY THE ENGINEER. ONE SIGN (R10–3i–mod–9) PER PUSHBUTTON SHALL BE PROVIDED. THE CONTRACTOR SHALL PROVIDE ONE (1) POLARA BRAND iINTELLIGENT CENTRAL CONTROL UNIT PER SIGNALIZED INTERSECTION AS OUTLINED IN THE PLANS. THE CENTRAL CONTROL UNIT WILL BE PAID FOR UNDER A SEPARATE PAY ITEM AS SHOWN ON THE TRAFFIC SIGNAL NOTES.

THE PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER EACH, COMPLETE AND IN PLACE, INCLUDING ALL CONNECTIONS TESTED AND ACCEPTED.

ITEM 632 – SIGNALIZATION, MISC.: ACCESSIBLE PEDESTRIAN PUSHBUTTON CENTRAL CONTROL UNIT

CONTRACTOR SHALL PROVIDE AND INSTALL A POLARA BRAND INTELLIGENT CENTRAL CONTROL UNIT, SHELF MOUNT (MODEL iCCS–S2), OR APPROVED EQUAL. THE CENTRAL CONTROL UNIT SHALL BE COMPATIBLE WITH THE PEDESTRIAN PUSH BUTTON, AS PER PLAN UNITS APPROVED AND PROVIDED. THE UNIT POWER SUPPLY, ALL NECESSARY CONNECTOR CABLES, THE INTERCONNECT BOARD AND PROGRAMMING BY THE VENDOR SHALL BE PROVIDED AND PAID FOR UNDER THIS PAY ITEM. ONE CENTRAL CONTROL UNIT IS REQUIRED PER SIGNALIZED INTERSECTION WITH POLARA BRAND iN2 – iNAVIGATOR 2–WIRE PUSH BUTTON STATION.

THE PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER EACH, COMPLETE AND IN PLACE, INCLUDING ALL CONNECTIONS TESTED AND ACCEPTED.

ITEM 632 – REMOVAL OF TRAFFIC SIGNAL INSTALLATION, AS PER PLAN

TRAFFIC SIGNAL INSTALLATIONS, INCLUDING SIGNAL HEADS, CABLE, PUSHBUTTONS, PULL BOXES, ETC., SHALL BE REMOVED IN ACCORDANCE WITH C&MS 632.26 AND AS INDICATED ON THE PLANS. REMOVED ITEMS SHALL BE STORED ON THE PROJECT FOR SALVAGE AND BE DELIVERED TO THE CITY OF DUBLIN TRAFFIC SIGNAL ENGINEER, AT 6555 SHIER RINGS ROAD, DUBLIN OHIO 43016.

THE CONTRACTOR SHALL REMOVE ALL EXISTING LOOP DETECTOR CABLES AND REMAINING EMPTY PULL BOXES. ALL VOIDS REMAINING FROM PULL BOX REMOVAL SHALL BE FILLED AND THE SURFACE AREA RESTORED TO EQUAL OR BETTER CONDITION OF THE ADJACENT AREA. SURFACE RESTORATION SHALL BE CONSIDERED INCIDENTAL TO THE REMOVAL OF TRAFFIC SIGNAL INSTALLATION, AS PER PLAN PAY ITEM.

THE CONTRACTOR SHALL TAKE GREAT CARE AS TO NOT DAMAGE ANY SIGNAL EQUIPMENT NOT LABELED FOR REMOVAL. IF IT IS DETERMINED BY THE ENGINEER THAT ANY EXISTING EQUIPMENT HAS BEEN DAMAGED DUE TO CONTRACTOR NEGLIGENCE, THE ITEM SHALL BE REPLACED IN LIKE KIND AT THE CONTRACTOR’S EXPENSE.

ALL HOLES IN THE SIGNAL SUPPORTS ASSOCIATED WITH THE REMOVAL OF EXISTING SIGNAL EQUIPMENT SHALL BE PLUGGED IN THEIR ENTIRETY AND PAINTED TO MATCH THE FINISH OF THE SIGNAL SUPPORTS. PROPER GALVANIC PRIMING SHALL BE PERFORMED AFTER THE HOLES ARE PLUGGED AND PRIOR TO THE FINISH COAT OF PAINT. THE CONTRACTOR SHALL CONSULT WITH VALMONT INDUSTRIES ON THE PROPER AND RECOMMENDED PROCEDURE FOR GALVANIC PRIMING AND FINAL FINISHING.

THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL ITEMS ON THE PROJECT NOT MARKED FOR SALVAGE AND DELIVER TO THE CITY AT NO ADDITIONAL COST TO THE PROJECT.

ITEM 632 – TEST HOLE PERFORMED

IT IS ANTICIPATED THAT THE CONTRACTOR WILL ENCOUNTER UNDERGROUND UTILITIES WHILE EXCAVATING FOR SIGNAL SUPPORT FOUNDATIONS OR SIMILAR FOUNDATIONS. AFTER ACCURATELY IDENTIFYING THE PROPOSED LOCATION OF THE FOUNDATION, AS SHOWN IN THE PLANS AND AFTER MODIFYING THAT LOCATION, IF NECESSARY, BASED ON THE FIELD MARKING OF UNDERGROUND UTILITY LOCATION, THE CONTRACTOR DISCOVERS A UTILITY CONFLICT DURING THE EXCAVATION OPERATION, THE CONTRACTOR WILL BE COMPENSATED FOR EACH PARTIAL FOUNDATION EXCAVATION ACCORDING TO THE BID PRICE.

BEFORE THE CONTRACTOR BEGINS THE EXCAVATION AT THE MODIFIED LOCATION, THE CONTRACTOR SHALL VERIFY THAT THERE WILL BE NO OVERHEAD UTILITY CONFLICTS RESULTING FROM THE NEW SIGNAL SUPPORT LOCATION. NEW SUPPORT LOCATIONS ARE TO BE APPROVED BY THE ENGINEER.

THE WORK WILL INCLUDE BACKFILLING, COMPACTING, AND RESTORATION OF THE EXCAVATION TO THE SITE’S ORIGINAL CONDITION.

EXCAVATIONS SHALL NOT BE LEFT OPEN OVERNIGHT.

PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER EACH ITEM 632 – TEST HOLE PERFORMED. A QUANTITY OF 1 HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AT THE DIRECTION OF THE ENGINEER.

ITEM 632 – PEDESTAL, 5’, TRANSFORMER BASE, AS PER PLAN

ALL PEDESTALS SHALL BE MANUFACTURED BY VALMONT OR AN APPROVED EQUAL. IN ADDITION TO THE REQUIREMENTS OF ODOT C&MS ITEMS 632.19 AND 732.15, THE PEDESTAL SHALL BE SUPPLIED WITH THE FOLLOWING FEATURES:

1. THE CONTRACTOR SHALL VERIFY THE ANCHOR BOLT CIRCLE, ANCHOR BOLT DIAMETER AND ORIENTATION PATTERN WITH THE PEDESTAL MANUFACTURER.
2. THE DESIGN SHALL COMPLY WITH 2004 AASHTO DESIGN CRITERIA AND ALL OTHER APPLICABLE ODOT STANDARDS.
3. THE PEDESTAL SHALL BE PROVIDED WITH A TRANSFORMER BASE, 17” TALL.
4. IT IS THE INTENT OF THESE PLANS THAT THE PEDESTALS, TRANSFORMER BASES AND ALL POLE HARDWARE SHALL MATCH THE CITY OF DUBLIN STANDARD FOR POLE PAINT COLOR (DUBLIN BRONZE). PRIOR TO SHIPPING, ALL MATERIALS SHALL BE FACTORY PRIME COATED AND FINISHED. IF DURING TRANSPORTATION, ERECTION AND INSTALLATION OF POLE HARDWARE, OR AT ANYTIME BEFORE FINAL ACCEPTANCE, THE PAINTED SURFACES ARE SCRATCHED OR MARRED IN ANY MANNER, THE CONTRACTOR SHALL BE REQUIRED TO APPLY “TOUCHUP” PAINT OF THE SAME TYPE AS SPECIFIED TO THE AFFECTED AREAS.
5. THE FINISHING PROCESS SHALL BE VALMONT DECA–BOND F–575B PROVIDING A 5 YEAR FINISH WARRANTY OR APPROVED EQUAL PROCESS, THE FINISH SYSTEM SHALL BE A POWDER COAT FINISH.
6. PAINT CHIP SAMPLES MUST BE SUBMITTED TO THE DUBLIN ENGINEER FOR REVIEW AND APPROVAL PRIOR TO ORDERING MATERIALS.
7. THE CONTRACTOR SHALL COORDINATE WITH CITY OF DUBLIN PERSONNEL TO BE ON SITE PRIOR DRILLING/CUTTING OR WELDING INTO PEDESTAL SUPPORTS, WHICH INCLUDES, BUT IS NOT LIMITED TO, THE PLACEMENT OF PEDESTRIAN PUSHBUTTONS.

PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER EACH, COMPLETE AND IN PLACE, ALL CONNECTIONS TESTED AND ACCEPTED.

ITEM 816 – VIDEO DETECTION SYSTEM, AS PER PLAN

UNDER THIS ITEM OF WORK THE CONTRACTOR SHALL FURNISH AND INSTALL A COMPLETE ITERIS VANTAGE NEXT WITH VANTAGE VECTOR HYBRID VIDEO DETECTION SYSTEM. THE VIDEO DETECTION SYSTEM SHALL INCLUDE THE APPROPRIATE NUMBER OF HYBRID IMAGE SENSOR UNITS, AS SPECIFIED BELOW, ALL MOUNTING HARDWARE, ALL CABLING/HARNESSES, AND CONTROLLER CABINET COMPONENT DEVICES (CAMERA INPUT PROCESSOR AND SHELF MOUNTED POWER SUPPLY). THE PROPOSED SYSTEM SHALL BE DESIGNED TO OPERATE ON A 120VAC, 60 HZ INCOMING CONTROLLER CABINET POWER LINE. VIDEO DETECTION ZONES SHALL BE ESTABLISHED FOR EACH INTERSECTION LEG AS DETAILED IN THE PLANS.

THE VIDEO DETECTION SYSTEM AT EACH PROJECT INTERSECTION SHALL INCLUDE THE FOLLOWING COMPONENT ITEMS AT A MINIMUM:

4 EACH: – VANTAGE NEXT WITH VANTAGE VECTOR HYBRID VIDEO DETECTION CAMERA, 120 VAC COMPLETE WITH 2 FOOT ASTRO BRAC VERTICAL RISERS WITH A STAINLESS STEEL CABLE BAND ATTACHMENT

4 EACH: – VANTAGE CAMERA POWER CONNECTOR CABLE PIGTAIL 18”

1 PER 4 CAMERAS: – VANTAGE EDGE2 FOUR (4) CAMERA INPUT PROCESSOR – INCLUDES PRINT MARKUP AND TECH SUPPORT

1 EACH: – POWER SUPPLY TS1 RACK MOUNTED 1.5 AMP VANTAGE VIDEO

200 LIN. FT. PER CAMERA: –VIDEO COMPOSITE DETECTION & POWER CABLE WITH 600 VOLT INSULATION, INCLUDING CONNECTORS

1 EACH: – SURGE PROTECTED POWER PANEL FOR ITERIS VIDEO DETECTION SYSTEM

1 EACH: – VIDEO SECURITY MONITOR, 9” LED FLATSCREEN

1 EACH: VANTAGE VECTOR MODULE

ALL VIDEO DETECTION EQUIPMENT EXCEPT THE IMAGE SENSOR UNITS (CAMERAS) SHALL BE HOUSED IN THE CONTROLLER CABINET. ALL DEVICES SHALL BE MOUNTED SO ALL CABLE CONNECTIONS ARE ACCESSIBLE AND ALL DEVICE DOORS CAN BE FULLY OPENED FOR SERVICING. SHELF MOUNTED DEVICES SHALL BE POSITIONED SO THEY ARE ACCESSIBLE AND DO NOT INTERFERE WITH OTHER CABINET DEVICES. VENDOR SHALL PROVIDE A FIVE (5) YEAR WARRANTY ON ALL PARTS OF THE VIDEO DETECTION SYSTEM.

THE IMAGE SENSOR UNITS SHALL BE MOUNTED ON THE MAST ARM USING A 2 FOOT ASTRO BRAC VERTICAL SUPPORT WITH STAINLESS STEEL CABLE BAND MOUNTING ASSEMBLY. STEEL STRAP BANDING IS NOT PERMITTED. ASTRO BRAC RISER LENGTH SHALL ALLOW A CAMERA MOUNTING HEIGHT OF 4 INCHES ABOVE THE EXISTING INTERNALLY ILLUMINATED STREET NAME SIGN. THE UNIT SHALL BE CENTERED WITH THE OPPOSING APPROACH. THE IMAGE SENSOR MOUNTING ASSEMBLY AND THE IMAGE SENSOR HOUSING INCLUDING THE VISOR SHALL BE FACTORY FINISHED IN A LIGHT GREY COLOR. THE COMPOSITE VIDEO DETECTION & POWER CABLE SHALL BE RUN CONTINUOUSLY FROM THE IMAGE SENSOR TO THE CONTROLLER CABINET (NO SPLICES).

IN ADDITION TO THE MATERIALS THAT ARE MENTIONED ABOVE OR SHOWN IN THE PLANS, THE CONTRACTOR SHALL ALSO FURNISH THE FOLLOWING DEVICES TO CITY TRAFFIC ENGINEER. THESE DEVICES SHALL BE PROVIDED PER PROJECT AND THE COST OF THE DEVICES SHALL BE INCIDENTAL TO THE COST OF THIS VIDEO DETECTION SYSTEM.

1 EACH: VANTAGE VECTOR HYBRID VIDEO DETECTION CAMERA, 120VAC (COMPLETE UNIT)

1 EACH: VANTAGE EDGE2 FOUR (4) CAMERA INPUT PROCESSOR

VEHICLE DETECTION ZONES ARE SHOWN IN THE PLANS TO ENSURE THAT ALL VEHICLE DETECTION NEEDS ARE MET BUT MAY BE EXPANDED OR MODIFIED AS NEEDED BY THE CONTRACTOR. THE CONTRACTOR SHALL PROVIDE AN AS–BUILT SET OF DRAWINGS FOR THE VIDEO DETECTION ZONES PER INTERSECTION AND SHALL PROVIDE AN ITERIS BACK UP FILE FOR EACH INTERSECTION. SAID INFORMATION SHALL BE PROVIDED TO THE CITY TRAFFIC ENGINEER.

PAYMENT FOR ALL OF THE ABOVE WILL BE MADE AT THE CONTRACT UNIT PRICE BID, INCLUDING ALL LABOR, MATERIALS AND APPURTENANCES FOR EACH COMPLETE VIDEO VEHICLE DETECTION SYSTEM, INSTALLED TESTED AND ACCEPTED.



GPD GROUP
Claus, Pyle, Schorner, Burris & Detwiler, Inc.
Consulting Engineers, Architects, Planners & Scientists, Inc.

DESIGNER	
JAH	
REVIEWER	
KMG 2/18/25	
PROJECT ID	
25-002-CIP	
SHEET	TOTAL
P.50	60

SHEET	LOCATION	625	625	625	625	625	625	625	632	632	632	632	632	632	816										
		CONDUIT, 2", 725.051	CONDUIT, 3", 725.051	CONDUIT, JACKED OR DRILLED, 725.051, AS PER PLAN, 3"	TRENCH, AS PER PLAN	TRENCH IN PAVED AREA, AS PER PLAN	GROUND ROD	UNDERGROUND WARNING/MARKING TAPE, AS PER PLAN	ACCESSIBLE PEDESTRIAN PUSHBUTTON, AS PER PLAN	SIGNALIZATION, MISC.: ACCESSIBLE PEDESTRIAN PUSHBUTTON CENTRAL CONTROL UNIT	PEDESTAL FOUNDATION	SIGNAL CABLE, 2 CONDUCTOR, NO. 14 AWG	PEDESTAL, 5', TRANSFORMER BASE, AS PER PLAN	REMOVAL OF TRAFFIC SIGNAL INSTALLATION, AS PER PLAN	VIDEO DETECTION SYSTEM, AS PER PLAN										
		FT	FT	FT	FT	FT	EACH	FT	EACH	EACH	EACH	FT	EACH	EACH	EACH										
52	WOERNER TEMPLE RD. / AVERY RD.	23	66	275	45	44	1	89	8	1	1	1182	1	1	1										
54	WOERNER TEMPLE RD. / EMERALD PKWY.								8	1		1624		1	1										
56	N. HIGH ST. / ROCK CRESS PKWY.								6	1		1016		1	1										
TOTALS CARRIED TO GENERAL SUMMARY		23	66	275	45	44	1	89	22	3	1	3822	1	3	3										

NOTE:

- PEDESTRIAN SIGNAL HEAD, PUSHBUTTONS, AND ASSOCIATED SIGNS SHALL BE MOUNTED AS PER SCD TC-85.10 ON THE SUPPORT POLE.
- CONDUIT LOCATIONS SHALL BE CONSIDERED APPROXIMATE AND ARE TO BE VERIFIED BY THE CONTRACTOR PRIOR TO INSTALLING ANY UNDERGROUND CABLE.
- NO FIELD SURVEY WAS PERFORMED FOR THIS PROJECT. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS OF ALL UTILITIES AND EXISTING SIGNAL EQUIPMENT. ALL EXISTING CONDUIT LOCATIONS SHALL BE CONSIDERED APPROXIMATE AND ARE TO BE VERIFIED BY THE CONTRACTOR PRIOR TO EXCAVATION AND INSTALLING ANY UNDERGROUND CABLE.

(1)-3" CONDUIT W/ (2)-2C AND (1)-VIDEO
DIRECTIONALLY DRILLED = 86'

EX PB-1 (TO REMAIN)

(1)-3" CONDUIT W/ (2)-2C AND (1)-VIDEO
TRENCH IN PAVED AREA = 12'

IN TRENCH = 33'

TOTAL = 45'

EX. SIGNAL SUPPORT 'D' (TO REMAIN)

W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),

(2)-EX. PEDESTRIAN HEADS (TO REMAIN),

(2)-EX. PUSHBUTTONS (TO BE REMOVED),

(2)-NEW PUSHBUTTONS,

(1)-NEW VIDEO DETECTION CAMERA

(2)-NEW 2C AND (1)-NEW VIDEO

IN EXISTING CONDUIT = 28'

GROUND MOUNTED CABINET (TO REMAIN)

(8)-NEW 2C, (4)-NEW VIDEO AND (1)-LIGHTING

IN EXISTING CONDUIT = 6'

EX PB-2 (TO REMAIN)

(1)-3" CONDUIT W/ (4)-2C AND (2)-VIDEO

DIRECTIONALLY DRILLED = 97'

EX PB-3 (TO REMAIN)

(2)-NEW 2C AND (1)-NEW VIDEO

IN EXISTING CONDUIT = 7'

EX. SIGNAL SUPPORT 'A' (TO REMAIN)

W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),

(2)-EX. PEDESTRIAN HEADS (TO REMAIN),

(2)-EX. PUSHBUTTONS (TO BE REMOVED),

(2)-NEW PUSHBUTTONS,

(1)-NEW VIDEO DETECTION CAMERA

(1)-3" CONDUIT W/ (2)-2C AND (1)-VIDEO

TRENCH IN PAVED AREA = 9'

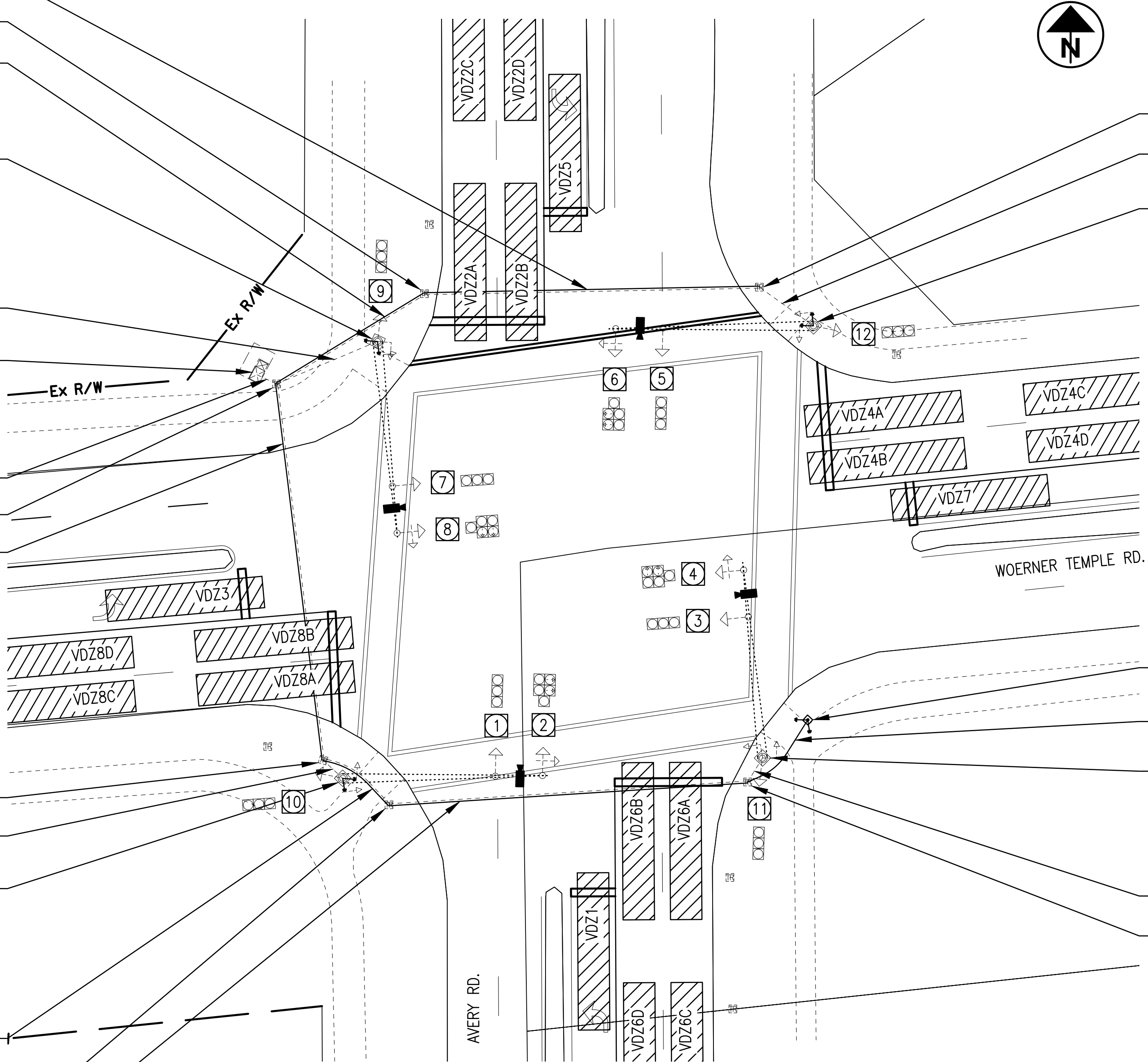
IN TRENCH = 12'

TOTAL = 21'

EX PB-4 (TO REMAIN)

(1)-3" CONDUIT W/ (2)-2C AND (1)-VIDEO

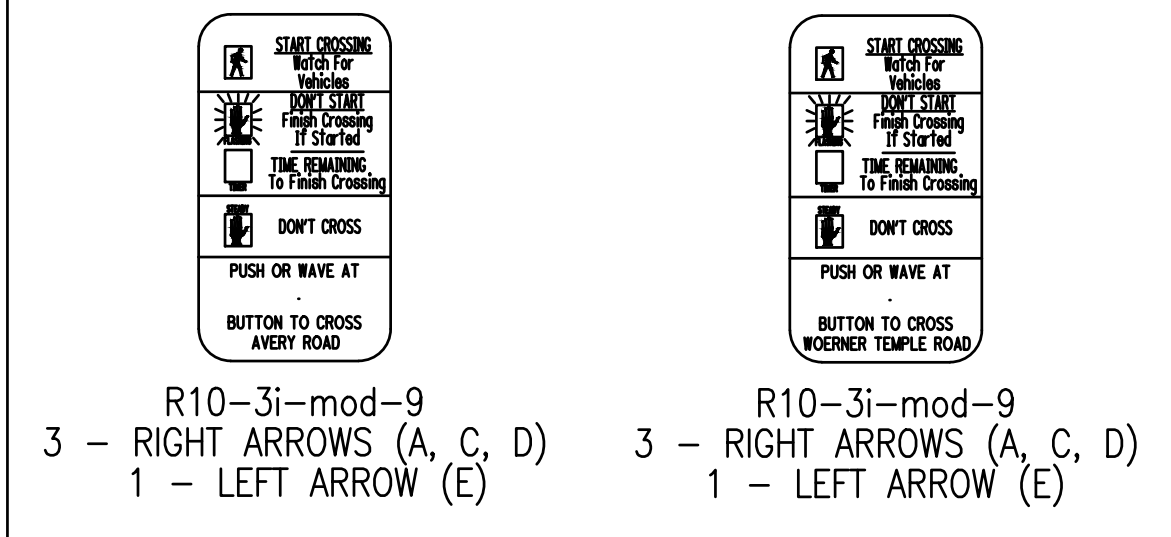
DIRECTIONALLY DRILLED = 92'



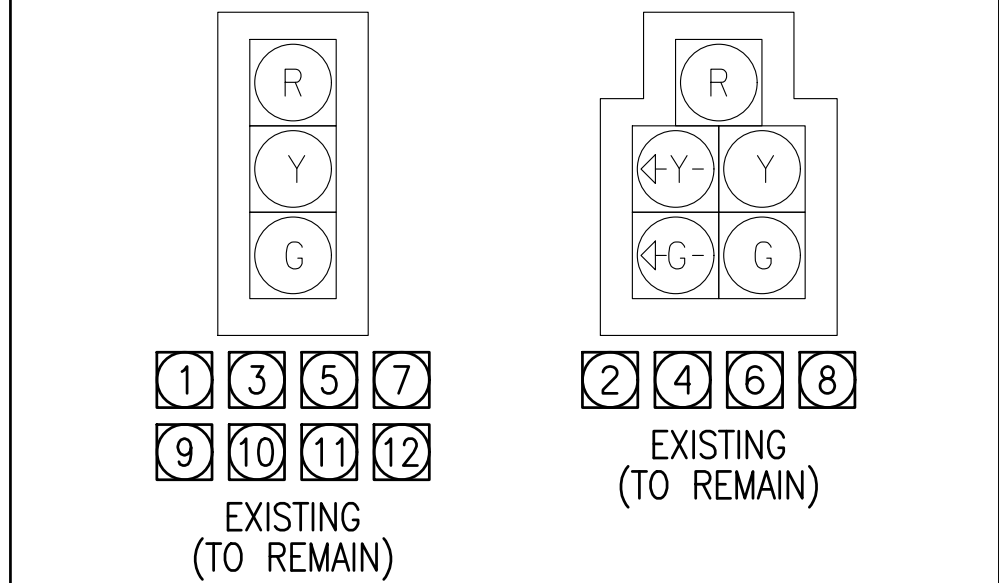
REMOVAL CHART

QUANTITY	REMOVAL ITEM DESCRIPTION	DELIVERED	DISPOSED
8	PUSHBUTTONS		X
10	PULL BOXES		X
LUMP	SIGNAL CABLES		X

PEDESTRIAN SIGNS



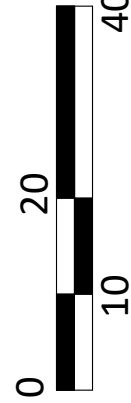
SIGNAL INDICATIONS



LEGEND

	PROP.	EXIST.
SIGNAL SUPPORT POLE		
PEDESTAL SUPPORT		
TRAFFIC SIGNAL, 3 UNIT HEAD, 12"		
TRAFFIC SIGNAL, 5 UNIT HEAD, 12"		
VIDEO DETECTION CAMERA		
DETECTION ZONE		
PULL BOX		
PEDESTRIAN SIGNAL		
PEDESTRIAN PUSHBUTTON		
GROUND MOUNTED CONTROLLER CABINET		

HORIZONTAL
SCALE IN FEET



TRAFFIC SIGNAL PLAN
WOERNER TEMPLE RD. / AVERY RD.

DESIGN AGENCY



DESIGNER

JAH

REVIEWER

KMG 2/18/25

PROJECT ID

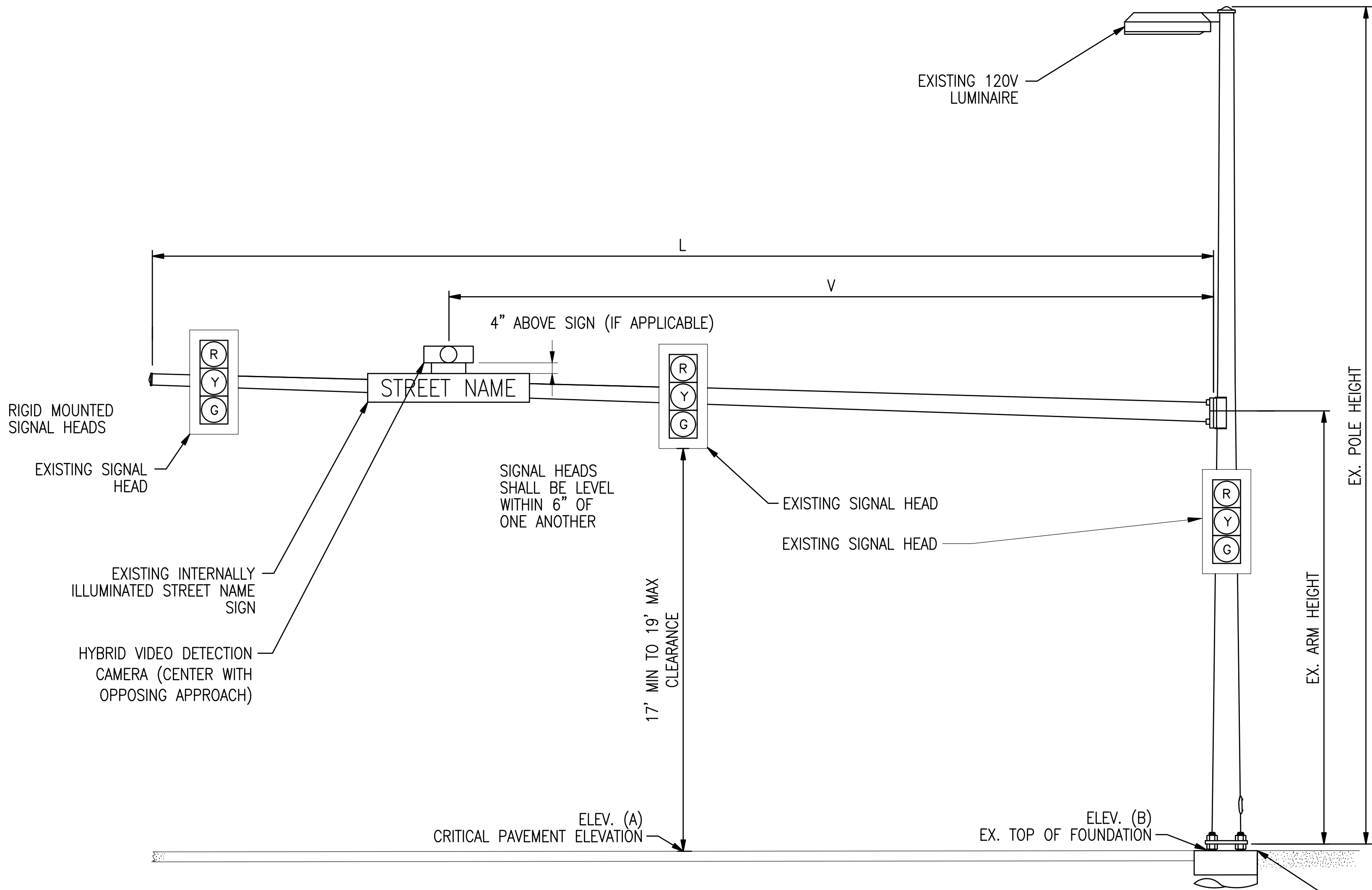
25-002-CIP

SHEET

P.52

TOTAL

60



SIGNAL SUPPORT ELEVATION

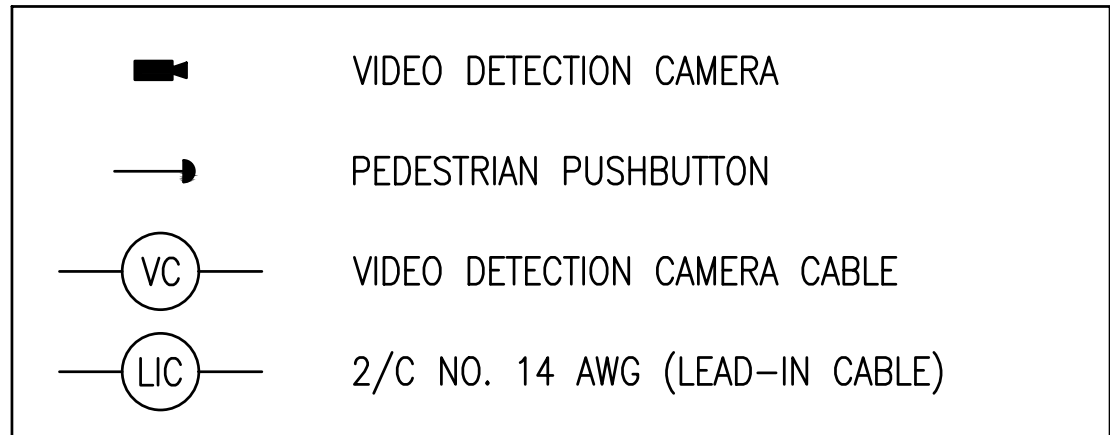
VIDEO DETECTOR CHART

DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY (SEC.)	DELAY INHIBIT PHASE	PURPOSE	SIZE	DISTANCE BEHIND TOP BAR
VDZ1	NB LEFT CALL	PRESENCE	Ø1	2	Ø1	STOP LINE	8 X 40	-
VDZ2A	SB CALL	PRESENCE	Ø2	8	Ø2	STOP LINE	8 X 40	-
VDZ2B	SB CALL	PRESENCE	Ø2	-	-	STOP LINE	8 X 40	-
VDZ2C	SB CALL	PULSE	Ø2	-	-	ADVANCE	MAX	50-MAX
VDZ2D	SB CALL	PULSE	Ø2	-	-	ADVANCE	MAX	50-MAX
VDZ3	EB LEFT CALL	PRESENCE	Ø3	-	-	STOP LINE	8 X 40	-
VDZ4A	WB CALL	PRESENCE	Ø4	8	Ø4	STOP LINE	8 X 40	-
VDZ4B	WB CALL	PRESENCE	Ø4	-	-	STOP LINE	8 X 40	-
VDZ4C	WB CALL	PULSE	Ø4	-	-	ADVANCE	MAX	50-MAX
VDZ4D	WB CALL	PULSE	Ø4	-	-	ADVANCE	MAX	50-MAX
VDZ5	SB LEFT CALL	PRESENCE	Ø5	2	Ø5	STOP LINE	8 X 40	-
VDZ6A	NB CALL	PRESENCE	Ø6	8	Ø6	STOP LINE	8 X 40	-
VDZ6B	NB CALL	PRESENCE	Ø6	-	-	STOP LINE	8 X 40	-
VDZ6C	NB CALL	PULSE	Ø6	-	-	ADVANCE	MAX	50-MAX
VDZ6D	NB CALL	PULSE	Ø6	-	-	ADVANCE	MAX	50-MAX
VDZ7	WB LEFT CALL	PRESENCE	Ø7	-	-	STOP LINE	8 X 40	-
VDZ8A	EB CALL	PRESENCE	Ø8	8	Ø8	STOP LINE	8 X 40	-
VDZ8B	EB CALL	PRESENCE	Ø8	-	-	STOP LINE	8 X 40	-
VDZ8C	EB CALL	PULSE	Ø8	-	-	ADVANCE	MAX	50-MAX
VDZ8D	EB CALL	PULSE	Ø8	-	-	ADVANCE	MAX	50-MAX

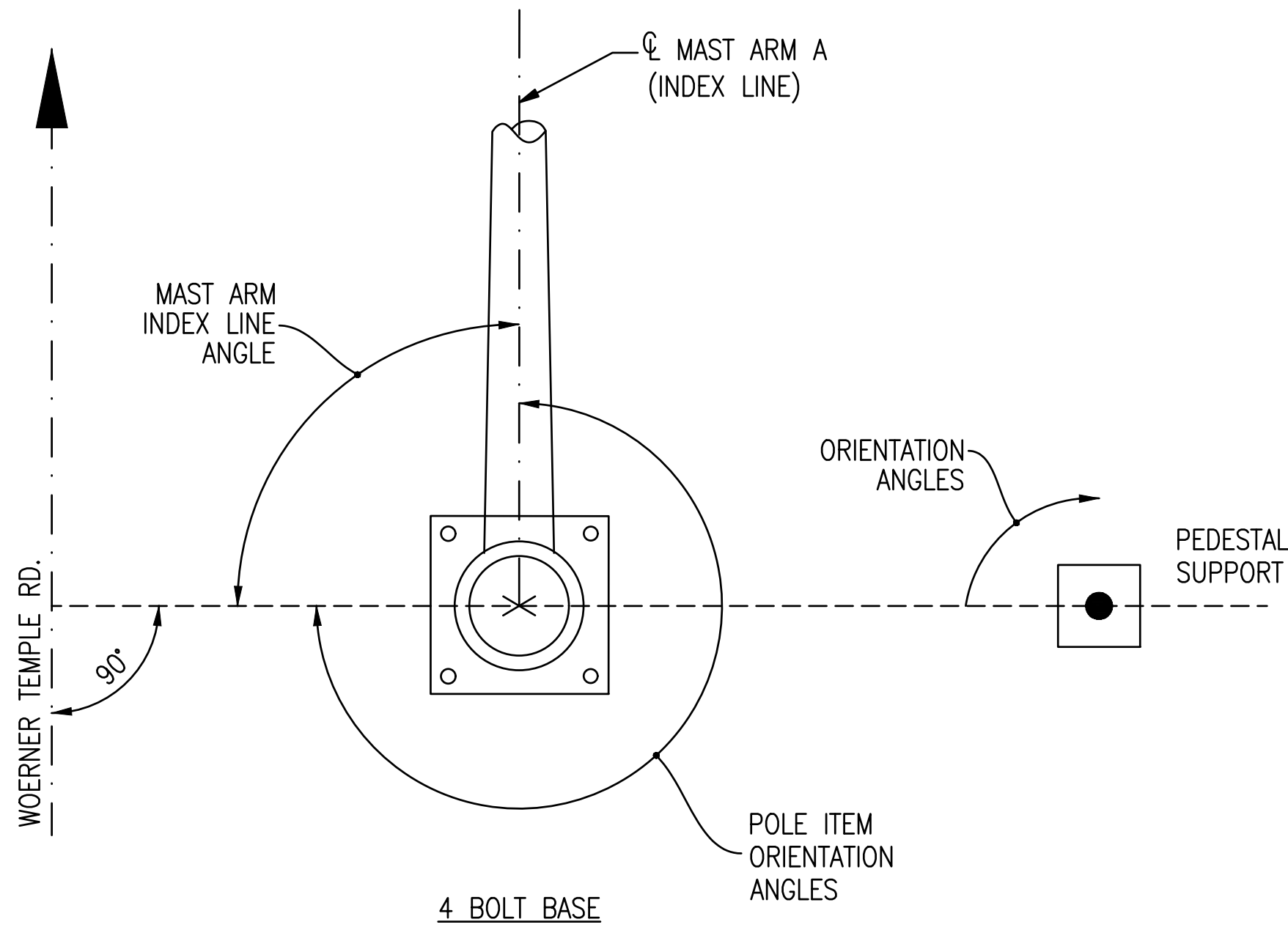
NOTES:

- ALL EXISTING SIGNAL TIMINGS SHALL REMAIN AS CURRENTLY PROGRAMMED.
- ALL MOVEMENTS SHALL BE ACTUATED. THE PRIMARY THRU MOVEMENT SHOULD HAVE MIN RECALL ACTIVE TO REST IN GREEN.
- ALL DETECTOR DELAYS SHALL BE PLACED IN THE CONTROLLER.

LEGEND



*NOTE: ALL EXISTING VEHICULAR SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS, AND INTERNALLY ILLUMINATED STREET SIGNS SHALL REMAIN AS CURRENTLY WIRED IN THE CONTROLLER CABINET.



NOTES:

- ALL ANGLES MEASURED CLOCKWISE.
- BASE PLATE IS ORIENTED SQUARE TO MAST ARM A, EVEN IF THE SUPPORT HAS TWO ARMS.

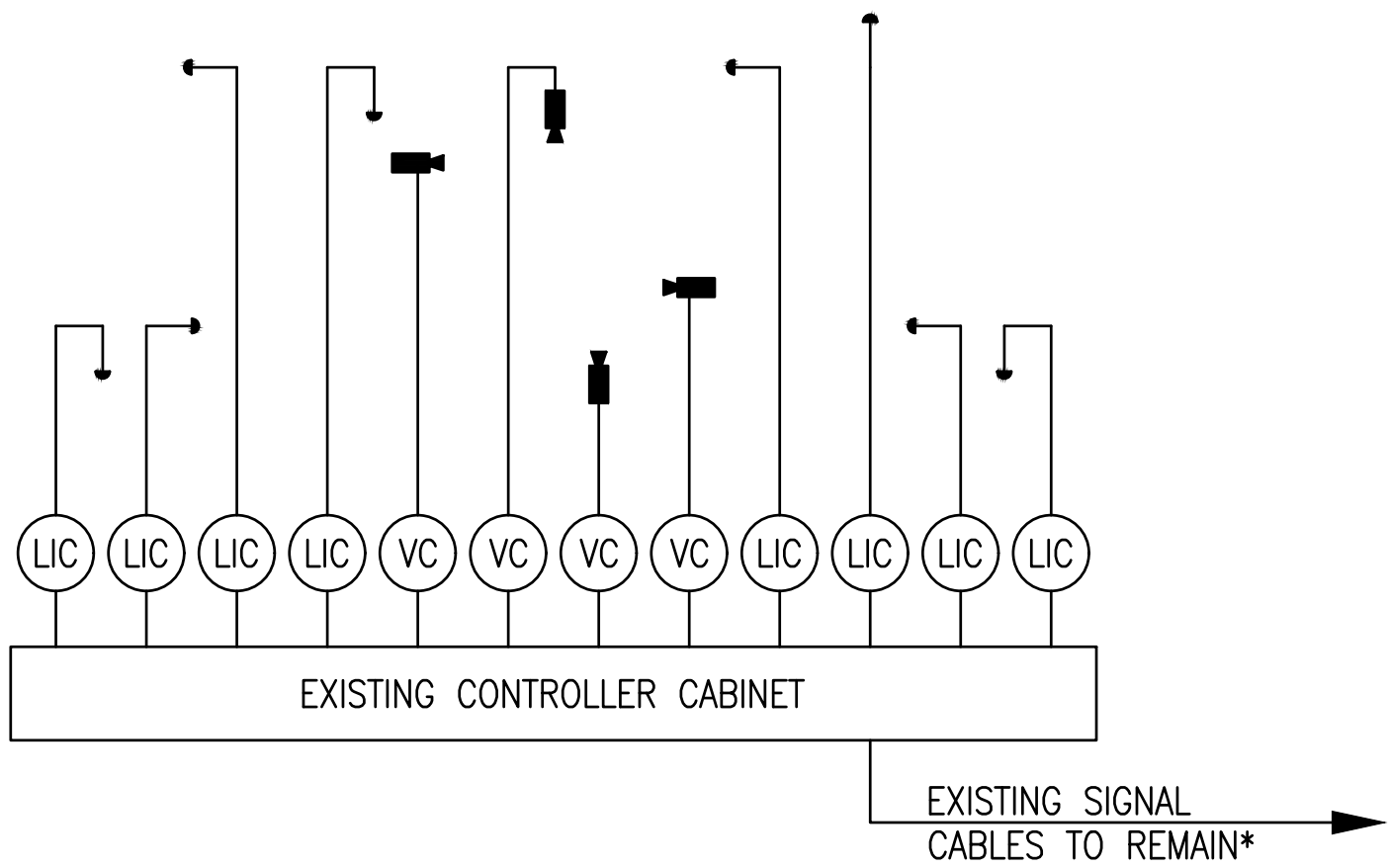
POLE ORIENTATION

POLE ORIENTATION TABLE

SUPPORT NO.	SIGNAL SUPPORT DETAILS		ORIENTATION ANGLES FROM INDEX LINE	
	POLE HEIGHT	V	PEDESTRIAN PUSHBUTTON	HANDHOLE
	FT	FT	DEG	DEG
EX. A	-	45	5*/80*	-
EX. B	-	42	-	-
EX. C	-	44.5	5*/85*	-
EX. D	-	43	0*/100*	-
E	8	-	180/280	-

*NOTE: THE PROVIDED ANGLE ORIENTATIONS SHALL BE CONSIDERED APPROXIMATE. THE CONTRACTOR SHALL INSTALL THE PROPOSED PEDESTRIAN PUSHBUTTON LOCATIONS AT THE SAME LOCATION AS THE EXISTING PEDESTRIAN PUSHBUTTONS. NO NEW HOLES SHALL BE DRILLED INTO THE EXISTING SUPPORTS.

WIRING DIAGRAM



NOTE:

1. PEDESTRIAN SIGNAL HEAD, PUSHBUTTONS, AND ASSOCIATED SIGNS SHALL BE MOUNTED AS PER SCD TC-85.10 ON THE SUPPORT POLE.
2. CONDUIT LOCATIONS SHALL BE CONSIDERED APPROXIMATE AND ARE TO BE VERIFIED BY THE CONTRACTOR PRIOR TO INSTALLING ANY UNDERGROUND CABLE.
3. NO FIELD SURVEY WAS PERFORMED FOR THIS PROJECT. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS OF ALL UTILITIES AND EXISTING SIGNAL EQUIPMENT. ALL EXISTING CONDUIT LOCATIONS SHALL BE CONSIDERED APPROXIMATE AND ARE TO BE VERIFIED BY THE CONTRACTOR PRIOR TO EXCAVATION AND INSTALLING ANY UNDERGROUND CABLE.
4. FOR TRAFFIC SIGNAL LEGEND, SEE SHEET 52.

EX. PB-1 (TO REMAIN)

(2)-NEW 2C AND (1)-NEW VIDEO
IN EXISTING CONDUIT = 19'

EX. SIGNAL SUPPORT 'D' (TO REMAIN)
W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),
(2)-EX. PEDESTRIAN HEADS (TO REMAIN),
(2)-EX. PUSHBUTTONS (TO BE REMOVED),
(2)-NEW PUSHBUTTONS,
(1)-NEW VIDEO DETECTION CAMERA

EX. SIGNAL SUPPORT 'A' (TO REMAIN)
W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),
(2)-EX. PEDESTRIAN HEADS (TO REMAIN),
(2)-EX. PUSHBUTTONS (TO BE REMOVED),
(2)-NEW PUSHBUTTONS,
(1)-NEW VIDEO DETECTION CAMERA

(2)-NEW 2C AND (1)-NEW VIDEO
IN EXISTING CONDUIT = 29'

EX. PB-2 (TO REMAIN)

(2)-NEW 2C AND (1)-NEW VIDEO
IN EXISTING CONDUIT = 83'

(2)-NEW 2C AND (1)-NEW VIDEO
IN EXISTING CONDUIT = 82'

EX. PB-3 (TO REMAIN)

(2)-NEW 2C AND (1)-NEW VIDEO
IN EXISTING CONDUIT = 46'

EX. SIGNAL SUPPORT 'C' (TO REMAIN)
W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),
(2)-EX. PEDESTRIAN HEADS (TO REMAIN),
(2)-EX. PUSHBUTTONS (TO BE REMOVED),
(2)-NEW PUSHBUTTONS,
(1)-NEW VIDEO DETECTION CAMERA

(2)-NEW 2C AND (1)-NEW VIDEO
IN EXISTING CONDUIT = 23'

EX. PB-4 (TO REMAIN)

(4)-NEW 2C AND (2)-NEW VIDEO
IN EXISTING CONDUIT = 95'

EX. PB-5 (TO REMAIN)

(8)-NEW 2C AND (4)-NEW VIDEO
IN EXISTING CONDUIT = 27'

GROUND MOUNTED CABINET (TO REMAIN)

(4)-NEW 2C AND (2)-NEW VIDEO
IN EXISTING CONDUIT = 42'

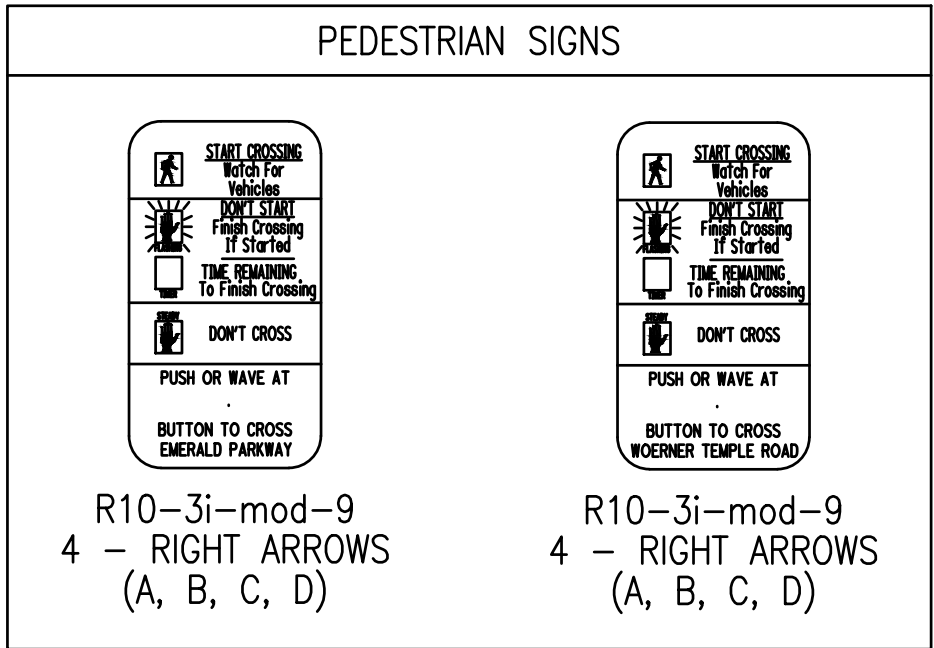
EX. SIGNAL SUPPORT 'B' (TO REMAIN)
W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),
(2)-EX. PEDESTRIAN HEADS (TO REMAIN),
(2)-EX. PUSHBUTTONS (TO BE REMOVED),
(2)-NEW PUSHBUTTONS,
(1)-NEW VIDEO DETECTION CAMERA

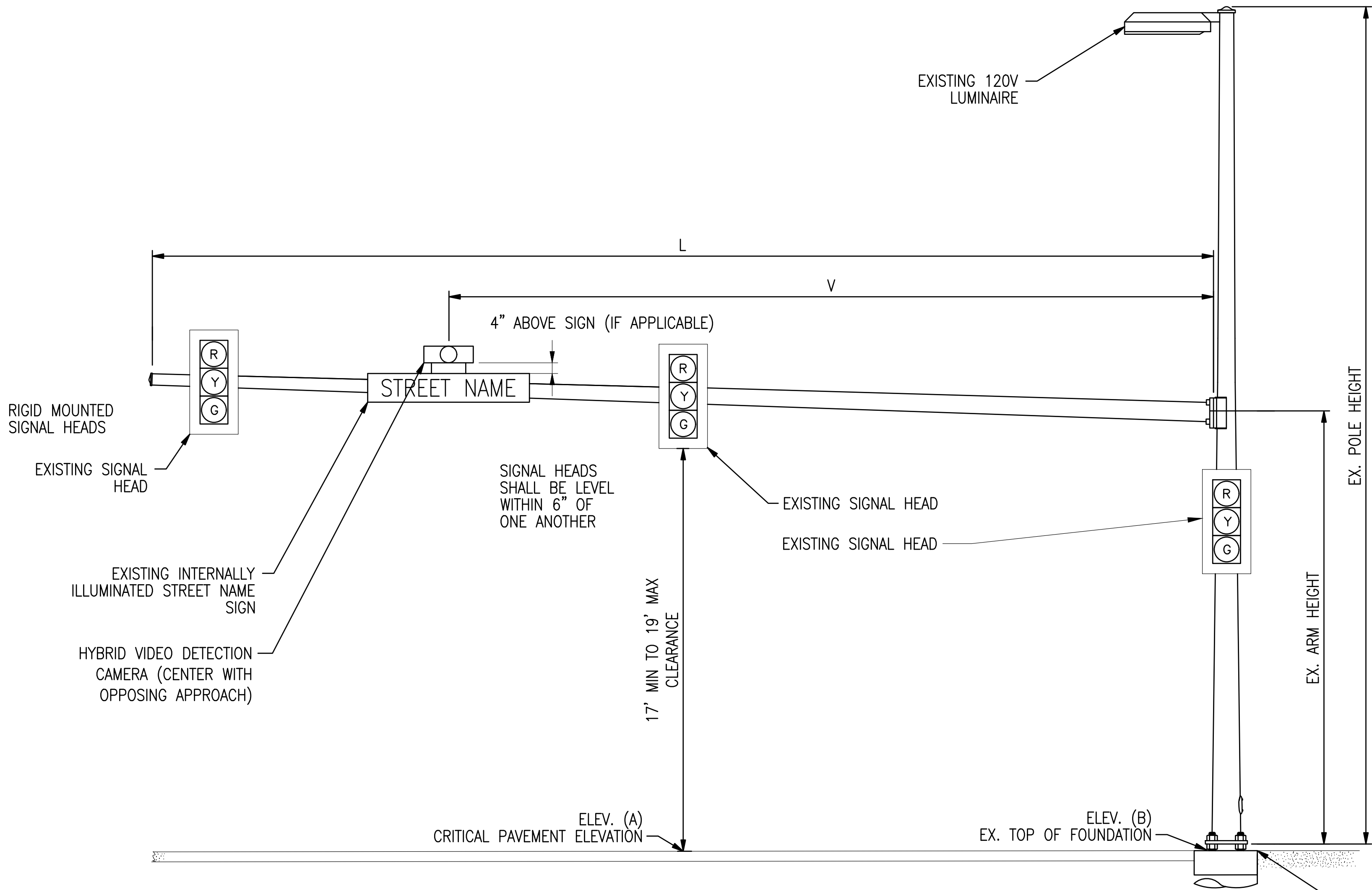
(2)-NEW 2C AND (1)-NEW VIDEO
IN EXISTING CONDUIT = 13'

EX. PB-6 (TO REMAIN)

REMOVAL CHART

QUANTITY	REMOVAL ITEM DESCRIPTION	DELIVERED	DISPOSED
8	PUSHBUTTONS		X
8	PULL BOXES		X
LUMP	SIGNAL CABLES		X





SIGNAL SUPPORT ELEVATION

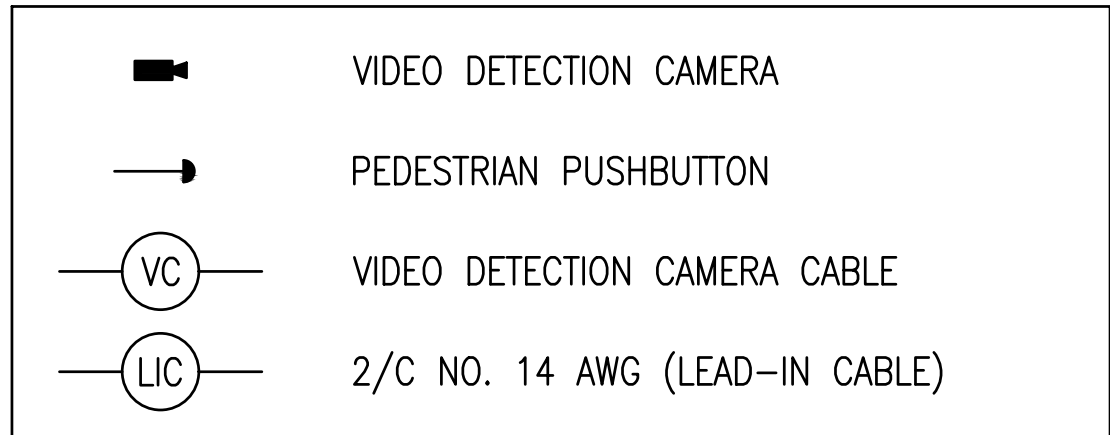
VIDEO DETECTOR CHART

DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY (SEC.)	DELAY INHIBIT PHASE	PURPOSE	SIZE	DISTANCE BEHIND TOP BAR
VDZ1	NB LEFT CALL	PRESENCE	Ø1	2	Ø1	STOP LINE	8 X 40	-
VDZ2A	SB CALL	PRESENCE	Ø2	8	Ø2	STOP LINE	8 X 40	-
VDZ2B	SB CALL	PRESENCE	Ø2	-	-	STOP LINE	8 X 40	-
VDZ2C	SB CALL	PULSE	Ø2	-	-	ADVANCE	MAX	50-MAX
VDZ2D	SB CALL	PULSE	Ø2	-	-	ADVANCE	MAX	50-MAX
VDZ3	EB LEFT CALL	PRESENCE	Ø3	-	-	STOP LINE	8 X 40	-
VDZ4A	WB CALL	PRESENCE	Ø4	8	Ø4	STOP LINE	8 X 40	-
VDZ4B	WB CALL	PRESENCE	Ø4	-	-	STOP LINE	8 X 40	-
VDZ4C	WB CALL	PRESENCE	Ø4	-	-	STOP LINE	8 X 40	-
VDZ4D	WB CALL	PULSE	Ø4	-	-	ADVANCE	MAX	50-MAX
VDZ4E	WB CALL	PULSE	Ø4	-	-	ADVANCE	MAX	50-MAX
VDZ5	SB LEFT CALL	PRESENCE	Ø5	2	Ø5	STOP LINE	8 X 40	-
VDZ6A	NB CALL	PRESENCE	Ø6	8	Ø6	STOP LINE	8 X 40	-
VDZ6B	NB CALL	PRESENCE	Ø6	-	-	STOP LINE	8 X 40	-
VDZ6C	NB CALL	PULSE	Ø6	-	-	ADVANCE	MAX	50-MAX
VDZ6D	NB CALL	PULSE	Ø6	-	-	ADVANCE	MAX	50-MAX
VDZ7	WB LEFT CALL	PRESENCE	Ø7	-	-	STOP LINE	8 X 40	-
VDZ8A	EB CALL	PRESENCE	Ø8	8	Ø8	STOP LINE	8 X 40	-
VDZ8B	EB CALL	PRESENCE	Ø8	-	-	STOP LINE	8 X 40	-
VDZ8C	EB CALL	PULSE	Ø8	-	-	ADVANCE	MAX	50-MAX
VDZ8D	EB CALL	PULSE	Ø8	-	-	ADVANCE	MAX	50-MAX

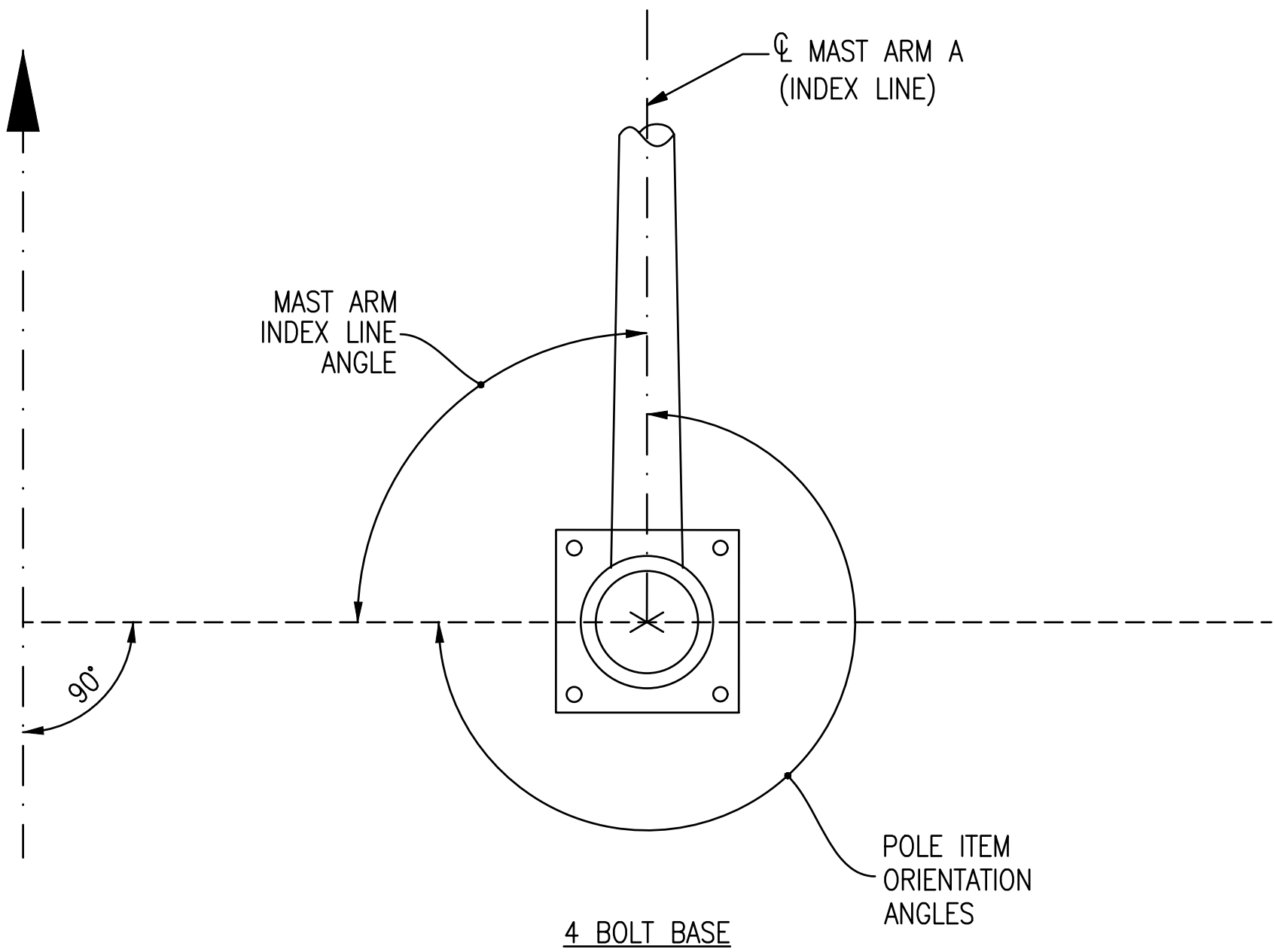
NOTES:

- ALL EXISTING SIGNAL TIMINGS SHALL REMAIN AS CURRENTLY PROGRAMMED.
- ALL MOVEMENTS SHALL BE ACTUATED. THE PRIMARY THRU MOVEMENT SHOULD HAVE MIN RECALL ACTIVE TO REST IN GREEN.
- ALL DETECTOR DELAYS SHALL BE PLACED IN THE CONTROLLER.

LEGEND



*NOTE: ALL EXISTING VEHICULAR SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS, AND INTERNALLY ILLUMINATED STREET SIGNS SHALL REMAIN AS CURRENTLY WIRED IN THE CONTROLLER CABINET.



NOTES:

- ALL ANGLES MEASURED CLOCKWISE.
- BASE PLATE IS ORIENTED SQUARE TO MAST ARM A, EVEN IF THE SUPPORT HAS TWO ARMS.

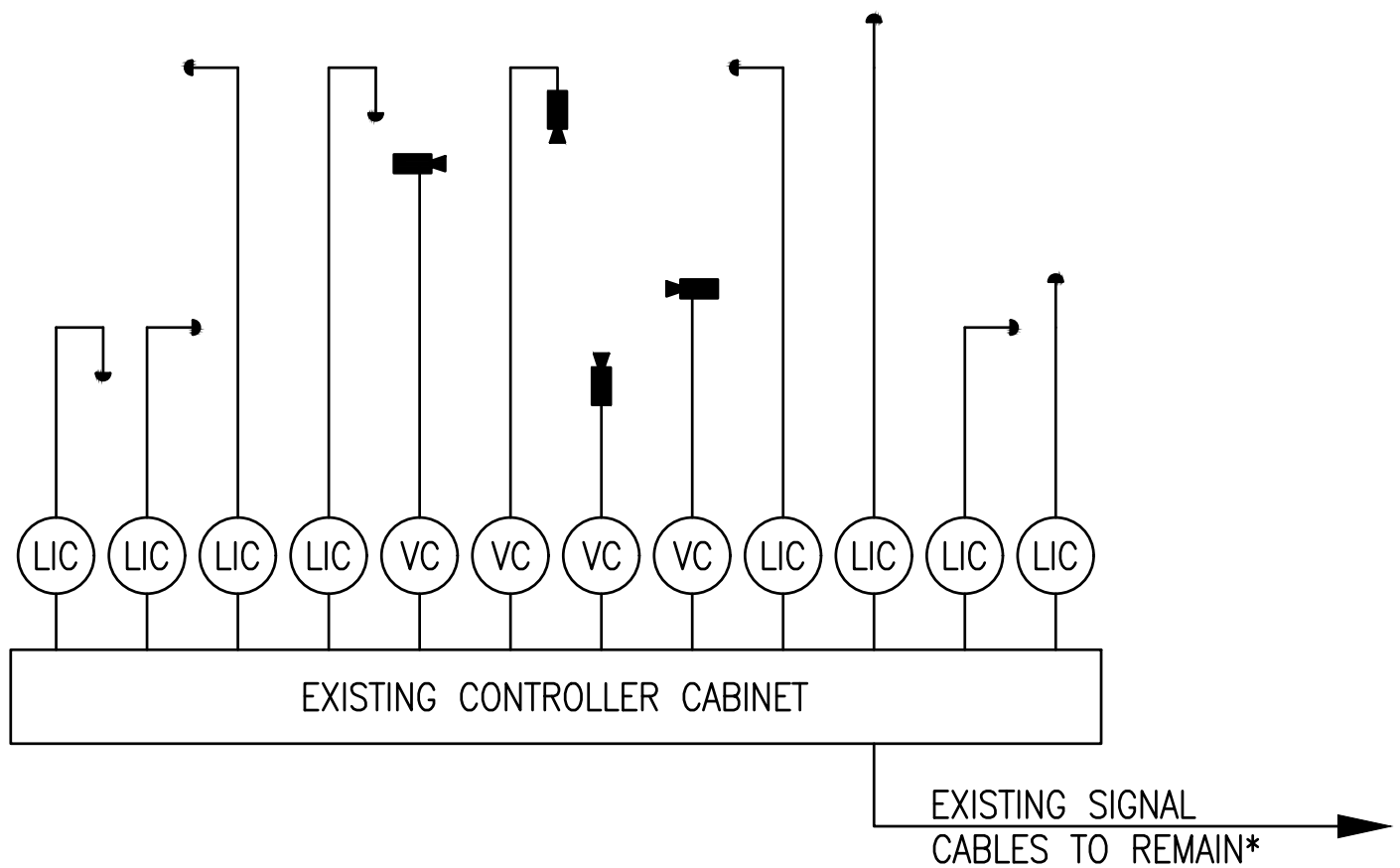
POLE ORIENTATION

POLE ORIENTATION TABLE

SUPPORT NO.	SIGNAL SUPPORT DETAILS	ORIENTATION ANGLES FROM INDEX LINE	
	V	PEDESTRIAN PUSHBUTTON	HANDHOLE
	FT	DEG	DEG
EX. A	42.5	0*/95*	-
EX. B	43	5*/90*	-
EX. C	41	0*/85*	-
EX. D	43	90*/355*	-

*NOTE: THE PROVIDED ANGLE ORIENTATIONS SHALL BE CONSIDERED APPROXIMATE. THE CONTRACTOR SHALL INSTALL THE PROPOSED PEDESTRIAN PUSHBUTTON LOCATIONS AT THE SAME LOCATION AS THE EXISTING PEDESTRIAN PUSHBUTTONS. NO NEW HOLES SHALL BE DRILLED INTO THE EXISTING SUPPORTS.

WIRING DIAGRAM



NOTE:

1.

PEDESTRIAN SIGNAL HEAD, PUSHBUTTONS, AND ASSOCIATED SIGNS SHALL BE MOUNTED AS PER SCD TC-85.10 ON THE SUPPORT POLE.
2.

CONDUIT LOCATIONS SHALL BE CONSIDERED APPROXIMATE AND ARE TO BE VERIFIED BY THE CONTRACTOR PRIOR TO INSTALLING ANY UNDERGROUND CABLE.
3.

NO FIELD SURVEY WAS PERFORMED FOR THIS PROJECT. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS OF ALL UTILITIES AND EXISTING SIGNAL EQUIPMENT. ALL EXISTING CONDUIT LOCATIONS SHALL BE CONSIDERED APPROXIMATE AND ARE TO BE VERIFIED BY THE CONTRACTOR PRIOR TO EXCAVATION AND INSTALLING ANY UNDERGROUND CABLE.
4.

FOR TRAFFIC SIGNAL LEGEND, SEE SHEET 52.

EX. PB-1 (TO REMAIN)

(6)-NEW 2C AND (3)-NEW VIDEO

IN EXISTING CONDUIT = 17'

EXISTING GROUND MOUNTED CABINET (TO REMAIN)

(6)-NEW 2C AND (3)-NEW VIDEO

IN EXISTING CONDUIT = 47'

EX. PB-2 (TO REMAIN)

(1)-NEW 2C

IN EXISTING CONDUIT = 9'

EX. SIGNAL SUPPORT N/W-1 (TO REMAIN)

W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),

(1)-EX. PEDESTRIAN HEAD (TO REMAIN),

(1)-EX. PUSHBUTTON (TO BE REMOVED),

(1)-NEW PUSHBUTTON

(1)-NEW 2C

IN EXISTING CONDUIT = 30'

EX. SIGNAL SUPPORT N/W-2 (TO REMAIN)

W/ (1)-EX. PEDESTRIAN HEAD (TO REMAIN),

(1)-EX. PUSHBUTTON (TO BE REMOVED),

(1)-NEW PUSHBUTTON

(2)-NEW 2C AND (1)-NEW VIDEO

IN EXISTING CONDUIT = 76'

EX. SIGNAL SUPPORT S/W-1 (TO REMAIN)

W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),

(1)-EX. PEDESTRIAN HEAD (TO REMAIN),

(1)-EX. PUSHBUTTON (TO BE REMOVED),

(1)-NEW PUSHBUTTON,

(1)-NEW VIDEO DETECTION CAMERA

(1)-NEW 2C AND (1)-NEW VIDEO

IN EXISTING CONDUIT = 12'

EX. PB-3 (TO REMAIN)

(1)-NEW 2C

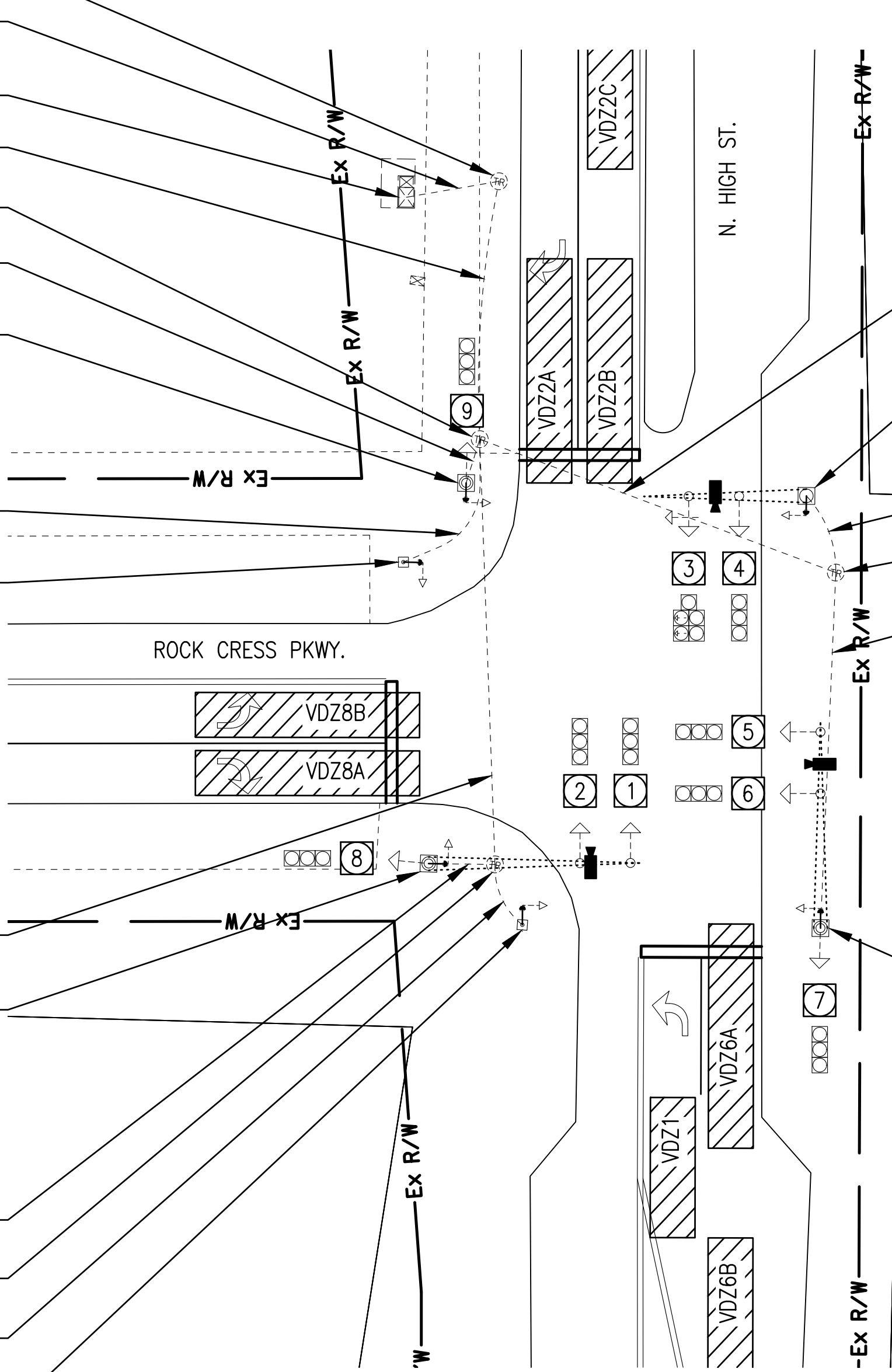
IN EXISTING CONDUIT = 13'

EX. SIGNAL SUPPORT S/W-2 (TO REMAIN)

W/ (1)-EX. PEDESTRIAN HEAD (TO REMAIN),

(1)-EX. PUSHBUTTON (TO BE REMOVED),

(1)-NEW PUSHBUTTON



(2)-NEW 2C AND (2)-NEW VIDEO

IN EXISTING CONDUIT = 68'

EX. SIGNAL SUPPORT N/E-1 (TO REMAIN)

W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),

(1)-EX. PEDESTRIAN HEAD (TO REMAIN),

(1)-EX. PUSHBUTTON (TO BE REMOVED),

(1)-NEW PUSHBUTTON,

(1)-NEW VIDEO DETECTION CAMERA

(1)-NEW 2C AND (1)-NEW VIDEO

IN EXISTING CONDUIT = 16'

EX. PB-4 (TO REMAIN)

(1)-NEW 2C AND (1)-NEW VIDEO

IN EXISTING CONDUIT = 64'

EX. SIGNAL SUPPORT S/E-1 (TO REMAIN)

W/ (1)-EX. AUXILIARY HEAD (TO REMAIN),

(1)-EX. PEDESTRIAN HEAD (TO REMAIN),

(1)-EX. PUSHBUTTON (TO BE REMOVED),

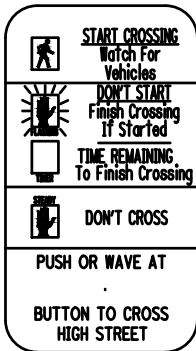
(1)-NEW PUSHBUTTON,

(1)-NEW VIDEO DETECTION CAMERA

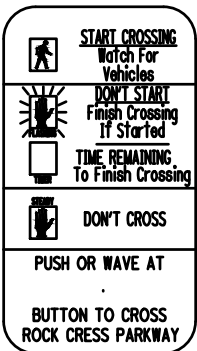
REMOVAL CHART

QUANTITY	REMOVAL ITEM DESCRIPTION	DELIVERED	DISPOSED
2	PUSHBUTTONS		X
2	PULL BOXES		X
LUMP	SIGNAL CABLES		X

PEDESTRIAN SIGNS

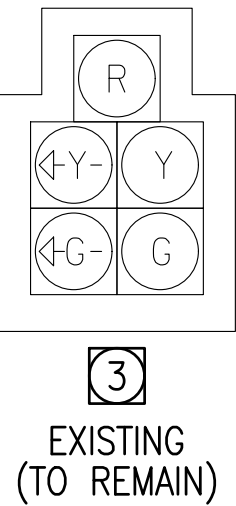
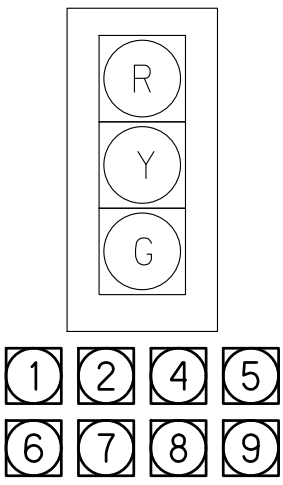


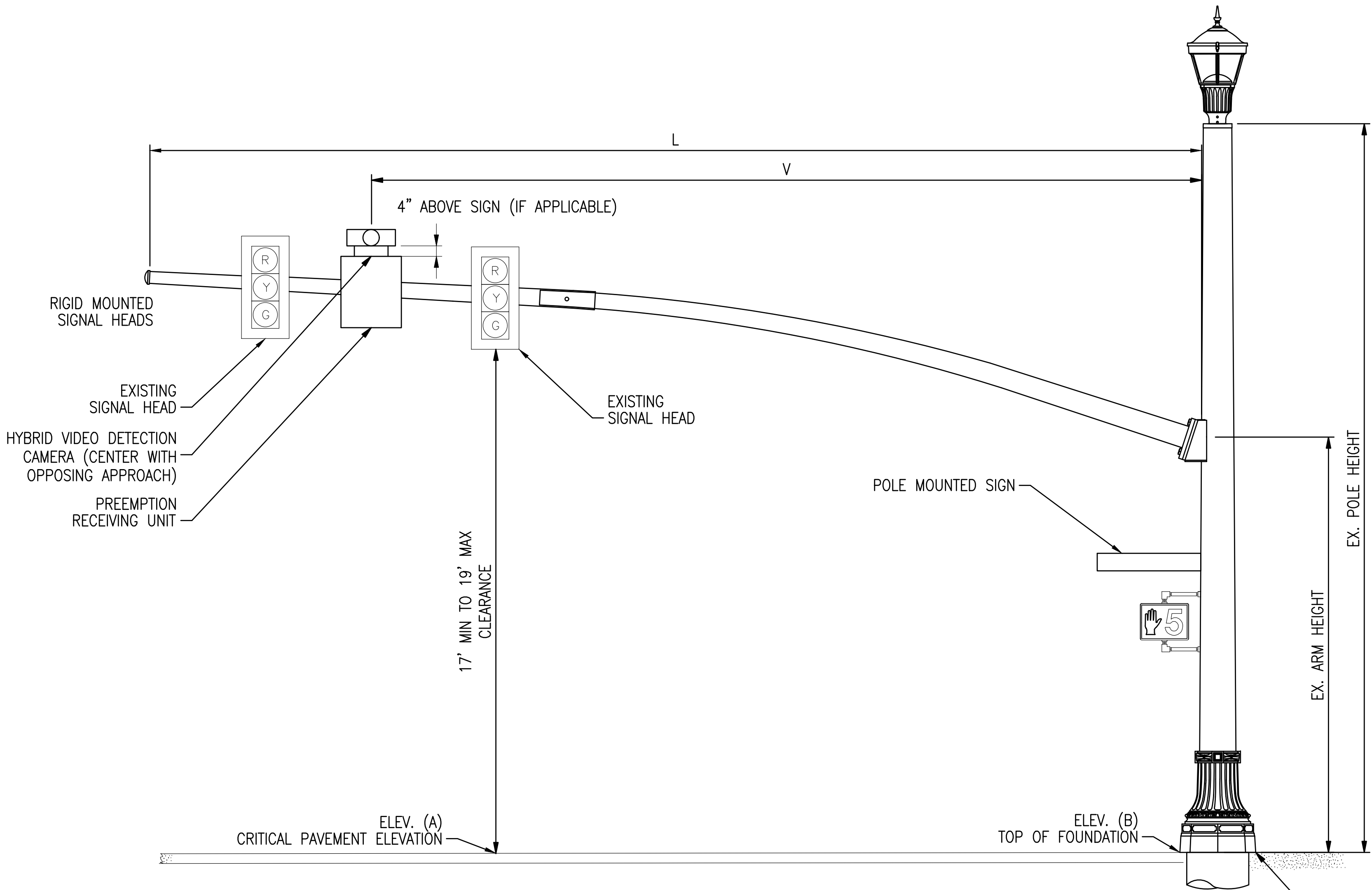
R10-3j-mod-9
2 - RIGHT ARROWS
(N/W-1, S/E-1)
2 - LEFT ARROWS
(N/E-1, S/W-2)



R10-3i-mod-9
1 - RIGHT ARROW (S/W-1)
1 - LEFT ARROW (N/W-2)

SIGNAL INDICATIONS





SIGNAL SUPPORT ELEVATION

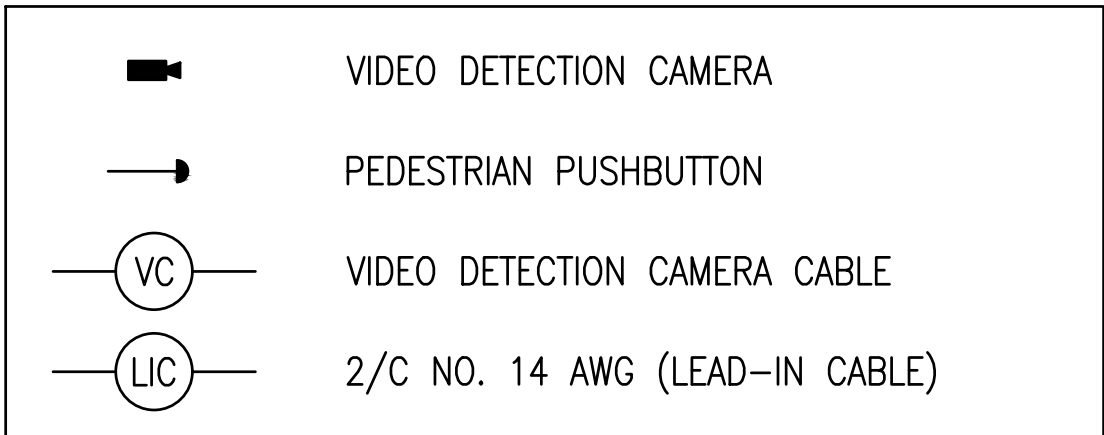
VIDEO DETECTOR CHART

DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY (SEC.)	DELAY INHIBIT PHASE	PURPOSE	SIZE	DISTANCE BEHIND TOP BAR
VDZ1	NB LEFT CALL	PRESENCE	Ø1	-	-	STOP LINE	8 X 40	-
VDZ2A	SB CALL	PRESENCE	Ø2	8	Ø2	STOP LINE	8 X 40	-
VDZ2B	SB CALL	PRESENCE	Ø2	-	-	STOP LINE	8 X 40	-
VDZ2C	SB CALL	PULSE	Ø2	-	-	ADVANCE	MAX	50-MAX
VDZ6A	NB CALL	PRESENCE	Ø6	8	Ø6	STOP LINE	8 X 40	-
VDZ6B	NB CALL	PULSE	Ø6	-	-	ADVANCE	MAX	50-MAX
VDZ8A	EB CALL	PRESENCE	Ø8	8	Ø8	STOP LINE	8 X 40	-
VDZ8B	EB CALL	PRESENCE	Ø8	-	-	STOP LINE	8 X 40	-

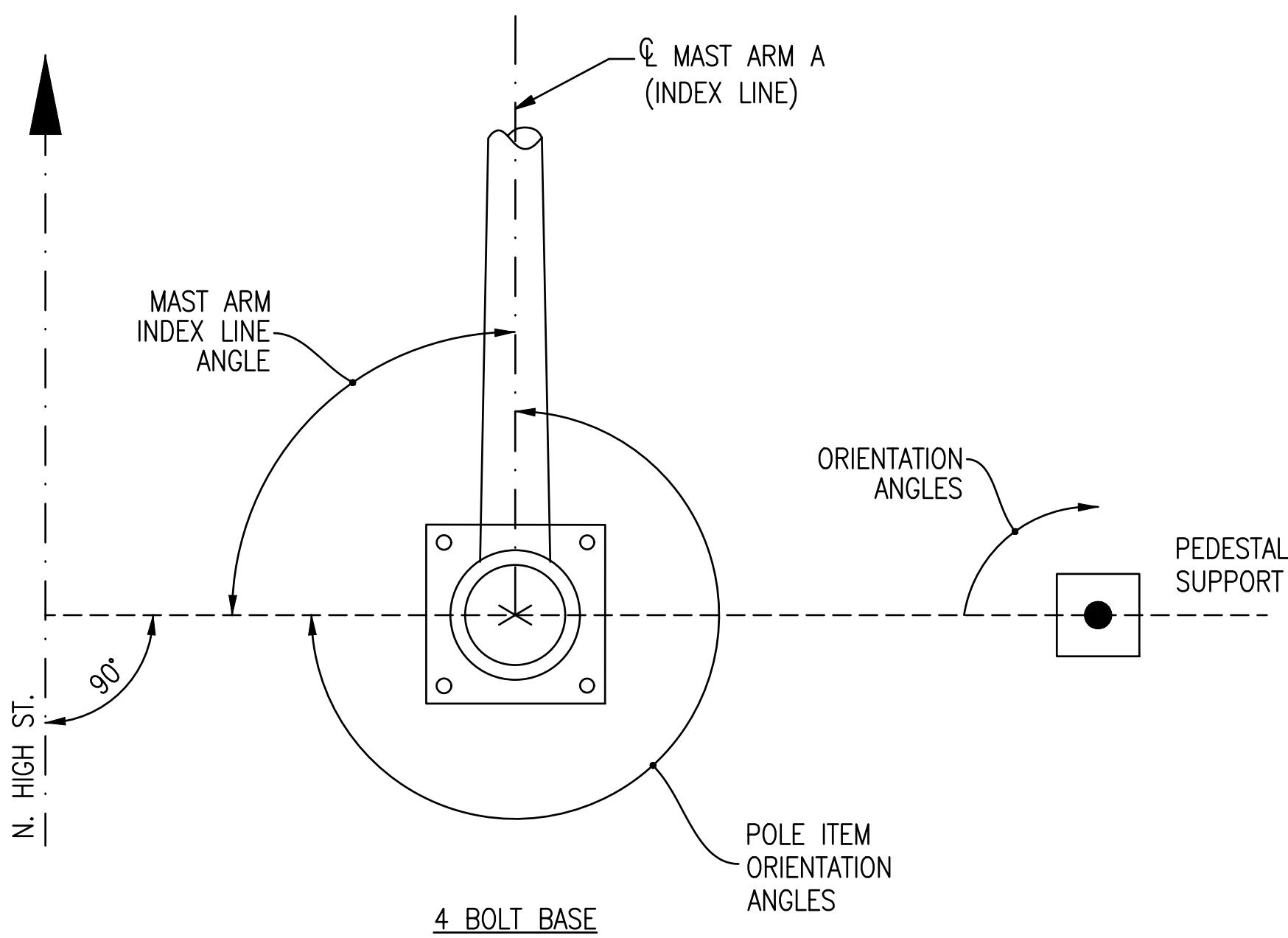
NOTES:

- ALL EXISTING SIGNAL TIMINGS SHALL REMAIN AS CURRENTLY PROGRAMMED.
- ALL MOVEMENTS SHALL BE ACTUATED. THE PRIMARY THRU MOVEMENT SHOULD HAVE MIN RECALL ACTIVE TO REST IN GREEN.
- ALL DETECTOR DELAYS SHALL BE PLACED IN THE CONTROLLER.

LEGEND



*NOTE: ALL EXISTING VEHICULAR SIGNAL HEADS, AND PEDESTRIAN SIGNAL HEADS SHALL REMAIN AS CURRENTLY WIRED IN THE CONTROLLER CABINET.



NOTES:

- ALL ANGLES MEASURED CLOCKWISE.
- BASE PLATE IS ORIENTED SQUARE TO MAST ARM A, EVEN IF THE SUPPORT HAS TWO ARMS.

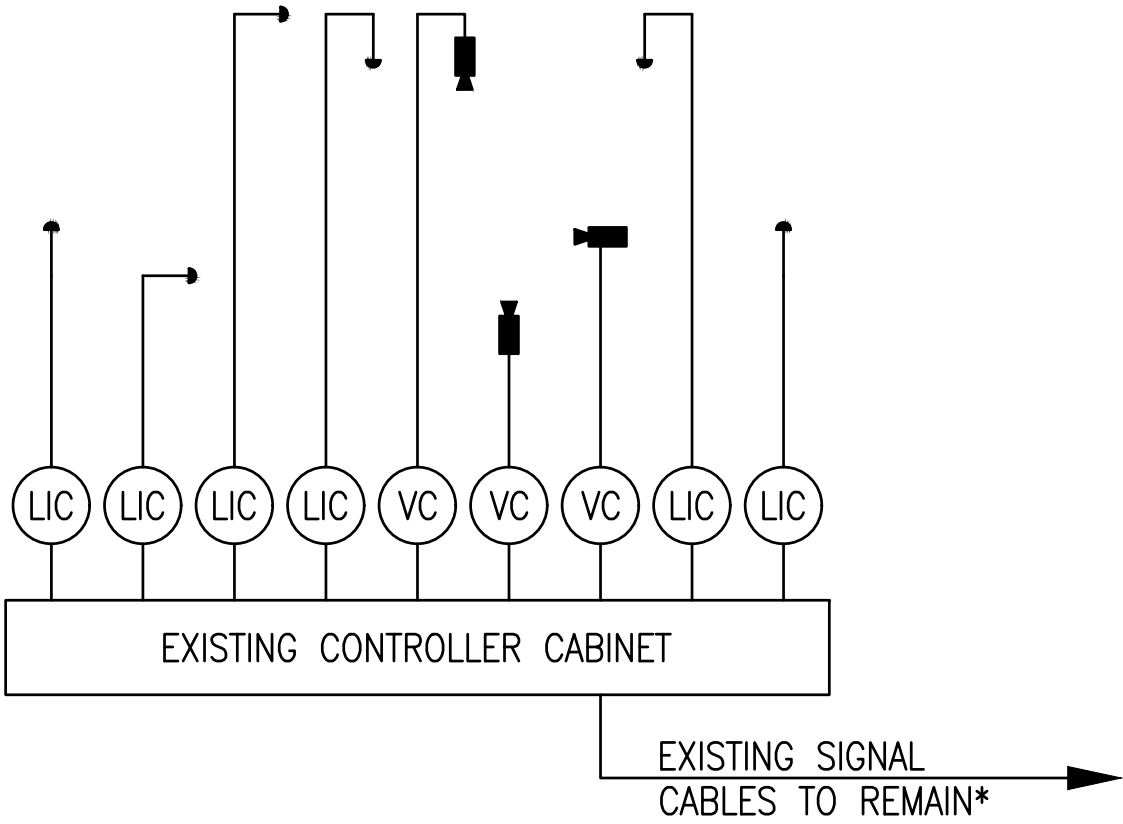
POLE ORIENTATION


POLE ORIENTATION TABLE

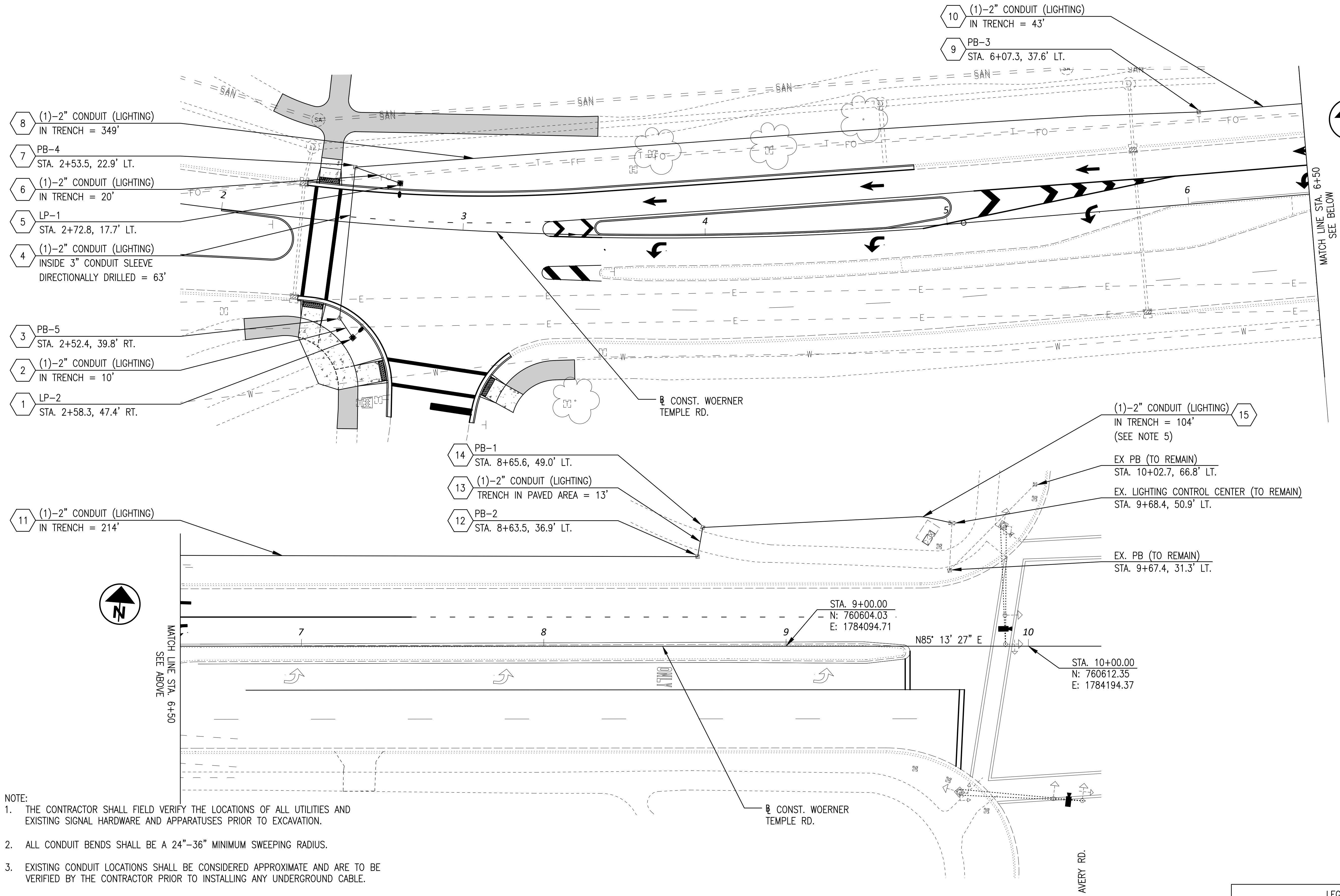
SUPPORT NO.	SIGNAL SUPPORT DETAILS	ORIENTATION ANGLES FROM INDEX LINE	
	V	PEDESTRIAN PUSHBUTTON	HANDHOLE
	FT	DEG	DEG
EX. N/W-1	-	90*	-
EX. N/W-2	-	0*	-
EX. S/W-1	29	0*	-
EX. S/W-2	-	270*	-
EX. N/E-1	16.5	270*	-
EX. S/E-1	30	0*	-

*NOTE: THE PROVIDED ANGLE ORIENTATIONS SHALL BE CONSIDERED APPROXIMATE. THE CONTRACTOR SHALL INSTALL THE PROPOSED PEDESTRIAN PUSHBUTTON LOCATIONS AT THE SAME LOCATION AS THE EXISTING PEDESTRIAN PUSHBUTTONS. NO NEW HOLES SHALL BE DRILLED INTO THE EXISTING SUPPORTS.

WIRING DIAGRAM



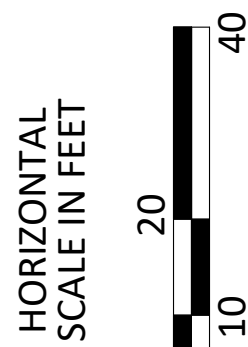
<div>2025/02/25 C:\2024\2024085\02 Dublin 2025 Street Maintenance\400-Engineering\Lighting\Sheets\2024085.02_L\N001.dwg</div>	<div>ITEM 625 – TRENCH, AS PER PLAN</div> <p>IN ADDITION TO ODOT ITEM 625, THE TRENCH SIZE, DEPTH, AND BACKFILL SHALL MEET CITY SPECIFICATIONS. IT IS ALSO THE INTENT OF THIS ITEM THAT CONDUIT TO BE LOCATED UNDER NEW ASPHALT/CONCRETE SHALL BE INSTALLED PRIOR TO PLACEMENT OF THE ASPHALT/CONCRETE, AND THAT COMPACTED ITEM 304 – BACKFILL, OR ITEM 636 – TYPE 2 OR 3 SHALL BE USED AT THOSE LOCATIONS (AS PER CITY SPECIFICATIONS). NO CONCRETE ENCASEMENT SHALL BE PROVIDED. THE COST OF THE BACKFILL SHALL BE INCLUDED IN THE UNIT COST BID FOR THIS ITEM. THE COST OF THE UNDERGROUND WARNING/MARKING TAPE SHALL BE PAID AS A SEPARATE ITEM.</p> <p>PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER LINEAR FOOT.</p>	<div>ITEM 625 – LIGHT POLE FOUNDATION (24" X 7" DEEP), AS PER PLAN</div> <p>THE TOP OF THE FOUNDATIONS SHALL BE TWO INCHES ABOVE GROUND LEVEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE FOUNDATION ELEVATIONS PRIOR TO INSTALLATION OF THE FOUNDATIONS. THE FOUNDATION DEPTH IN NO CASE SHALL BE LESS THAN THE SPECIFIED FOUNDATION DEPTH PER PAY ITEM, BUT SHALL OTHERWISE BE IN CONFORMANCE TO ODOT SPECIFICATIONS REGARDING FOUNDATIONS IN BEDROCK. THE FOUNDATION SHALL FOLLOW CITY STANDARD SL-04.</p> <p>THE CONTRACTOR SHALL COORDINATE WITH THE CITY OF DUBLIN TO CONFIRM THE ANCHOR BOLT SIZE, BOLT PATTERN AND BOLT CIRCLE PRIOR TO INSTALLING THE LIGHT POLE FOUNDATION.</p> <p>PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID, COMPLETE AND IN PLACE, AND ACCEPTED.</p>	<div>ITEM 632 – TEST HOLE PERFORMED</div> <p>IT IS ANTICIPATED THAT THE CONTRACTOR WILL ENCOUNTER UNDERGROUND UTILITIES WHILE EXCAVATING FOR SIGNAL SUPPORT FOUNDATIONS OR SIMILAR FOUNDATIONS. AFTER ACCURATELY IDENTIFYING THE PROPOSED LOCATION OF THE FOUNDATION, AS SHOWN IN THE PLANS AND AFTER MODIFYING THAT LOCATION, IF NECESSARY, BASED ON THE FIELD MARKING OF UNDERGROUND UTILITY LOCATION, THE CONTRACTOR DISCOVERS A UTILITY CONFLICT DURING THE EXCAVATION OPERATION, THE CONTRACTOR WILL BE COMPENSATED FOR EACH PARTIAL FOUNDATION EXCAVATION ACCORDING TO THE BID PRICE.</p> <p>BEFORE THE CONTRACTOR BEGINS THE EXCAVATION AT THE MODIFIED LOCATION, THE CONTRACTOR SHALL VERIFY THAT THERE WILL BE NO OVERHEAD UTILITY CONFLICTS RESULTING FROM THE NEW SIGNAL SUPPORT LOCATION. NEW SUPPORT LOCATIONS ARE TO BE APPROVED BY THE ENGINEER.</p> <p>THE WORK WILL INCLUDE BACKFILLING, COMPACTING, AND RESTORATION OF THE EXCAVATION TO THE SITE'S ORIGINAL CONDITION.</p> <p>EXCAVATIONS SHALL NOT BE LEFT OPEN OVERNIGHT.</p> <p>PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER EACH ITEM 632 – TEST HOLE PERFORMED TO BE USED AT THE DIRECTION OF THE ENGINEER. A QUANTITY OF 1 HAS BEEN CARRIED TO THE GENERAL SUMMARY.</p>	<div>LIGHTING NOTES</div> <div>DESIGN AGENCY</div> <div><div>GPD GROUP® <small>Glaus, Pyle, Schorrer, Burris & Dehaven, Inc. Copyright © 2004, All Rights Reserved. Rev. 10/22</small></div></div> <div>DESIGNER</div> <div>JAH</div> <div>REVIEWER</div> <div>KMG 2/18/25</div> <div>PROJECT ID</div> <div>25-002-CIP</div> <div>SHEET</div> <div>TOTAL</div> <div>P.58 60</div>
	<div>ITEM 625 – TRENCH IN PAVED AREA, AS PER PLAN</div> <p>IN ADDITION TO ODOT ITEM 625, THE TRENCH SIZE, DEPTH, AND BACKFILL SHALL MEET CITY SPECIFICATIONS.</p> <p>THE CONTRACTOR SHALL PERFORM ALL RESTORATION WITH MATERIALS IDENTICAL TO THE EXISTING SURFACE, INCLUDING, BUT NOT LIMITED TO ASPHALT, CONCRETE AND BRICK SIDEWALK, INTEGRAL CURB, AND SPECIAL SURFACES (SUCH AS COLORED OR TEXTURED) AS ENCOUNTERED. PATCHING OF CONCRETE SIDEWALKS AND DRIVEWAYS SHALL NOT BE PERMITTED. THE CONTRACTOR SHALL REPLACE SIDEWALKS AND DRIVEWAYS IN ENTIRE ORIGINAL SLAB SECTIONS.</p> <p>THIS ITEM SHALL INCLUDE ALL LABOR AND MATERIAL COSTS ASSOCIATED WITH THE PROVISION AND RESTORATION OF THE EXISTING SURFACE AS OUTLINED ABOVE. PAYMENT FOR THIS WORK SHALL BE AT THE CONTRACT UNIT PRICE AND WILL BE FULL COMPENSATION FOR ALL RESTORATION WORK, INCLUDING MATERIALS, EQUIPMENT, LABOR, INCIDENTALS, AND DISPOSAL OF ALL SURPLUS MATERIALS. THE COST OF THE UNDERGROUND WARNING/MARKING TAPE SHALL BE PAID AS A SEPARATE ITEM. PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER LINEAR FOOT.</p>	<div>ITEM 625 – LIGHT POLE, 35'-0", GENERAL STREET LIGHT</div> <p>ALL LIGHT POLES SHALL BE WITHOUT BANNER ARMS. THE CONTRACTOR SHALL INSTALL THE LIGHT POLES PER ODOT ITEM 625. ALL EQUIPMENT AND MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY OF DUBLIN'S STANDARD CONSTRUCTION DRAWING SL-03.</p> <p>A TRANSFORMER BASE SHALL BE PROVIDED IN ACCORDANCE WITH CITY OF DUBLIN STANDARD DRAWING SL-02. TRANSFORMER BASE SHALL BE PAID FOR SEPARATELY.</p> <p>PAYMENT FOR ITEM 625 – LIGHT POLE, 35'-0", GENERAL STREET LIGHT SHALL BE AT THE CONTRACT BID PRICE AND SHALL INCLUDE ALL COORDINATION, MOVEMENT OF GOODS, LABOR, MATERIALS AND TOOLS TO INSTALL THE PROPOSED LIGHT POLE, COMPLETE AND IN PLACE, TESTED AND ACCEPTED.</p>	<div>ITEM 625 – LUMINAIRE: 80 LED, TYPE III MEDIUM, 480 V</div> <p>THIS ITEM SHALL CONSIST OF AN CREE EDGE AREA LIGHT, LED LUMINAIRE WITH TYPE III DISTRIBUTION, DIRECT MOUNTING, 480 VOLT SINGLE PHASE, 133 W, 80 LED, 525 MA DRIVE CURRENT, COLOR TO BE BRONZE. THE LUMINAIRE SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF CITY STANDARD DRAWING SL-01.</p> <p>PAYMENT FOR ITEM 625 – LUMINAIRE: 80 LED, TYPE III MEDIUM, 480 V SHALL BE AT THE CONTRACT BID PRICE AND SHALL INCLUDE ALL COORDINATION, MOVEMENT OF GOODS, LABOR, MATERIALS AND TOOLS TO INSTALL THE PROPOSED LUMINAIRE, COMPLETE AND IN PLACE, TESTED AND ACCEPTED.</p>	
	<div>ITEM 625 – UNDERGROUND WARNING/MARKING TAPE, AS PER PLAN</div> <p>IN ADDITION TO ITEM 625 AND 725 OF THE ODOT STANDARD SPECIFICATIONS, THE FOLLOWING SHALL ALSO APPLY:</p> <p>THE TAPE SHALL BE THREE (3") INCH YELLOW PLASTIC CAUTION TAPE. THE TAPE SHALL BE INSTALLED OVER ALL 725.05 CONDUIT, OUTSIDE OF DRIVEWAYS AND ROADWAYS. AS PER CITY SPECIFICATIONS.</p> <p>PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE FOR THIS ITEM, COMPLETE AND IN PLACE.</p>	<div>ITEM 625 – LIGHTING, MISC.: CONTROL CENTER MODIFICATIONS</div> <p>THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE EXISTING LIGHTING CONTROL CENTER TO INCORPORATE THE ADDITIONAL CIRCUIT FOR THE PROPOSED LIGHTING AT THE WOERNER TEMPLE RD. / POOL ENTRANCE DR. INTERSECTION. THESE CHANGES INCLUDE, BUT ARE NOT LIMITED TO, THE INSTALLATION OF ANY NECESSARY CIRCUIT BREAKERS AND FUSES INTO THE EXISTING CONTROL CENTER TO POWER THE PROPOSED LIGHT POLES.</p> <p>PAYMENT SHALL BE MADE AT THE CONTRACT UNIT PRICE PER EACH AND SHALL INCLUDE ALL LABOR, EQUIPMENT AND MATERIALS TO MODIFY THE CONTROL CENTER AND RETURN THE LIGHTING CONTROL CENTER TO A FULLY OPERATIONAL STATUS. A QUANTITY OF 1 HAS BEEN CARRIED TO THE GENERAL SUMMARY.</p>		
	<div>ITEM 625 – GROUND ROD, AS PER PLAN</div> <p>IN ADDITION TO ITEM 625 AND 713, THIS ITEM SHALL INCLUDE A "CAD" WELD WIRE CONNECTION AT THE GROUND ROD, AS PER CITY OF DUBLIN SPECIFICATIONS, THE COST WHICH SHALL BE INCLUDED IN THE GROUND ROD INSTALLATION.</p> <p>PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER EACH, COMPLETE AND IN PLACE, ALL CONNECTIONS TESTED AND ACCEPTED.</p>			



- NOTE:
1. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS OF ALL UTILITIES AND EXISTING SIGNAL HARDWARE AND APPARATUSES PRIOR TO EXCAVATION.
 2. ALL CONDUIT BENDS SHALL BE A 24"-36" MINIMUM SWEEPING RADIUS.
 3. EXISTING CONDUIT LOCATIONS SHALL BE CONSIDERED APPROXIMATE AND ARE TO BE VERIFIED BY THE CONTRACTOR PRIOR TO INSTALLING ANY UNDERGROUND CABLE.
 4. PARTIAL FIELD SURVEY WAS PERFORMED FOR THIS PROJECT. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS OF ALL UTILITIES AND EXISTING SIGNAL EQUIPMENT. ALL EXISTING CONDUIT LOCATIONS SHALL BE CONSIDERED APPROXIMATE AND ARE TO BE VERIFIED BY THE CONTRACTOR PRIOR TO EXCAVATION AND INSTALLING ANY UNDERGROUND CABLE.
 5. THE CONTRACTOR SHALL CONNECT THE PROPOSED CONDUIT TO THE EXISTING SPARE CONDUIT ELL OF THE LIGHTING CONTROL CENTER TO ROUTE THE LIGHTING CABLES AND CONNECT THE PROPOSED LUMINAIRES TO THE EXISTING CIRCUIT.

LEGEND		
	PROP.	EXIST.
LIGHT POLE		
PULL BOX		
GROUND MOUNTED CONTROL CENTER		

LIGHTING PLAN
WOERNER TEMPLE RD. / POOL ENTRANCE DRIVE



DESIGN AGENCY



DESIGNER

JAH

REVIEWER

KMG 2/18/25

PROJECT ID

25-002-CIP

SHEET

P.60

TOTAL

60

CITY OF DUBLIN, FRANKLIN COUNTY, OHIO

**THE OVERLOOK AT
TARTAN RIDGE**

FILE, MCKITRICK ROAD

APPENDIX SHEET 1

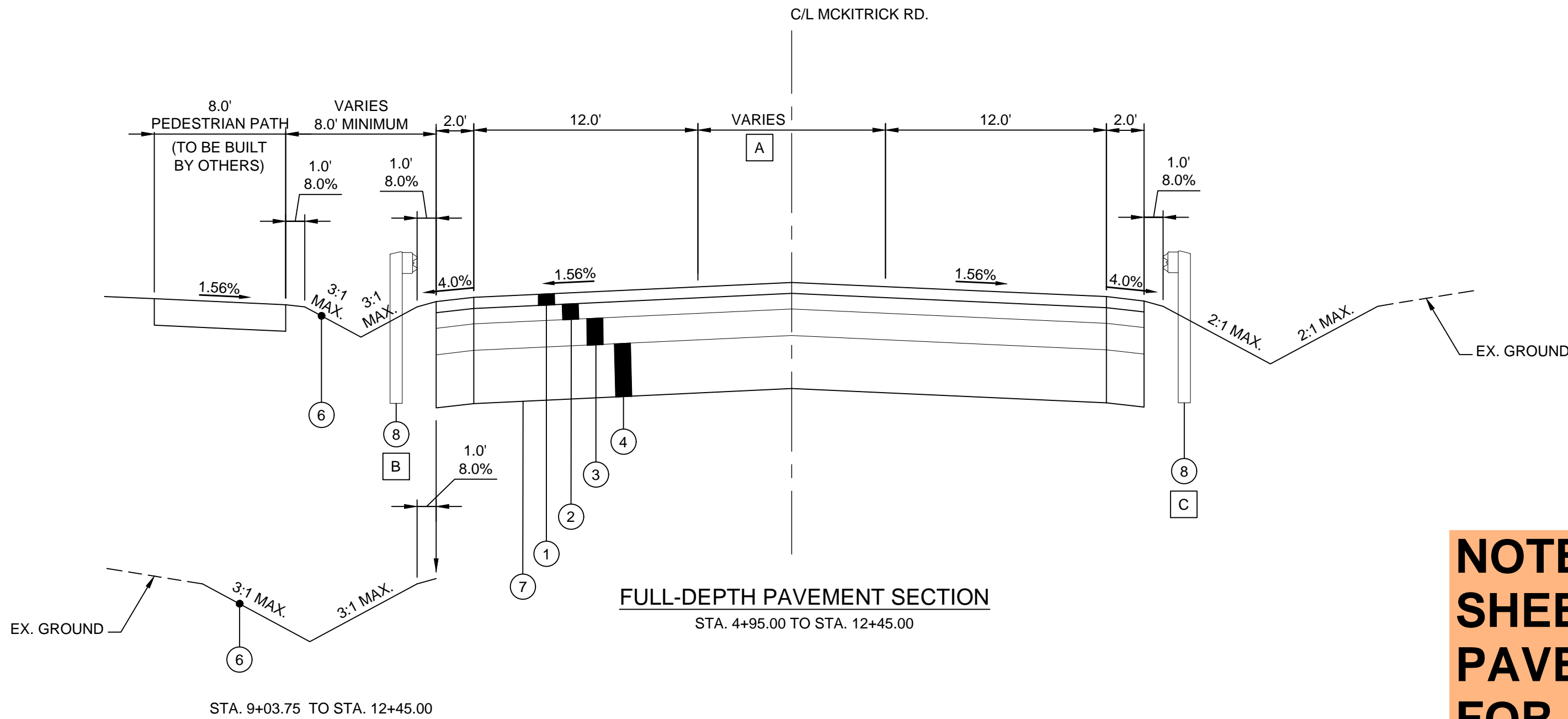
[illegible]

THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

1/23

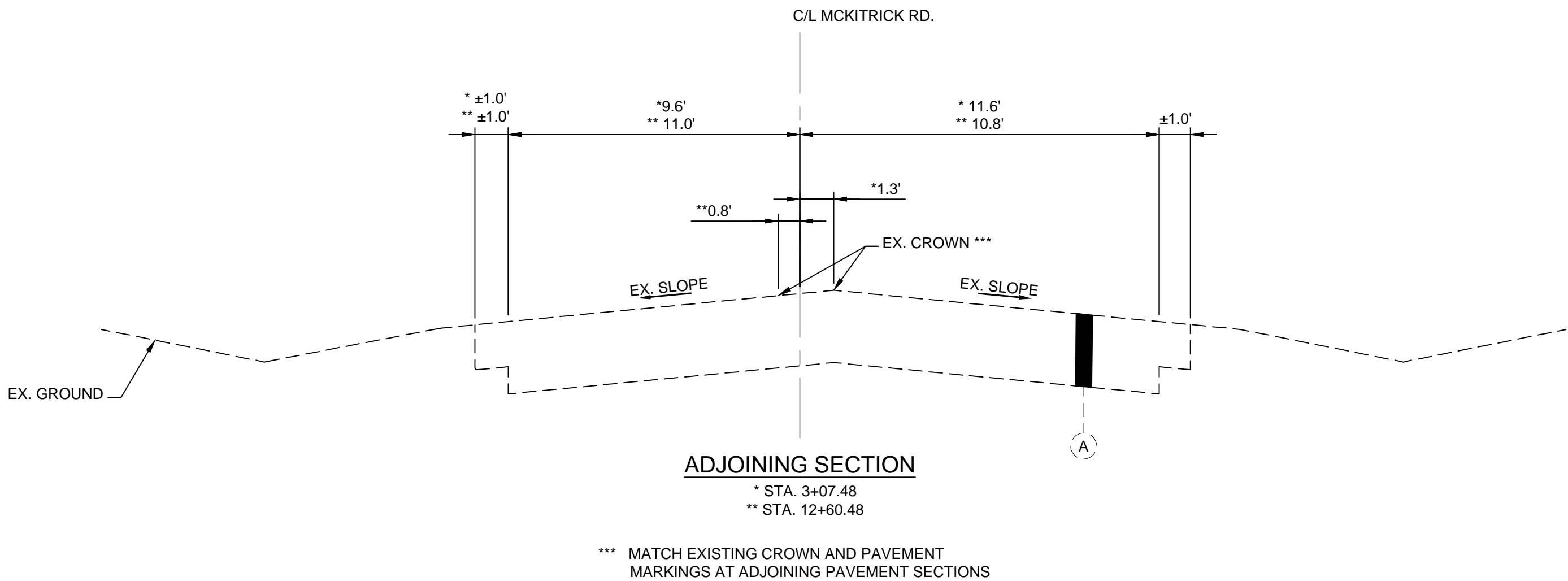
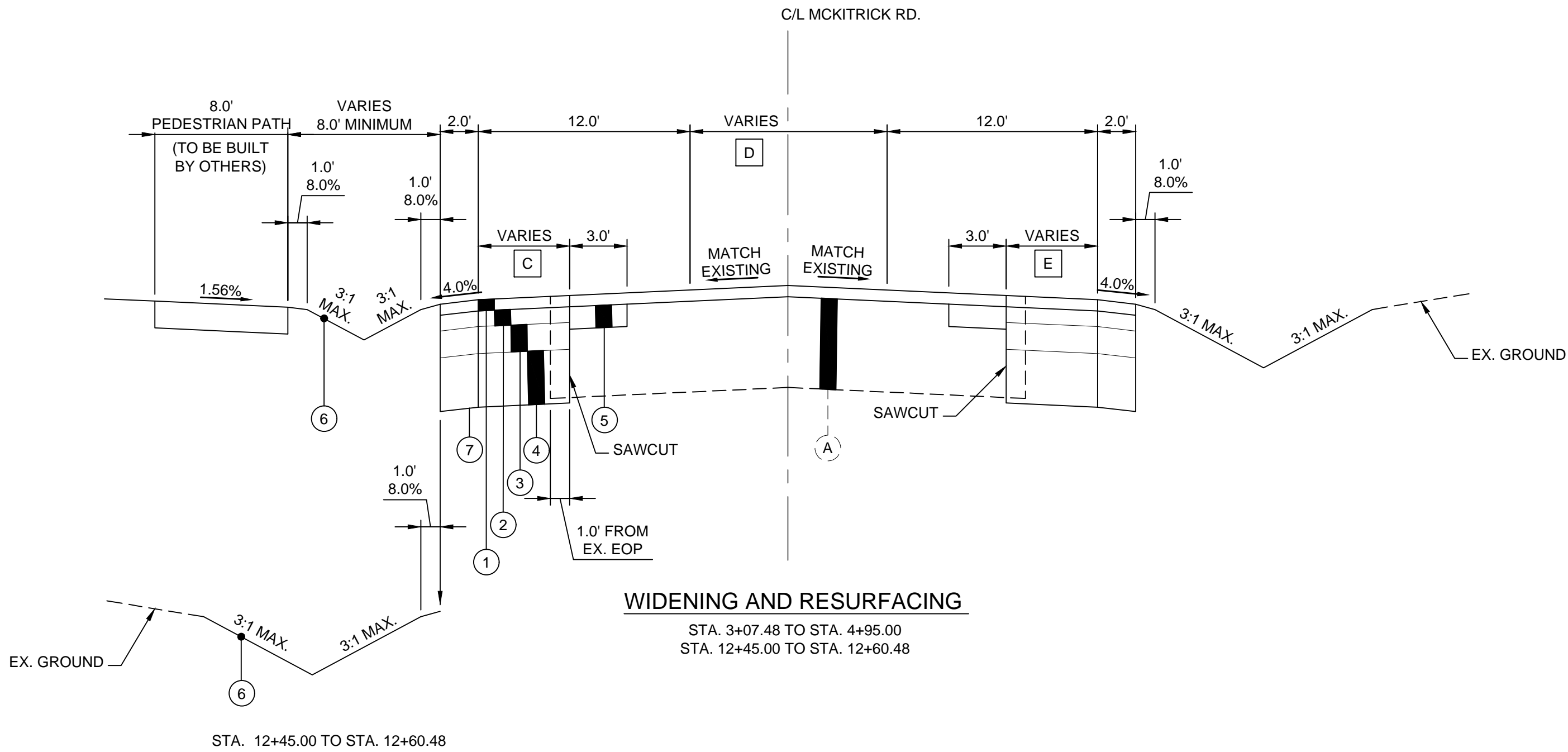
N:\0360\06838\00000_MCKITRICK RD\Design\Roadway\Sheets\06838_TYP001.dwg, 2/12/2021 1:02 PM, patton, john

- A** STA. 4+95.00 TO STA. 7+57.48 = 6.25' - 12.0'
STA. 7+57.48 TO STA. 9+60.48 = 12.0'
STA. 9+60.48 TO STA. 12+45.00 = 12.0' - 0.62'
- B** GUARDRAIL, TYPE MGS WITH LONG POST:
STA. 9+53.04 TO 9+91.50
STA. 10+27.10 TO 11+02.10
- C** GUARDRAIL, TYPE MGS WITH LONG POST:
STA. 7+15.95 TO STA. 8+65.95



**NOTE: REFER TO PROJECT PLANS
SHEET 40 FOR PAVEMENT DETAIL.
PAVEMENT DETAILS INCLUDED HERE
FOR HISTORICAL REFERENCE**

- C** STA. 3+07.48 TO STA. 4+95.00 = 1.0' - 4.89'
STA. 12+45.00 TO STA. 12+60.48 = 1.50' - 1.0'
- D** STA. 3+07.48 TO STA. 4+95.00 = 0' - 6.25'
STA. 12+45.00 TO STA. 12+60.48 = 0.62' - 0'
- E** STA. 3+07.48 TO STA. 4+95.00 = 1.0' - 5.52'
STA. 12+45.00 TO STA. 12+60.48 = 1.19' - 1.0'



- ① - ITEM 441 - 1-1/4" ASPHALT CONCRETE SURFACE COURSE TYPE 1 PG. 64-22 MEDIUM TRAFFIC
- ② - ITEM 441 - 1-3/4" ASPHALT CONCRETE INTERMEDIATE COURSE TYPE 2 PG.64-22 MEDIUM TRAFFIC
- ③ - ITEM 301 - 3" ASPHALT AGGREGATE BASE
- ④ - ITEM 304 - 6" AGGREGATE BASE
- ⑤ - ITEM 441 - 2-1/2" ASPHALT CONCRETE INTERMEDIATE COURSE TYPE 2 PG.64-22 MEDIUM TRAFFIC
- ⑥ - ITEM 659 - SEEDING AND MULCHING
- ⑦ - ITEM 204 - SUBGRADE COMPACTION
- ⑧ - ITEM 606 - GUARDRAIL, TYPE MGS WITH LONG POST

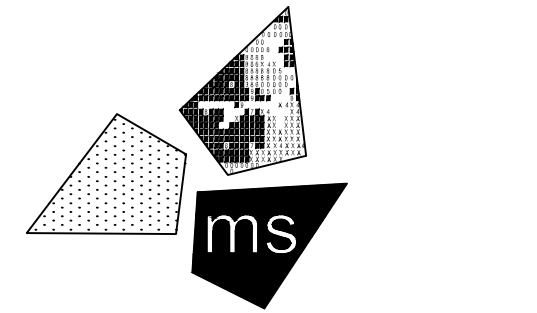
Ⓐ - EXISTING

APPENDIX SHEET 2

ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT
TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE
TYPICAL SECTION

SEAL

DRAWN: LLK
ENGINEER: JAP
CHECKED: DLT
PROJECT NO: 60-06838
SCALE: AS NOTED

DRAWING
2/23

N:\03\60\06838\00000_MCKITTRICK RD\Design(Roadway)\Sheets\06838_GEN001.dwg, 2/12/2021 1:02 PM, patton, john

GENERAL NOTES

1. CITY OF COLUMBUS AND OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS, CURRENT EDITIONS, AND ANY SUPPLEMENTS THERETO (HEREAFTER REFERRED TO AS STANDARD SPECIFICATIONS), SHALL GOVERN ALL CONSTRUCTION ITEMS UNLESS OTHERWISE NOTED. IF A CONFLICT BETWEEN SPECIFICATIONS IS FOUND, THE MORE STRICT SPECIFICATION WILL APPLY AS DECIDED BY THE CITY ENGINEER. ITEM NUMBERS LISTED REFER TO CITY OF COLUMBUS ITEM NUMBERS UNLESS OTHERWISE NOTED.
2. THE CITY ENGINEER WILL NOT BE RESPONSIBLE FOR MEANS, METHODS, PROCEDURES, TECHNIQUES, OR SEQUENCES OF CONSTRUCTION THAT ARE NOT SPECIFIED HEREIN. THE CITY ENGINEER WILL NOT BE RESPONSIBLE FOR SAFETY ON THE WORK SITE, OR FOR FAILURE BY THE CONTRACTOR TO PERFORM WORK ACCORDING TO CONTRACT DOCUMENTS.
3. THE DEVELOPER OR CONTRACTOR SHALL BE RESPONSIBLE TO OBTAIN ALL NECESSARY PERMITS INCLUDING BUT NOT LIMITED TO OHIO EPA PERMITS TO INSTALL (PTI) AND NOTICES OF INTENT (NOI), BUILDING PERMITS, ETC.
4. THE CONTRACTOR SHALL NOTIFY THE CITY OF DUBLIN DIVISION OF ENGINEERING IN WRITING AT LEAST 3 WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION.
5. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL FEDERAL, STATE AND LOCAL SAFETY REQUIREMENTS INCLUDING THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970. THE CONTRACTOR SHALL EXERCISE PRECAUTION ALWAYS FOR THE PROTECTION OF PERSONS (INCLUDING EMPLOYEES) AND PROPERTY. IT SHALL ALSO BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INITIATE, MAINTAIN AND SUPERVISE ALL SAFETY REQUIREMENTS, PRECAUTIONS AND PROGRAMS IN CONNECTION WITH THE WORK, INCLUDING THE REQUIREMENTS FOR CONFINED SPACES PER 29 CFR 191 O. 146.
6. FOLLOWING COMPLETION OF CONSTRUCTION OF THE SITE IMPROVEMENTS AND BEFORE REQUESTING OCCUPANCY, A PROOF SURVEY SHALL BE PROVIDED TO THE DIVISION OF ENGINEERING THAT DOCUMENTS "AS-BUILT" ELEVATIONS, DIMENSIONS, SLOPES AND ALIGNMENTS OF ALL ELEMENTS OF THIS PROJECT. THE PROOF SURVEY SHALL BE PREPARED, SIGNED AND SUBMITTED BY THE PROFESSIONAL ENGINEER WHO SEALED THE CONSTRUCTIONS DRAWINGS.
7. THE CONTRACTOR SHALL RESTRICT CONSTRUCTION ACTIVITY TO PUBLIC RIGHT-OF-WAY AND AREAS DEFINED AS PERMANENT AND/ OR TEMPORARY CONSTRUCTION EASEMENTS, UNLESS OTHERWISE AUTHORIZED BY THE CITY ENGINEER.
8. THE CONTRACTOR SHALL CAREFULLY PRESERVE BENCH MARKS, PROPERTY CORNERS, REFERENCE POINTS, STAKES AND OTHER SURVEY REFERENCE MONUMENTS OR MARKERS. IN CASES OF WILLFUL OR CARELESS DESTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORATIONS. RESETTING OF MARKERS SHALL BE PERFORMED BY AN OHIO PROFESSIONAL SURVEYOR AS APPROVED BY THE CITY ENGINEER.
9. NON-RUBBER TIRED VEHICLES SHALL NOT BE MOVED ON OR ACROSS PUBLIC STREETS OR HIGHWAYS WITHOUT THE WRITTEN PERMISSION OF THE CITY ENGINEER.
10. THE CONTRACTOR SHALL RESTORE ALL DISTURBED AREAS TO EQUAL OR BETTER CONDITION THAN EXISTED BEFORE CONSTRUCTION. DRAINAGE DITCHES OR WATER COURSES THAT ARE DISTURBED BY CONSTRUCTION SHALL BE RESTORED TO THE GRADES AND CROSS-SECTIONS THAT EXISTED BEFORE CONSTRUCTION.
11. TRACKING OR SPILLING MUD, DIRT OR DEBRIS UPON STREETS, RESIDENTIAL OR COMMERCIAL DRIVES, SIDEWALKS OR BIKE PATHS IS PROHIBITED ACCORDING TO SECTION 97.38 OF THE DUBLIN CODE OF ORDINANCES. ANY SUCH OCCURRENCE SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR AT NO COST TO THE CITY. IF THE CONTRACTOR FAILS TO REMOVE SAID MUD, DIRT, DEBRIS, OR SPILLAGE, THE CITY RESERVES THE RIGHT TO REMOVE THESE MATERIALS AND CLEAN AFFECTED AREAS, THE COST OF WHICH SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
12. DISPOSAL OF EXCESS EXCAVATION WITHIN SPECIAL FLOOD HAZARD AREAS (100-YEAR FLOODPLAIN) IS NOT PERMITTED.
13. ALL SIGNS, LANDSCAPING, STRUCTURES OR OTHER APPURTENANCES WITHIN RIGHT-OF-WAY DISTURBED OR DAMAGED DURING CONSTRUCTION SHALL BE REPLACED OR REPAIRED TO THE SATISFACTION OF THE CITY ENGINEER. THE COST OF THIS WORK SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
14. ALL FIELD TILE BROKEN OR ENCOUNTERED DURING EXCAVATION SHALL BE REPLACED OR REPAIRED AND CONNECTED TO THE PUBLIC STORM SEWER SYSTEM AS DIRECTED BY THE CITY ENGINEER. THE COST OF THIS WORK SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
15. ALL PRECAST CONCRETE PRODUCTS SHALL BE INSPECTED AT THE LOCATION OF MANUFACTURE. APPROVED PRECAST CONCRETE PRODUCTS WILL BE STOMPED OR HAVE SUCH IDENTIFICATION NOTING THAT INSPECTION HAS BEEN CONDUCTED BY THE CITY OF COLUMBUS. PRECAST CONCRETE PRODUCTS WITHOUT PROOF OF INSPECTION SHALL NOT BE APPROVED FOR INSTALLATION.
16. BACKFILL WITHIN A 1:1 INFLUENCE LINE OF EXISTING STRUCTURES (HOUSES, GARAGES, ETC.) OR PUBLIC INFRASTRUCTURE (PAVEMENT, CURBS, SIDEWALKS, BIKE PATHS, ETC.) SHALL BE COMPACTED GRANULAR BACKFILL ACCORDING TO ITEM 912 OF THE STANDARD SPECIFICATIONS OR FLOWABLE CDF, TYPE ILL ACCORDING TO ITEM 613. ITEM 911 OF THE STANDARD SPECIFICATIONS MAY BE USED ELSEWHERE.
17. THE CONTRACTOR SHALL SUBMIT A COPY OF THE APPROVED CONSTRUCTION DRAWINGS AND A LIST OF PROPOSED PRECAST CONCRETE PRODUCT MANUFACTURERS TO THE CITY OF COLUMBUS CONSTRUCTION INSPECTION DIVISION BEFORE COMMENCING CONSTRUCTION.

SEND THE INFORMATION TO THE FOLLOWING ADDRESS:
CONSTRUCTION INSPECTION DIVISION
CITY OF COLUMBUS
1800 EAST 17TH AVENUE
COLUMBUS, OHIO 43219

SEND A COPY OF THE TRANSMITTAL LETTER TO THE FOLLOWING ADDRESS:
DIVISION OF ENGINEERING
CITY OF DUBLIN
5800 SHIER RINGS ROAD
DUBLIN, OHIO 43016
18. ALL TRENCHES WITHIN PUBLIC RIGHT-OF-WAY SHALL BE BACKFILLED ACCORDING TO THE APPROVED CONSTRUCTION DRAWINGS OR SECURELY PLATED DURING NONWORKING HOURS. TRENCHES OUTSIDE THESE AREAS SHALL BE BACKFILLED OR SHALL BE PROTECTED BY APPROVED TEMPORARY FENCING OR BARRICADES DURING NONWORKING HOURS. CLEAN-UP SHALL FOLLOW CLOSELY BEHIND THE TRENCHING OPERATION.
19. ALL TREES WITHIN THE CONSTRUCTION AREA NOT SPECIFICALLY DESIGNATED FOR REMOVAL SHALL BE PRESERVED. WHETHER SHOWN OR NOT SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS, TREES TO BE PRESERVED SHALL BE PROTECTED WITH HIGH VISIBILITY FENCING PLACED A MINIMUM 15 FEET FROM THE TREE TRUNK. TREES 6 -INCHES OR GREATER AT DBH (DIAMETER BREAST HEIGHT) MUST BE PROTECTED WITH FENCING PLACED AT THE CRITICAL ROOT ZONE OR 15 FEET, WHICHEVER IS GREATER. TREES NOT INDICATED ON THE APPROVED CONSTRUCTION DRAWINGS FOR REMOVAL MAY NOT BE REMOVED WITHOUT PRIOR APPROVAL OF THE DIVISION OF ENGINEERING.

20. CONDUIT MUST BE DIRECTIONALLY BORED ACROSS STREETS INSTEAD OF OPEN CUT, UNLESS SPECIFICALLY APPROVED BY THE CITY ENGINEER. USE OF PNEUMATIC AIR RAM DEVICES IS NOT PERMITTED. PERMITS TO CONSTRUCT IN THE RIGHT-OF-WAY OF EXISTING STREETS MUST BE OBTAINED FROM THE CITY OF DUBLIN DIVISION OF ENGINEERING BEFORE COMMENCING CONSTRUCTION. SHOULD OPEN CUTTING OF EXISTING PAVEMENT BE PERMITTED, CONTROLLED DENSITY BACKFILL (TYPE ILL) SHALL BE USED IN PLACE OF COMPACTED GRANULAR BACKFILL, ACCORDING TO ITEM 613 OF THE STANDARD SPECIFICATIONS.
21. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONDITION OF TRENCHES WITHIN THE RIGHT-OF-WAY AND PUBLIC EASEMENTS FOR A PERIOD OF ONE YEAR FROM THE FINAL ACCEPTANCE OF THE WORK, AND SHALL MAKE ANY NECESSARY REPAIRS AT NO COST TO THE CITY.
22. PAVEMENTS SHALL BE CUT IN NEAT, STRAIGHT LINES THE FULL DEPTH OF THE EXISTING PAVEMENT, OR AS REQUIRED BY THE CITY ENGINEER. PAVEMENT REPLACEMENT SHALL BE CONDUCTED ACCORDING TO CITY OF COLUMBUS STANDARD DRAWING 1441 DR. A AND APPLICABLE CITY OF DUBLIN STANDARD DRAWINGS. THE REPLACEMENT OF DRIVEWAYS, HANDICAPPED RAMPS, SIDEWALKS, BIKE PATHS, PARKING LOT PAVEMENT, ETC. SHALL BE PROVIDED ACCORDING TO THE APPROVED CONSTRUCTION DRAWINGS AND CITY OF DUBLIN STANDARD CONSTRUCTION DRAWINGS.
23. TREE TRIMMING WITHIN THE CONSTRUCTION ZONE IS TO BE COMPLETED BY A CERTIFIED ARBORIST. AT THE COMPLETION OF THE PROJECT THE ARBORIST IS TO RETURN AND TRIM ANY BROKEN BRANCHES AS NEEDED.
24. ANY MODIFICATION TO THE WORK SHOWN ON DRAWINGS MUST HAVE PRIOR WRITTEN APPROVAL BY THE CITY ENGINEER, CITY OF DUBLIN.
25. ALL INLETS SHALL BE CHANNELIZED.
26. PARK AREAS SHALL BE FINE-GRADED AND SEEDED WITH THE FOLLOWING MIXTURE:
IMPROVED KENTUCKY BLUEGRASS, 40% OF WEIGHT (2 VARIETIES IN EQUAL PARTS)
IMPROVED PERENNIAL RYE, 60% OF WEIGHT (2 VARIETIES IN EQUAL PARTS)
GERMINATION RATE: 85%
APPLICATION RATE: 7 LBS PER 1000 SQ FT OR AS DIRECTED BY THE DIVISION OF PARKS & RECREATION, CITY OF DUBLIN, OHIO.
27. TRAFFIC CONTROL AND OTHER REGULATORY SIGNS SHALL BE TYPE S WITH A SQUARE POST ANCHOR BASE INSTALLATION AND MEET ALL REQUIREMENTS OF ODOT TC-41.20 AND APPLICABLE CITY OF DUBLIN SPECIFICATIONS.
28. STREET SIGNS SHALL MEET ALL CITY OF DUBLIN SPECIFICATIONS WITH LETTERING COLORED IN WHITE DISPLAYED OVER A BROWN BACKGROUND. SIGN TUBING SHALL BE BROWN IN COLOR AND CONFORM WITH THE TYPE S, SQUARE POST ANCHOR BASE INSTALLATION REQUIREMENTS OF ODOT TC-41.20.

UTILITIES

1. THE FOLLOWING UTILITIES ARE KNOWN TO BE LOCATED WITHIN THE LIMITS OF THIS PROJECT:

COLUMBIA GAS OF OHIO
ATTN. TAMMY SCHMID
200 CIVIC CENTER DR., 4TH FLOOR
COLUMBUS, OHIO 43215
1-800-440-6111

NISOURCE GAS DISTRIBUTION
(COLUMBIA GAS)
ROB CALDWELL
3550 JOHNNY APPLEESEED COURT
COLUMBUS, OHIO 43231
614-818-2104
rcaldwell@nisource.com

AMERICAN ELECTRIC POWER
ROBIN HAND
ENGINEERING LIAISON COORDINATOR
850 TECH CENTER DRIVE
GAHANNA, OHIO 43230-6605
614-883-6829

CITY OF DUBLIN DIVISION
OF ENGINEERING
PAUL HAMMERSMITH, P.E.
5800 SHIER RINGS ROAD
DUBLIN, OHIO 43016
614-410-4600

UNION RURAL ELECTRIC
COOPERATIVE, INC.
HEDY NELSON
P.O. BOX 393
15461 US HWY 36 - MARYSVILLE,
OHIO 43040
937-642-1826 EXT.9245
hedy@ure.com

FRONTIER TELECOM
ROB CHANDLER
19 EAST CENTRAL AVE.
DELAWARE, OH 43015
740-802-8990
Robert.L.Chandler@ftr.com

2. THE CONTRACTOR SHALL GIVE NOTICE OF INTENT TO CONSTRUCT TO OHIO UTILITIES PROTECTION SERVICE (TELEPHONE NUMBER 800-362-2764), PRODUCER'S UNDERGROUND PROTECTION SERVICE (TELEPHONE NUMBER 614-587-0486), AND TO OWNERS OF UNDERGROUND UTILITIES THAT ARE NOT MEMBERS OF A REGISTERED UNDERGROUND PROTECTION SERVICE. NOTICE SHALL BE GIVEN AT LEAST 2 WORKING DAYS BEFORE START OF CONSTRUCTION.
3. THE IDENTITY AND LOCATIONS OF EXISTING UNDERGROUND UTILITIES IN THE CONSTRUCTION AREA HAVE BEEN SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS AS ACCURATELY AS PROVIDED BY THE OWNER OF THE UNDERGROUND UTILITY. THE CITY OF DUBLIN AND THE CITY ENGINEER ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OR DEPTHS OF UNDERGROUND FACILITIES SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS. IF DAMAGE IS CAUSED, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF THE SAME AND FOR ANY RESULTING CONTINGENT DAMAGE.
4. LOCATION, SUPPORT, PROTECTION AND RESTORATION OF ALL EXISTING UTILITIES AND APPURTENANCES, WHETHER SHOWN OR NOT SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
5. WHEN UNKNOWN OR INCORRECTLY LOCATED UNDERGROUND UTILITIES ARE ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER AND THE CITY ENGINEER.
6. PUBLIC STREET LIGHTING MAY BE IN THE VICINITY OF THIS PROJECT. CONTACT THE CITY OF DUBLIN, DIVISION OF ENGINEERING AT 410-4637, TWO DAYS PRIOR TO BEGINNING WORK.

TRAFFIC CONTROL

1. TRAFFIC CONTROL SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR ACCORDING TO OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), CURRENT EDITION.
2. ALL TRAFFIC LANES OF PUBLIC ROADWAYS SHALL BE FULLY OPEN TO TRAFFIC FROM 7:00 AM TO 9:00 AM AND FROM 4:00 PM TO 6:00 PM UNLESS AUTHORIZED DIFFERENTLY BY THE CITY ENGINEER. AT ALL OTHER HOURS THE CONTRACTOR SHALL MAINTAIN MINIMUM ONE-LANE TWO-WAY TRAFFIC. UNIFORMED, OFF-DUTY POLICE OFFICERS SHALL REPLACE FLAGMEN DESIGNATED BY THE OMUTCD, AND SHALL BE PRESENT WHENEVER ONE-LANE, TWO-WAY TRAFFIC CONTROL IS IN EFFECT. POLICE CRUISERS MAY BE REQUIRED AS DIRECTED BY THE CITY ENGINEER.
3. IF THE CITY ENGINEER DETERMINES PROPER PROVISIONS FOR TRAFFIC CONTROL ARE NOT BEING PROVIDED BY THE CONTRACTOR, THE CITY ENGINEER SHALL ASSIGN UNIFORMED, OFF-DUTY POLICE OFFICERS TO THE PROJECT AT NO COST TO THE CITY.
4. STEADY-BURNING, TYPE 'C' LIGHTS SHALL BE REQUIRED ON ALL BARRICADES, DRUMS, AND SIMILAR TRAFFIC CONTROL DEVICES IN USE AT NIGHT.
5. ACCESS FROM PUBLIC ROADWAYS TO ALL ADJOINING PROPERTIES FOR EXISTING RESIDENTS OR BUSINESSES SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT FOR MAIL, PUBLIC WATER AND SANITARY SEWER SERVICE, AND EMERGENCY VEHICLES. THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN DETAILING THE PROPOSED MAINTENANCE OF TRAFFIC PROCEDURES. THE TRAFFIC CONTROL PLAN MUST INCORPORATE ANY TRAFFIC CONTROL DETAILS CONTAINED HEREIN. THE TRAFFIC CONTROL PLAN PROPOSED BY THE CONTRACTOR MUST BE APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION.

EROSION AND SEDIMENT CONTROL

1. THE CONTRACTOR OR DEVELOPER IS RESPONSIBLE FOR SUBMITTING A NOTICE OF INTENT (NOI) TO BE REVIEWED AND APPROVED BY THE OHIO EPA. THE NOI MUST BE SUBMITTED TO DEPA 45 DAYS PRIOR TO THE START OF CONSTRUCTION AND MAY ENTITLE COVERAGE UNDER THE OHIO EPA GENERAL PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY. A PROJECT LOCATION MAP MUST BE SUBMITTED WITH THE NOI. A SEDIMENT AND EROSION CONTROL PLAN MUST BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL IF A SEDIMENT AND EROSION CONTROL PLAN HAS NOT ALREADY BEEN INCLUDED WITH THE APPROVED CONSTRUCTION DRAWINGS. THIS PLAN MUST BE MADE AVAILABLE AT THE PROJECT SITE AT ALL TIMES. THE DESIGN OF EROSION CONTROL SYSTEMS SHALL FOLLOW THE REQUIREMENTS OF OHIO EPA, ITEM 207 OF OHIO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, AND THE CITY ENGINEER. AN INDIVIDUAL NPDES STORMWATER DISCHARGE PERMIT MAY BE REQUIRED. THE CONTRACTOR SHALL BE CONSIDERED THE PERMITTEE.
2. THE CONTRACTOR SHALL PROVIDE SEDIMENT CONTROL AT ALL POINTS WHERE STORM WATER RUNOFF LEAVES THE PROJECT, INCLUDING WATERWAYS, OVERLAND SHEET FLOW, AND STORM SEWERS.
3. ACCEPTED METHODS OF PROVIDING EROSION/SEDIMENT CONTROL INCLUDE BUT ARE NOT LIMITED TO: SEDIMENT BASINS, SILT FILTER FENCE, AGGREGATE CHECK DAMS, AND TEMPORARY GROUND COVER. HAY OR STRAW BALES ARE NOT PERMITTED.
4. THE CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE OF THE WORK AREA AT ALL TIMES CONSISTENT WITH EROSION CONTROL PRACTICES.
5. DISTURBED AREAS THAT WILL REMAIN UNWORKED FOR 14 DAYS OR MORE SHALL BE SEEDED OR PROTECTED WITHIN SEVEN CALENDAR DAYS OF THE DISTURBANCE. OTHER SEDIMENT CONTROLS THAT ARE INSTALLED SHALL BE MAINTAINED UNTIL VEGETATIVE GROWTH HAS BEEN ESTABLISHED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL TEMPORARY SEDIMENT DEVICES AT THE CONCLUSION OF CONSTRUCTION BUT NOT BEFORE GROWTH OF PERMANENT GROUND COVER.

BLASTING (IF PERMITTED)

1. THE CONTRACTOR MUST OBTAIN A BLASTING PERMIT FROM WASHINGTON TOWNSHIP FIRE DEPARTMENT PRIOR TO BLASTING FOR ROCK EXCAVATION. THE CONTRACTOR SHALL SUBMIT BLASTING REPORTS UPON COMPLETION OF BLASTING TO THE CITY ENGINEER, THE OWNER, AND THE OWNER'S ENGINEER. TOP OF ROCK ELEVATIONS SHALL BE SHOWN ON "AS-BUILT" CONSTRUCTION DRAWINGS.

SANITARY SEWERS NOTES 1-20 DELETED

WATER LINE NOTES 1-17 DELETED

STORM SEWERS

1. ALL STORM WATER DETENTION AND RETENTION AREAS AND MAJOR FLOOD ROUTING SWALES SHALL BE CONSTRUCTED TO FINISH GRADE AND HYDRO-SEEDED AND HYDRO-MULCHED ACCORDING TO ITEMS 203 AND 659 OF THE STANDARD SPECIFICATIONS.
2. WHERE PRIVATE STORM SEWERS CONNECT TO PUBLIC STORM SEWERS, THE LAST RUN OF PRIVATE STORM SEWER CONNECTING TO THE PUBLIC STORM SEWER SHALL BE REINFORCED CONCRETE PIPE CONFORMING TO ASTM DESIGNATION C76, WALL 8, CLASS IV FOR PIPE DIAMETERS 12 INCHES TO 15 INCHES, CLASS ILL FOR 1 8 INCHES TO 24 INCH PIPES, AND 27 INCHES AND LARGER PIPE SHALL BE CLASS II, UNLESS OTHERWISE SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS. INSPECTION IS REQUIRED BY THE CITY OF DUBLIN'S DIVISION OF ENGINEERING.
3. GRANULAR BACKFILL SHALL BE COMPACTED GRANULAR MATERIAL ACCORDING TO ITEM 912 OF THE STANDARD SPECIFICATIONS OR CONTROLLED DENSITY BACKFILL ACCORDING TO ITEM 613, TYPE ILL OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE CITY ENGINEER.
4. ALL STORM SEWERS SHALL BE REINFORCED CONCRETE PIPE CONFORMING TO ASTM DESIGNATION C76, WALL 8, CLASS IV FOR PIPE DIAMETERS 12 INCHES TO 15 INCHES, CLASS ILL FOR 1 8 INCHES TO 24 INCH PIPES, AND 27 INCHES AND LARGER PIPE SHALL BE CLASS II, UNLESS OTHERWISE SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS.
5. HEADWALLS AND ENDWALLS SHALL BE REQUIRED AT ALL STORM SEWER INLETS OR OUTLETS TO AND FROM STORMWATER MANAGEMENT FACILITIES. NATURAL STONE AND/OR BRICK APPROVED BY THE CITY ENGINEER SHALL BE PROVIDED ON ALL VISIBLE HEADWALLS AND/OR ENDWALLS AND OTHER EXPOSED CONCRETE SURFACES. SURFACES TO BE ACID WASHED BEFORE APPROVAL OF STONE FACING.
6. STORM INLETS OR CATCH BASINS SHALL BE CHANNELIZED AND HAVE BICYCLE SAFE GRATES. MANHOLE LIDS SHALL INCLUDE CITY OF DUBLIN LOGO AND ALL CURB INLET AND CATCH BASIN GRATES SHALL INCLUDE ENGRAVED LETTERING: "DUMP NO WASTE; DRAINS TO RIVER".
7. STORM SEWER OUTLETS GREATER THAN 18 INCHES IN DIAMETER ACCESSIBLE FROM STORMWATER MANAGEMENT FACILITIES OR WATERCOURSES SHALL BE PROVIDED WITH SAFETY GRATES, AS APPROVED BY THE CITY ENGINEER.

MAIL DELIVERY

1. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT U.S. MAIL DELIVERY WITHIN THE PROJECT LIMITS IS NOT DISRUPTED BY CONSTRUCTION OPERATIONS. THIS RESPONSIBILITY IS LIMITED TO RELOCATION OF MAILBOXES TO A TEMPORARY LOCATION THAT WILL ALLOW THE COMPLETION OF THE WORK AND SHALL ALSO INCLUDE THE RESTORATION OF MAILBOXES TO THEIR ORIGINAL LOCATION OR APPROVED NEW LOCATION. ANY RELOCATION OF MAILBOX SERVICES MUST BE FIRST COORDINATED WITH THE US POSTAL SERVICE AND THE HOMEOWNER.
2. BEFORE RELOCATING ANY MAILBOXES, THE CONTRACTOR SHALL CONTACT THE U.S. POSTAL SERVICE AND RELOCATE MAILBOXES ACCORDING TO THE REQUIREMENTS OF THE POSTAL SERVICE.

USE OF FIRE HYDRANTS

1. THE CONTRACTOR SHALL MAKE PROPER ARRANGEMENTS WITH THE DUBLIN SERVICE DEPARTMENT AND THE COLUMBUS DIVISION OF WATER FOR THE USE OF FIRE HYDRANTS WHEN USED FOR WORK PERFORMED UNDER THIS CONTRACT AND PROVIDE THE CITY OF DUBLIN A COPY OF THE HYDRANT USAGE PERMIT OBTAINED FROM THE CITY OF COLUMBUS. THE CONTRACTOR SHALL ALSO SEND COPIES OF PERMITS OBTAINED FROM DUBLIN AND COLUMBUS TO THE WASHINGTON AND/ OR PERRY TOWNSHIP FIRE DEPARTMENT. PERMITS SHALL BE KEPT AT THE CONSTRUCTION SITE AT ALL TIMES.
2. BEFORE THE FINAL ESTIMATE IS PAID, THE CONTRACTOR SHALL SUBMIT A LETTER FROM THE CITY OF COLUMBUS DIVISION OF WATER TO THE CITY ENGINEER STATING THAT THE CONTRACTOR HAS RETURNED THE SIAMESE VALVE TO THE CITY OF COLUMBUS AND HAS PAID ALL COSTS ARISING FROM THE USE OF THE FIRE HYDRANTS.

MISCELLANEOUS

1. HP STORM AND HP SANITITE OR APPROVED EQUAL ARE THE ONLY APPROVED ALTERNATIVES TO REINFORCED CONCRETE PIPE IN PAVED AND NON-PAVED AREAS AS APPROVED BY THE CITY ENGINEER. THIS INCLUDES APPLICATIONS INSIDE THE RIGHT-OF-WAY.
2. HP STORM AND HP SANITITE OR APPROVED EQUAL PIPE JOINTS SHALL BE WATERTIGHT ACCORDING TO REQUIREMENTS OF ASTM D3212. PIPES SHALL BE JOINED WITH A GASKETED INTEGRAL BELL & SPIGOT JOINT MEETING THE REQUIREMENTS OF ASTM F2881 (HP STORM) AND ASTM F2764 (HP SANITITE). GASKETS SHALL BE INSTALLED BY PIPE MANUFACTURER AND COVERED WITH A REMOVABLE, PROTECTIVE WRAP TO ENSURE THE GASKET IS FREE FROM DEBRIS. A JOINT LUBRICANT AVAILABLE FROM THE MANUFACTURER SHALL BE USED ON THE GASKET AND BELL DURING JOINT ASSEMBLY.
3. ALL BEDDING MATERIAL SHALL BE IN ACCORDANCE WITH CITY OF COLUMBUS STANDARD CONSTRUCTION DRAWING AA-S149.
4. BACKFILL MATERIAL SHALL BE PLACED IN ACCORDANCE WITH ITEM 911 OR ITEM 912 OF THE CITY OF COLUMBUS CONSTRUCTION MATERIAL SPECIFICATIONS (CMS).
5. BACKFILL MATERIAL IN AREAS LOCATED OUTSIDE THE PUBLIC RIGHT-OF-WAY SHALL BE PLACED IN ACCORDANCE WITH ITEM 901 OF THE CITY OF COLUMBUS (CMS).
6. HEIGHT OF COVER SHALL BE IN ACCORDANCE WITH THE OHIO DEPARTMENT OF TRANSPORTATION (ODOT) LOCATION AND DESIGN (L&D) MANUAL, VOLUME TWO, SECTION 1008.3.1.
7. ALL HP STORM AND HP SANITITE PIPE (FOR STORM SEWER) SHALL BE MANDREL TESTED IN ACCORDANCE WITH CITY OF COLUMBUS ITEM 901.21, WITH THE EXCEPTION THAT THE WAITING PERIOD PRIOR TO TESTING SHALL BE 30 DAYS.
8. FOR ANY AND ALL INSTALLATIONS REQUIRING THE MINIMIZATION OF TRENCH WATER MIGRATION, ANTI-SEEP COLLARS SHALL BE INSTALLED IN ACCORDANCE WITH THE ODOT L&D MANUAL, VOLUME TWO SECTION 1118.4.1.2 AND ODOT STANDARD HYDRAULIC CONSTRUCTION DRAWING WQ-1.2.

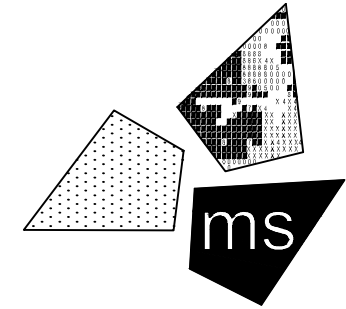
ISSUED:	DATE:

NOTICE

THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT

STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.

engineers, architects, planners

2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT

THE OVERLOOK AT TARTAN RIDGE
MCKITTRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

GENERAL NOTES

SEAL

DRAWN: LLK

ENGINEER: JAP

CHECKED: DLT

PROJECT NO: 60-06838

SCALE: AS NOTED

DRAWING

APPENDIX SHEET 3

ITEM 614, MAINTAINING TRAFFIC

1. THE CONTRACTOR SHALL INFORM ALL ENTITIES LISTED IN THE NOTIFICATIONS NOTE EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.
2. ONLY DURING OFF-PEAK PERIODS SHALL THE CONTRACTOR INSTALL AND SUBSEQUENTLY RESET ALL TRAFFIC CONTROL NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE.
3. PRIOR TO OPENING TRAFFIC, EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK

NOTICE OF CLOSURE SIGN TIME TABLE

MEASUREMENT AND PAYMENT

ITEM 614, MAINTAINING TRAFFIC LUMP SUM

ITEM 614, DETOUR SIGNING LUMP SUM

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616, WATER 5.92 M. GAL.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMTCD, A UNIFORMED CITY OF MARYSVILLE LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE CITY OF MARYSVILLE POLICE DEPARTMENT) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMTCD, A UNIFORMED CITY OF MARYSVILLE LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE CITY OF MARYSVILLE POLICE DEPARTMENT) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN A NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING THE SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

THE CONTRACTOR SHALL SUBMIT LEO REQUEST FORMS TO THE CITY OF MARYSVILLE POLICE DEPARTMENT AT police@marysvilleohio.org A MINIMUM OF 3 BUSINESS DAYS PRIOR TO THE DATE CONSTRUCTION ACTIVATES REQUIRE A LEO.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	32 HOURS
--	----------

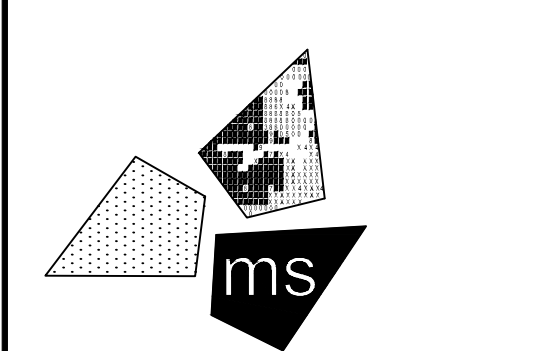
THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE CITY OF MARYSVILLE POLICE DEPARTMENT.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE, AS PER PLAN.

[illegible]

THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

--	--



PROJECT
THE OVERLOOK AT
TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

SEAL

DRAWN: LLK

ENGINEER: JAP

CHECKED: DLT

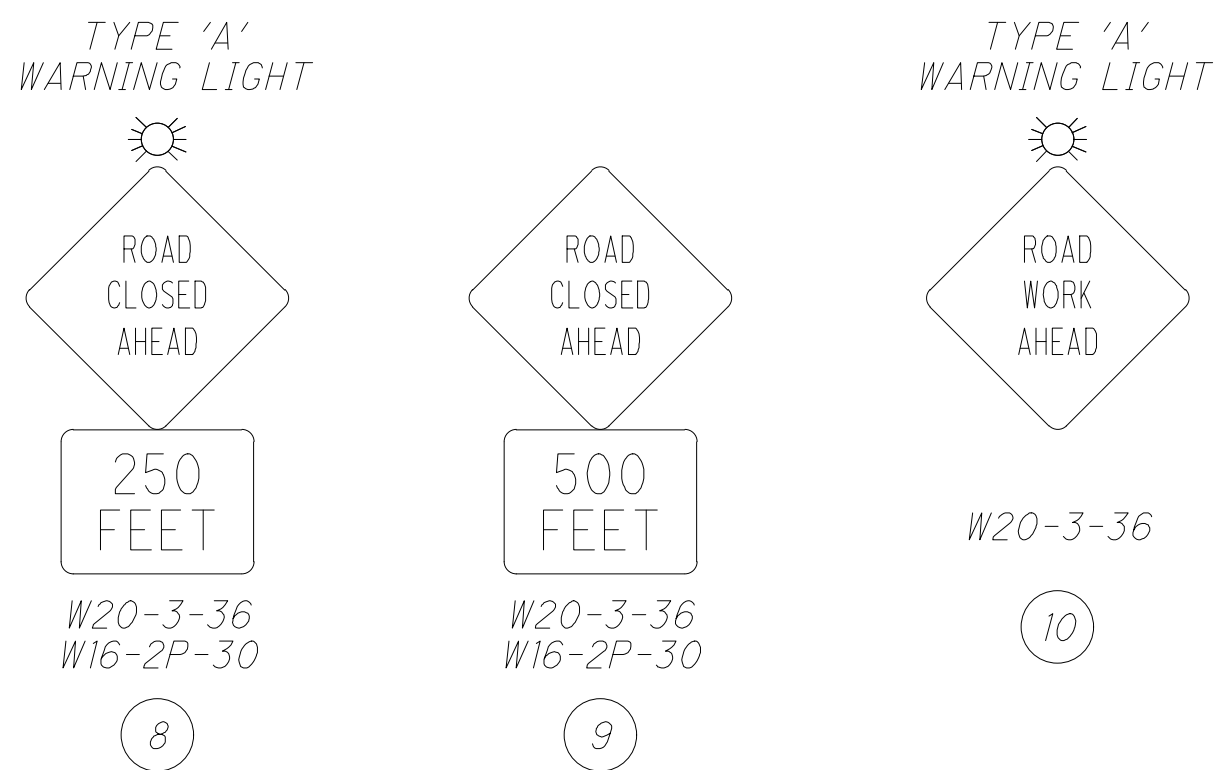
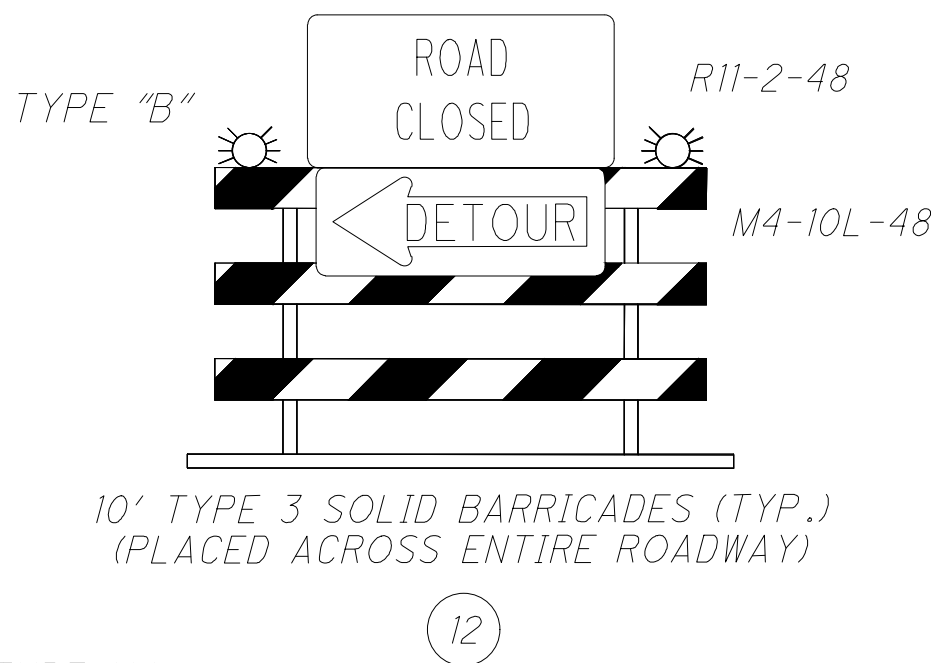
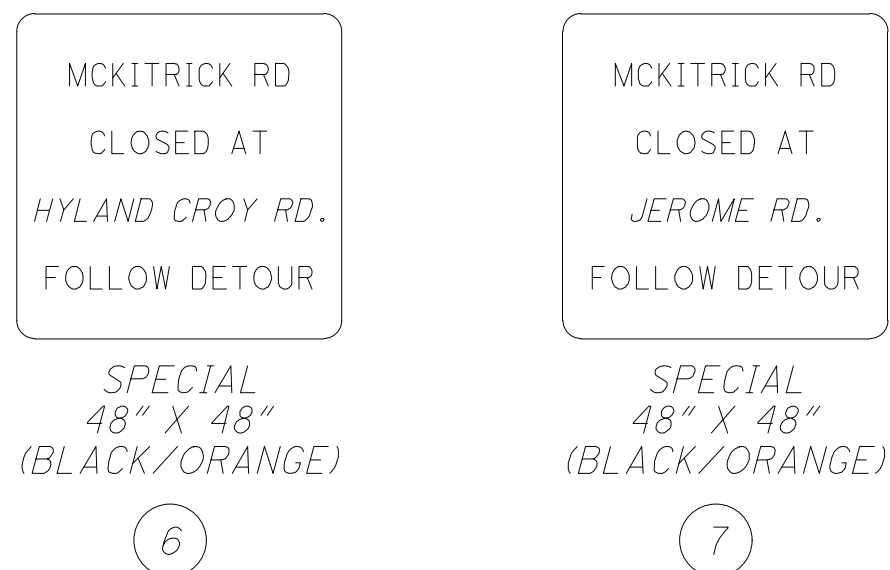
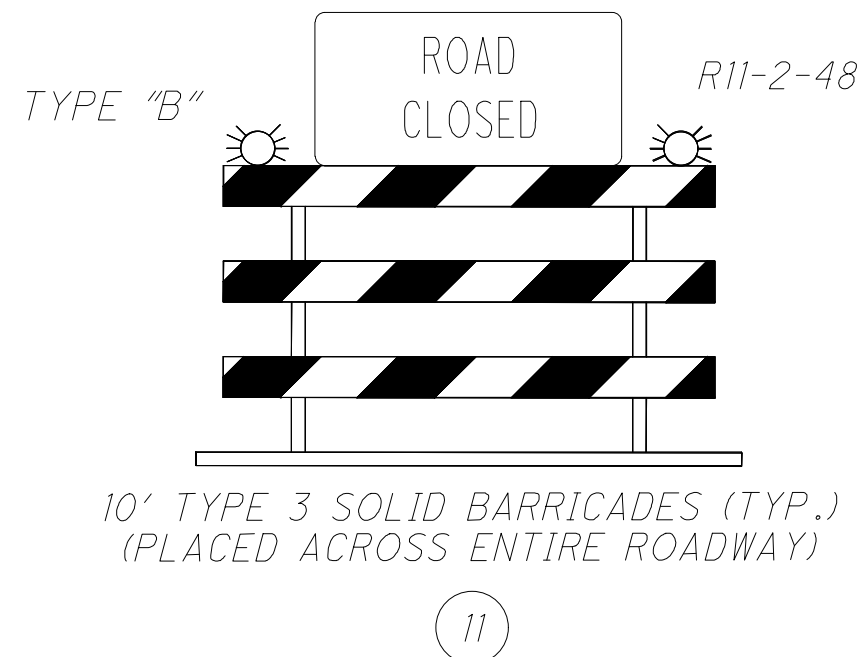
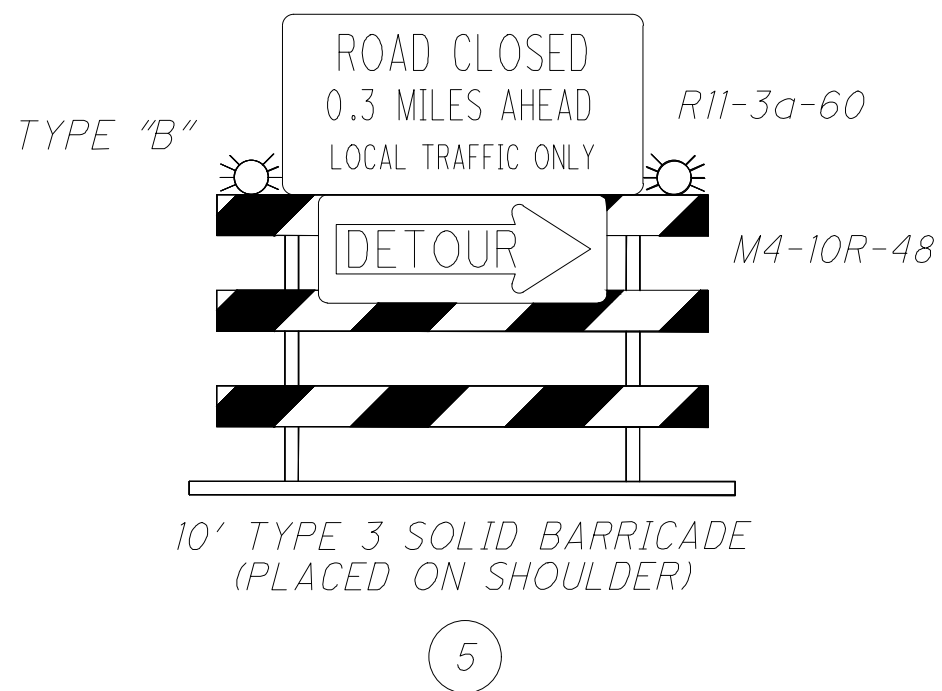
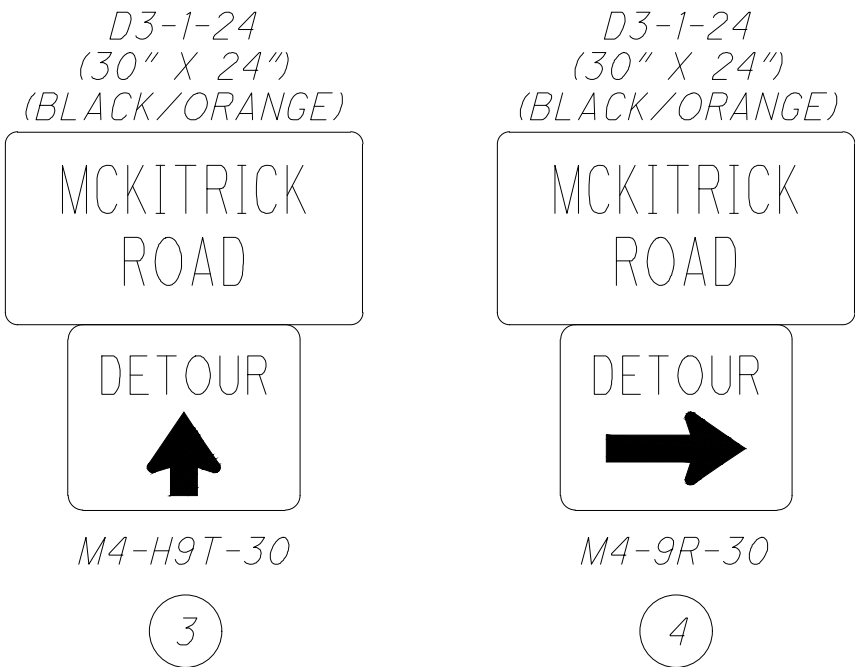
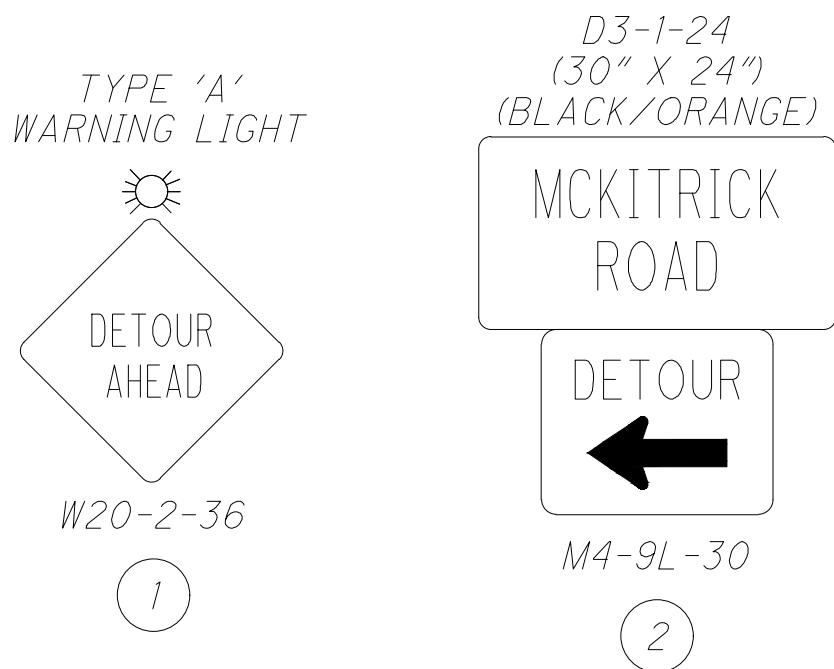
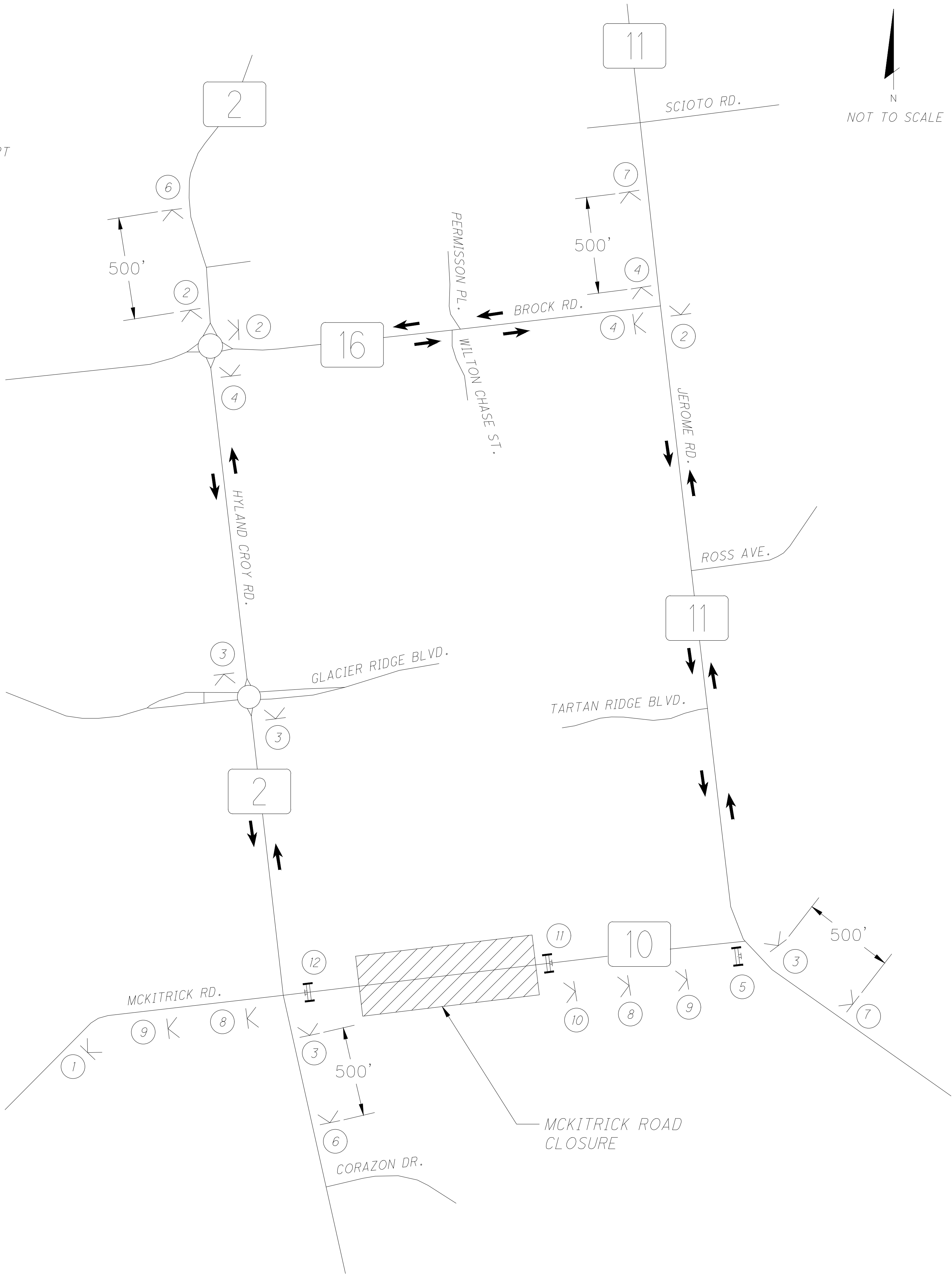
PROJECT NO: 60-06838

SCALE: AS NOTED

DRAWING

4/23

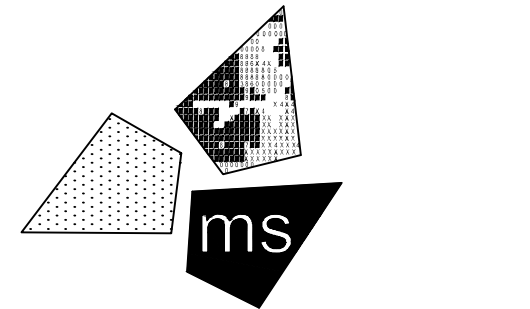
N:\0360\06838\00000_MCKITRICK RD\Design\Roadway\Sheets\06838_MOT001.dwg, 2/12/2021 1:02 PM, patton, john



ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE
MCKITRICK RD
DETOUR PLAN

SEAL

DRAWN: LLK
ENGINEER: JAP
CHECKED: DLT
PROJECT NO: 60-06838
SCALE: AS NOTED

DRAWING

5/23

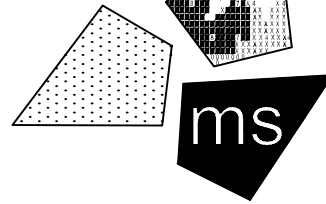
6/23

NOTICE

THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT

STAVROFF LAND AND
DEVELOPMENT, INC.



ms consultants, inc.

engineers, architects, planners

2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT

THE OVERLOOK AT
TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

GENERAL SUMMARY

SEAL

DRAWN: LLK

ENGINEER: JAP

CHECKED: DLT

PROJECT NO: 60-06838

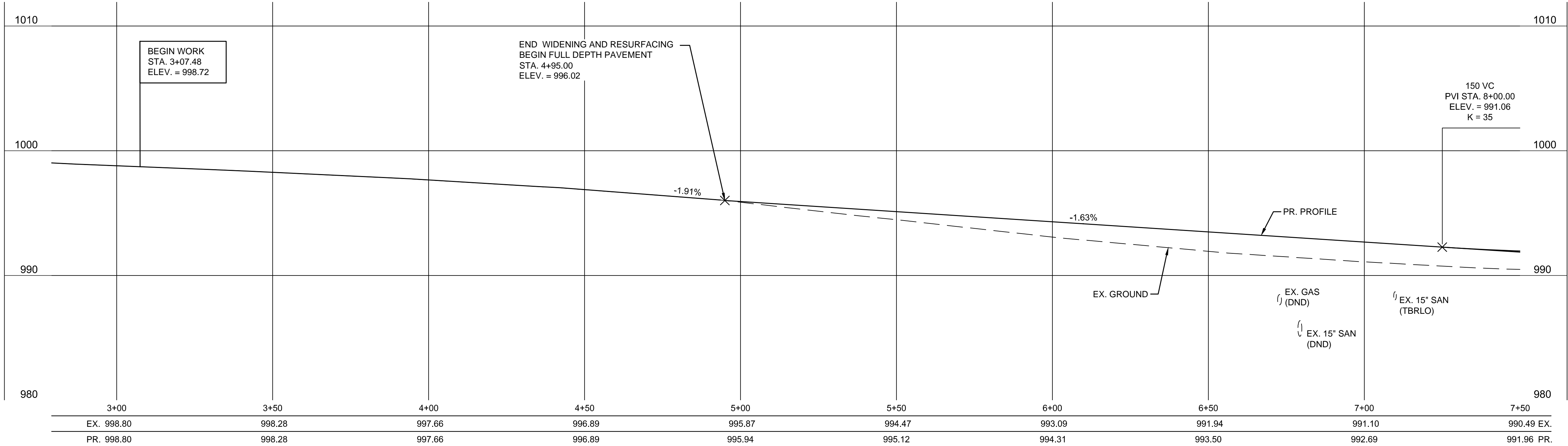
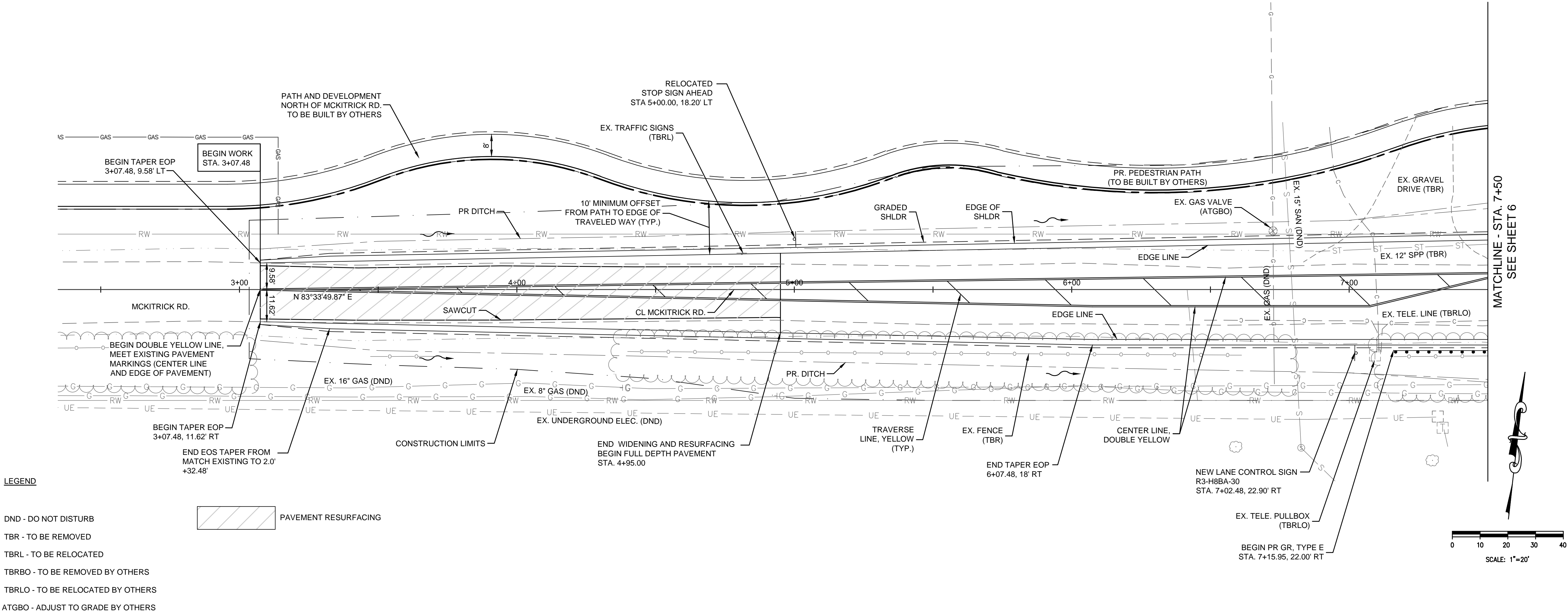
SCALE: AS NOTED

DRAWING

6/23

N:\03\60\06838\00000_MCKITRICK RD\Design\Roadway\Sheets\06838_RD\WY001.dwg, 2/12/2021 1:03 PM, patton, john

NOTE:
MCKITRICK RD. CENTERLINE BEARING AND LOCATION
BASED OFF EXISTING CENTERLINE OF RIGHT OF WAY.
PROPOSED CL STATIONING MAY DIFFER FROM EX. R/W CL.

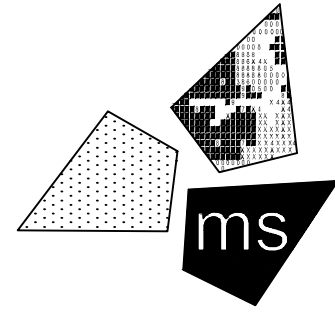


APPENDIX SHEET 7

ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND
DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT
TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

PLAN SHEET

SEAL

DRAWN: LLK

ENGINEER: JAP

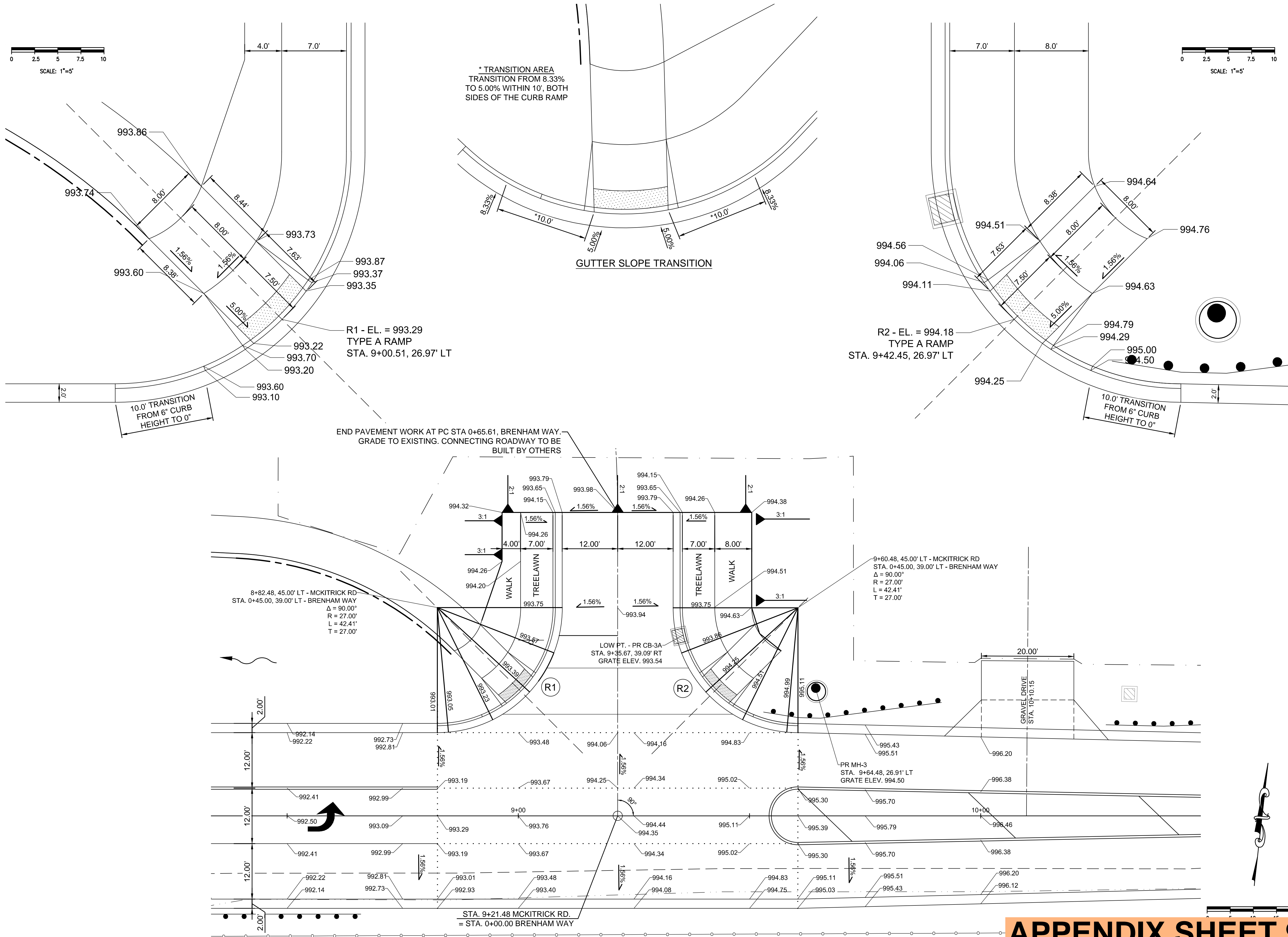
CHECKED: DLT

PROJECT NO: 60-06838

SCALE: AS NOTED

DRAWING

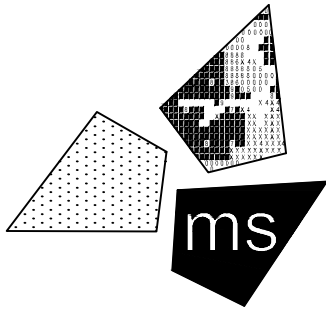
N:\0360\06838\00000_MCKITRICK RD\Design\Roadway\Sheets\06838_INDT001.dwg, 2/12/2021 1:05 PM, patton, john



ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

INTERSECTION DETAIL

SEAL

DRAWN: LLK

ENGINEER: JAP

CHECKED: DLT

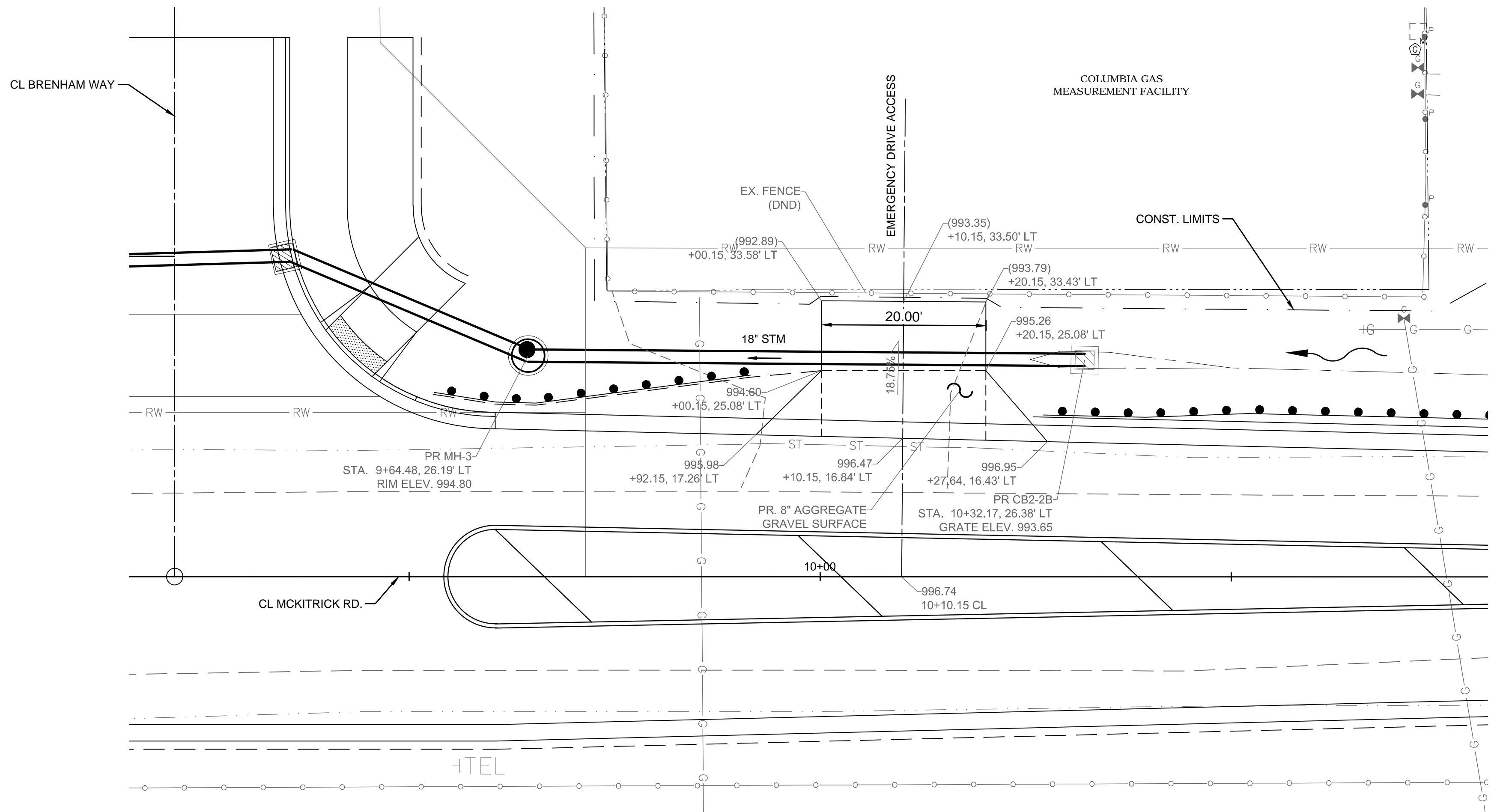
PROJECT NO: 60-06838

SCALE: AS NOTED

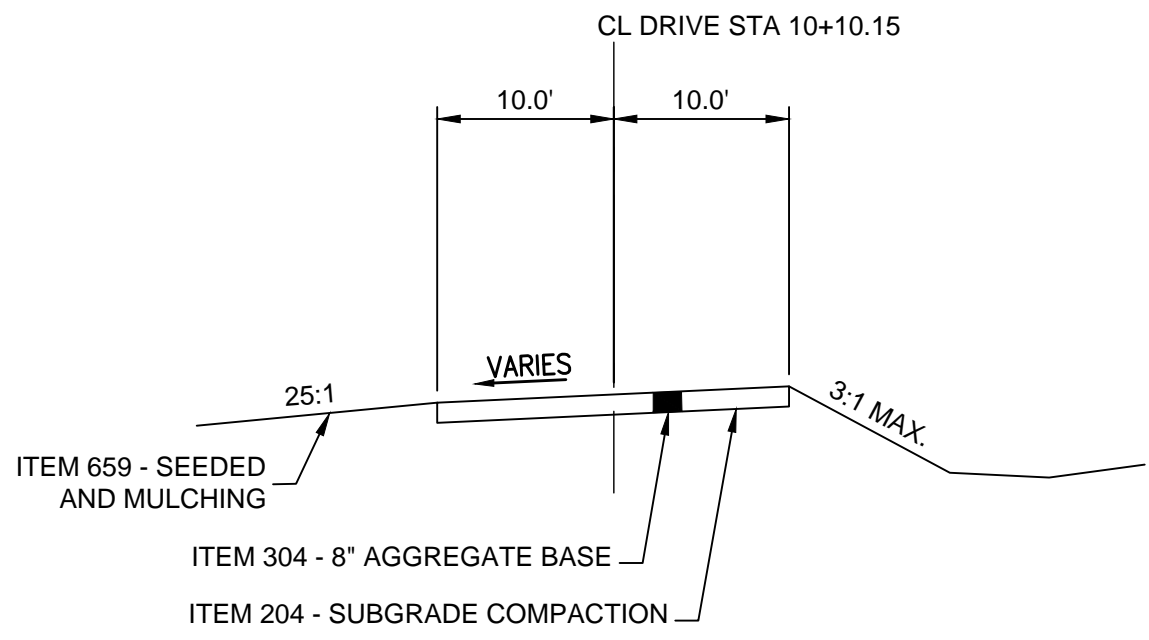
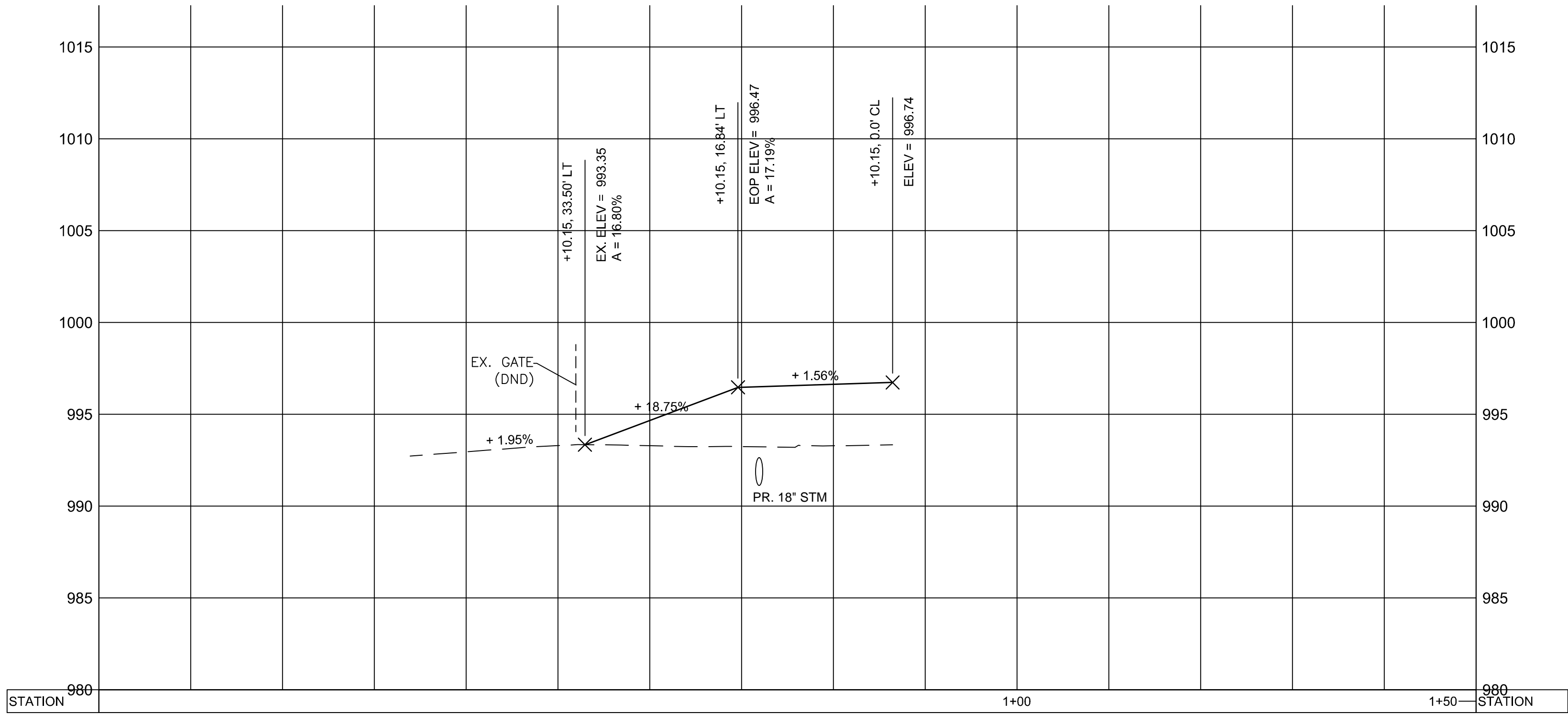
DRAWING

9/23

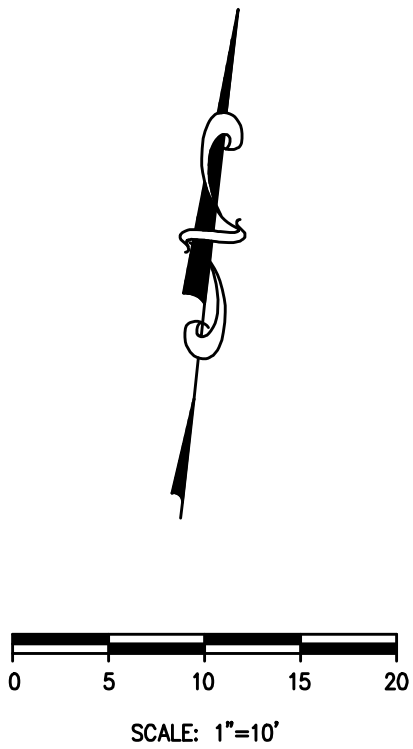
N:\0360\06838\00000_MCKITRICK RD\Design\Roadway\Sheets\06838_DRD001.dwg, 2/12/2021 1:05 PM, patton, john



DRIVE AT STA. 10+10.15



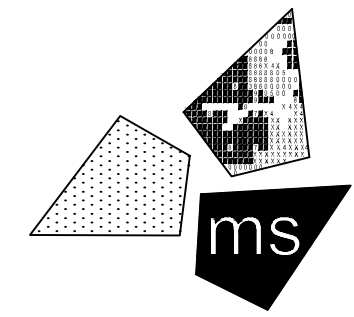
GRAVEL DRIVE TYPICAL SECTION



ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT
TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

DRIVE DETAIL 1

SEAL

DRAWN: LLK

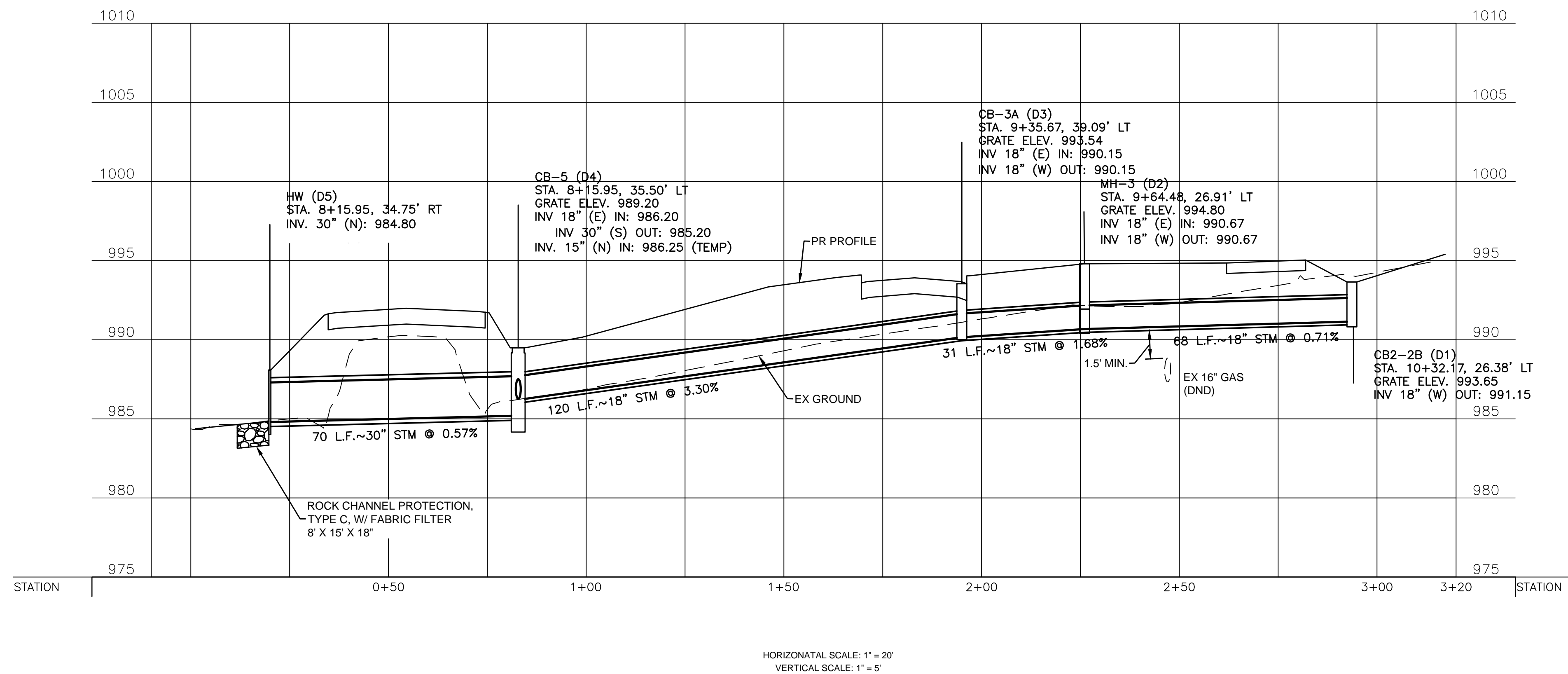
ENGINEER: JAP

CHECKED: DLT

PROJECT NO: 60-06838

SCALE: AS NOTED

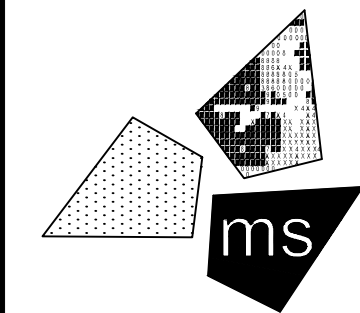
DRAWING

[illegible]

NOTICE

THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND
DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT
TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

STORM SEWER PROFILE

SEAL

DRAWN: LLK

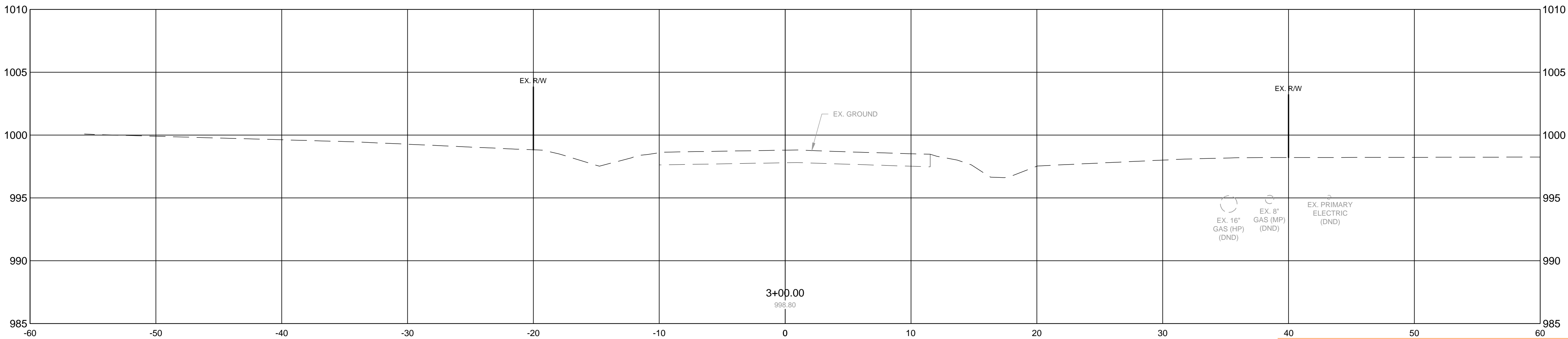
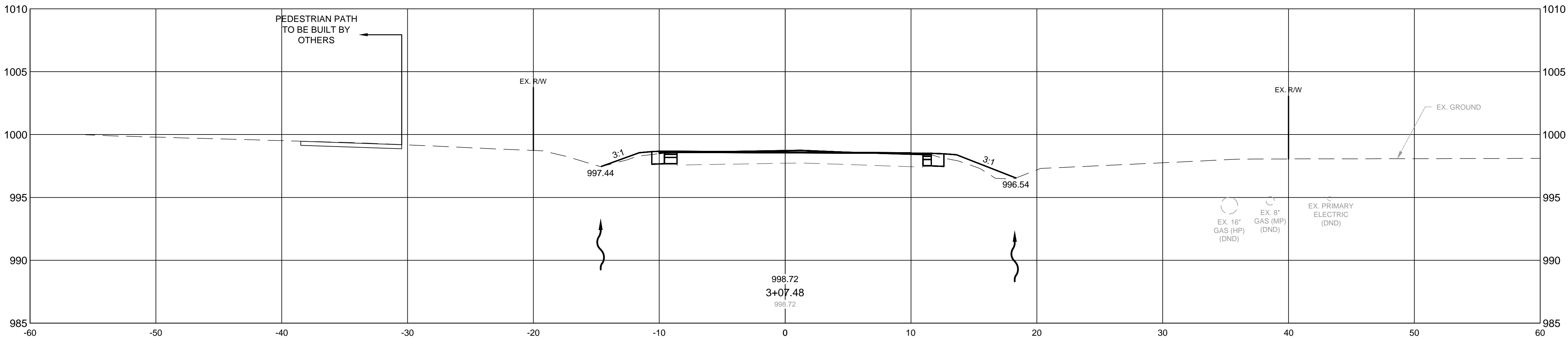
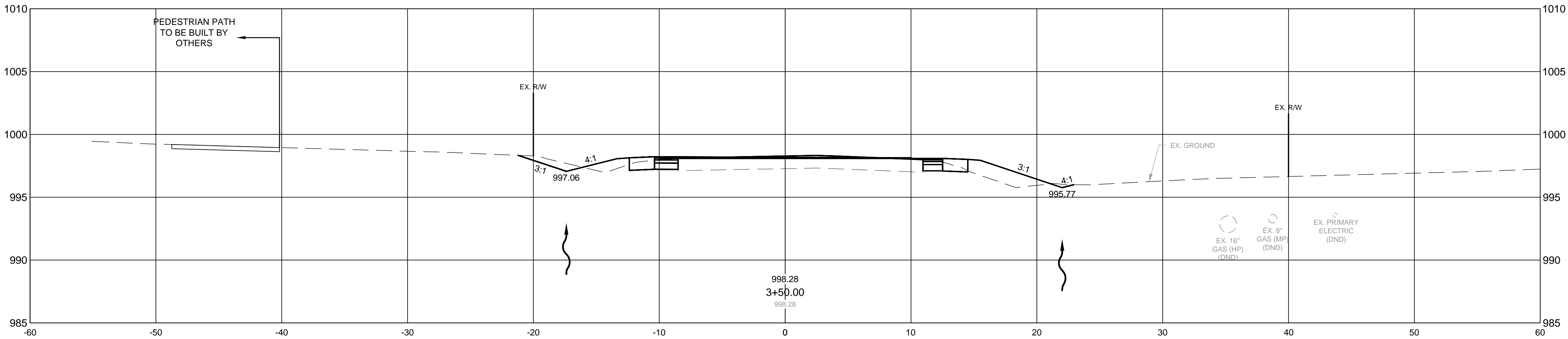
ENGINEER: JAP

CHECKED: DLT

PROJECT NO: 60-06838

SCALE: AS NOTED

DRAWING

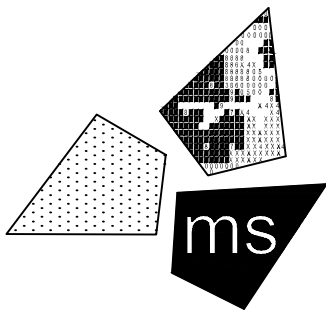


ISSUED:	DATE:

NOTICE

THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

CROSS SECTIONS

SEAL

DRAWN: LLK

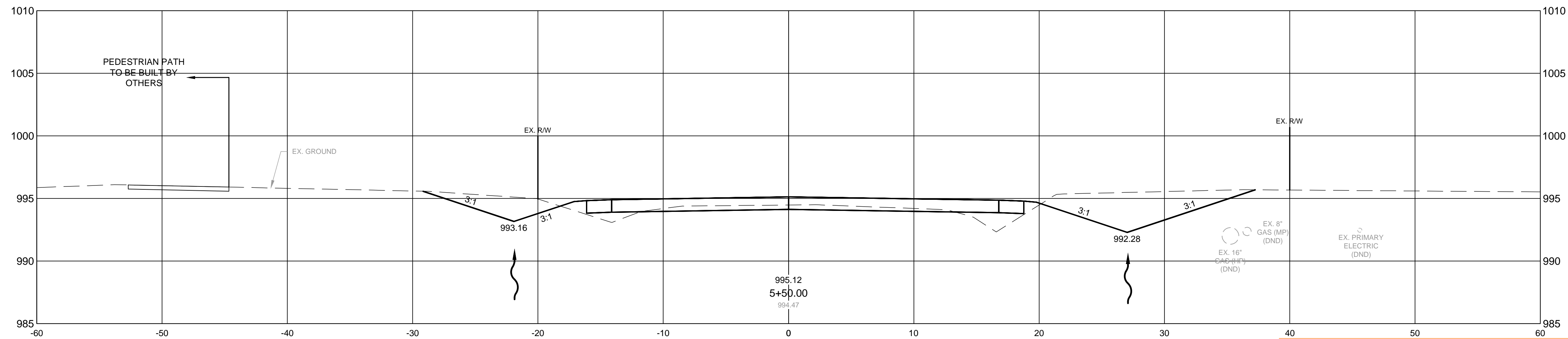
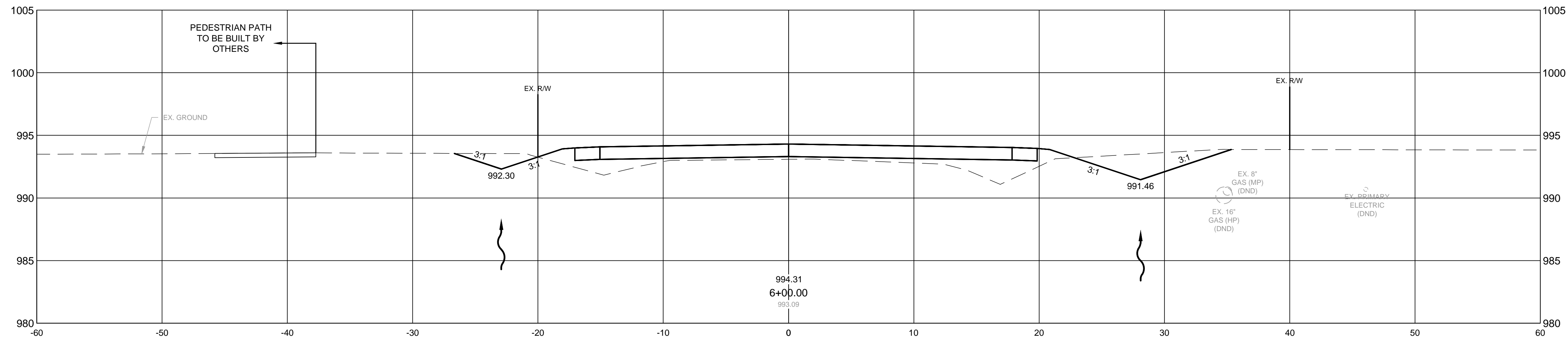
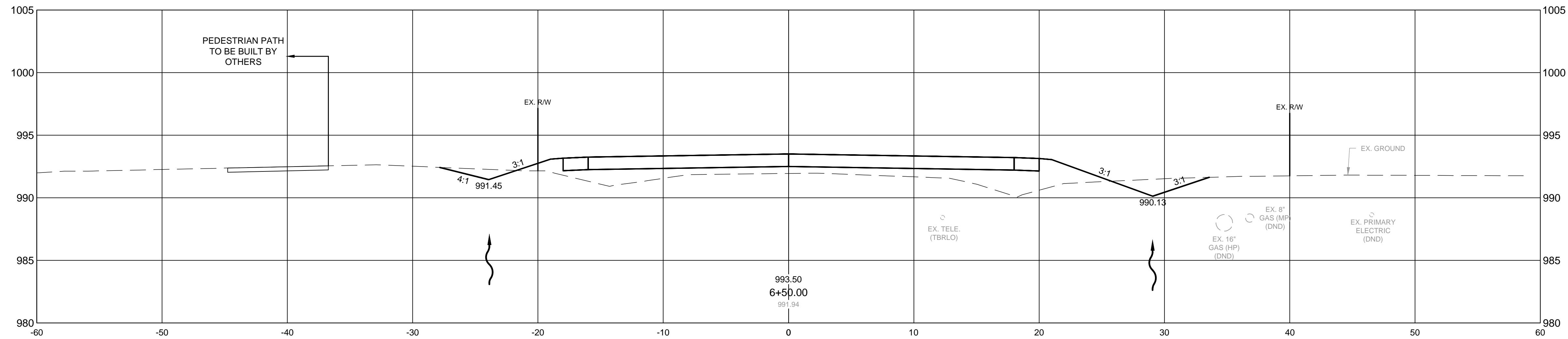
ENGINEER: JAP

CHECKED: DLT

PROJECT NO: 60-06838

SCALE: AS NOTED

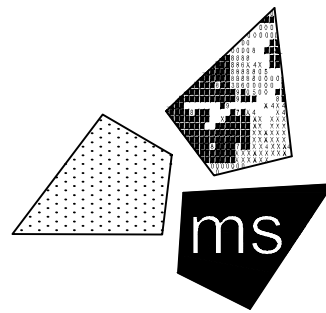
DRAWING



ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

CROSS SECTIONS

SEAL

DRAWN: LLK

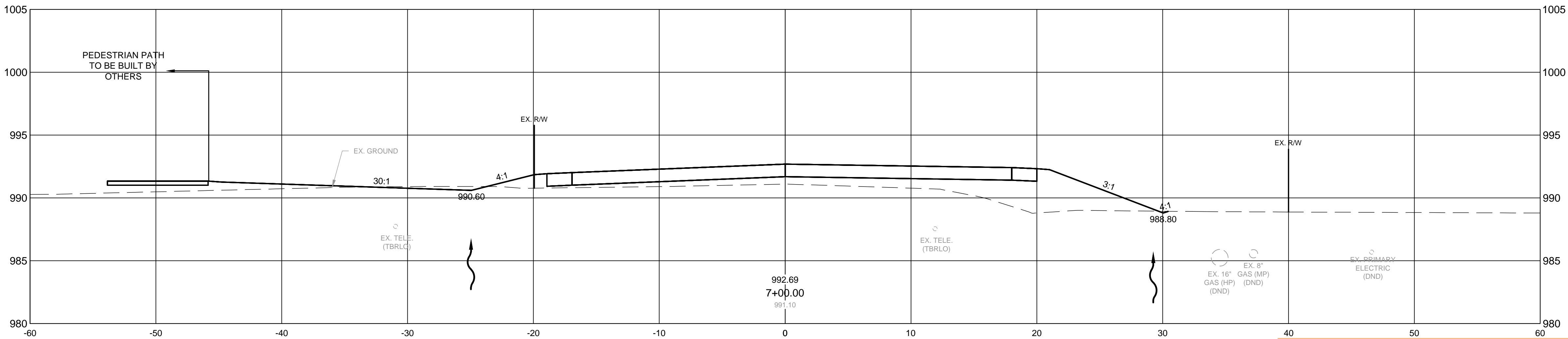
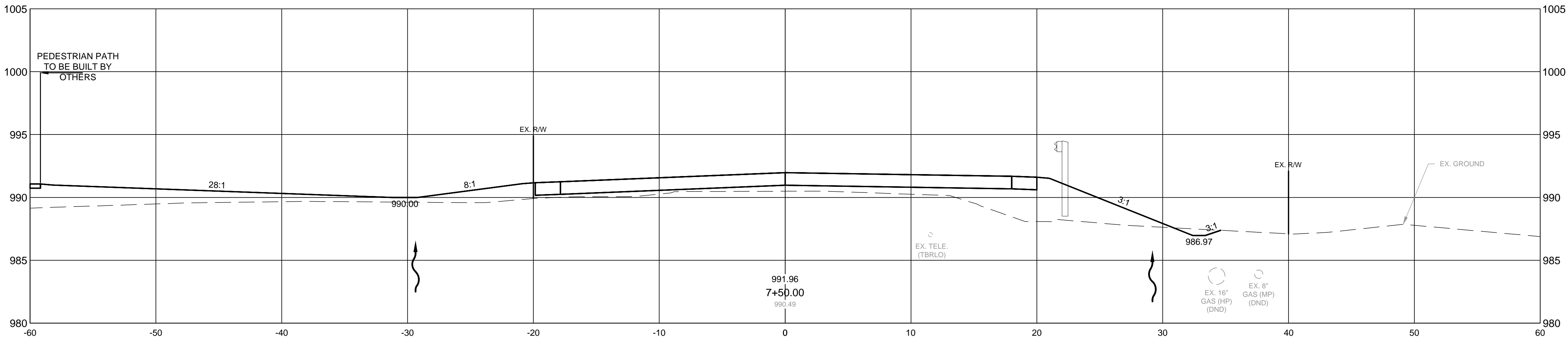
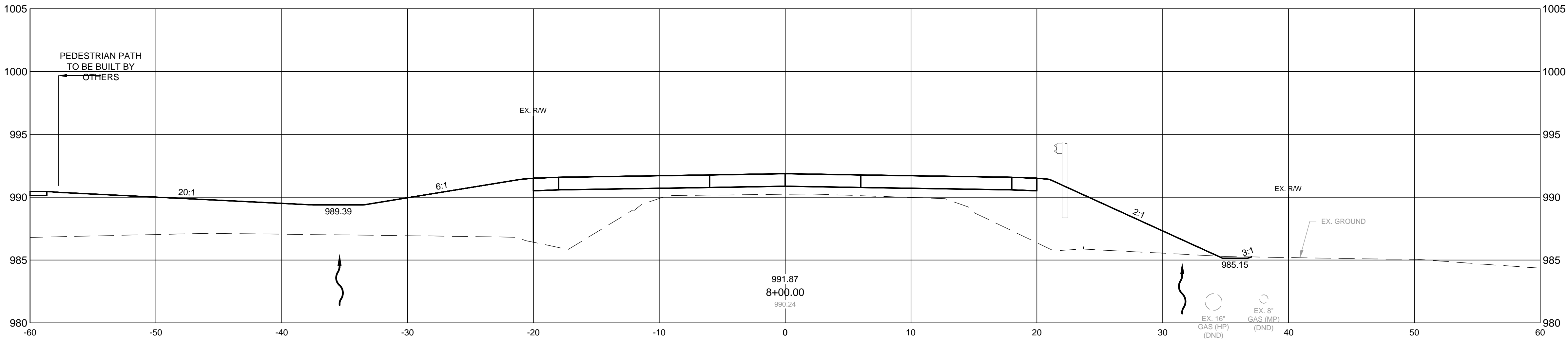
ENGINEER: JAP

CHECKED: DLT

PROJECT NO: 60-06838

SCALE: AS NOTED

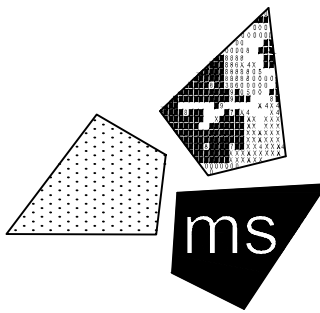
DRAWING



ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

CROSS SECTIONS

SEAL

DRAWN: LLK

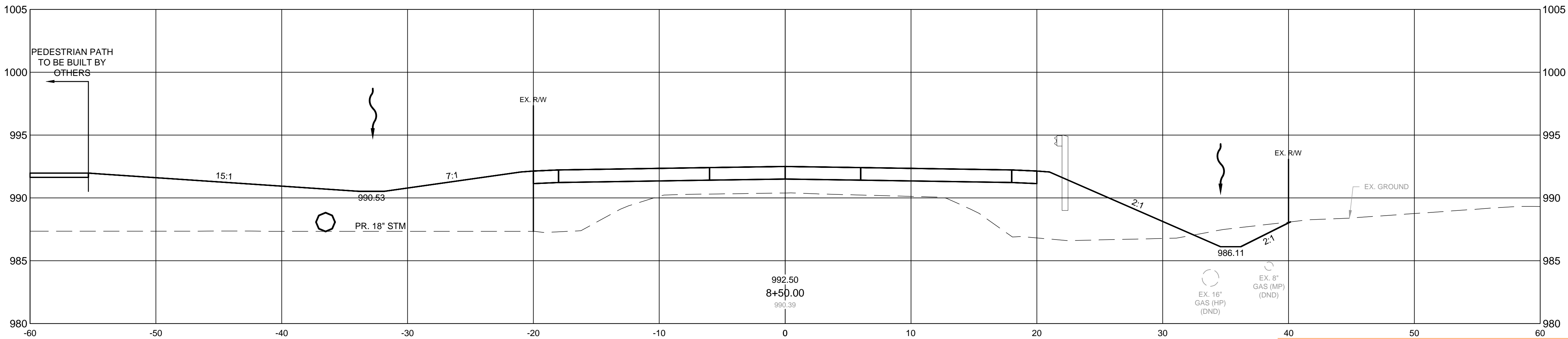
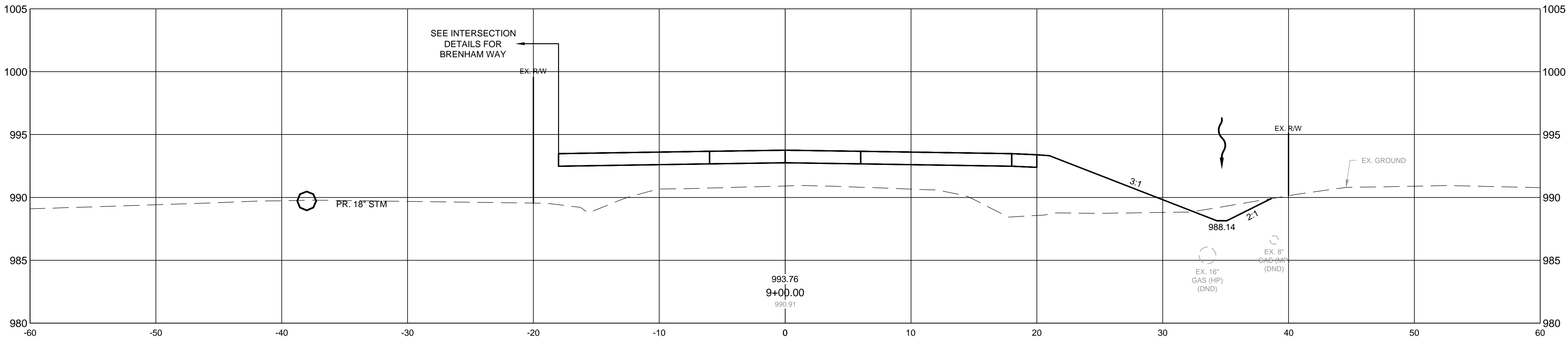
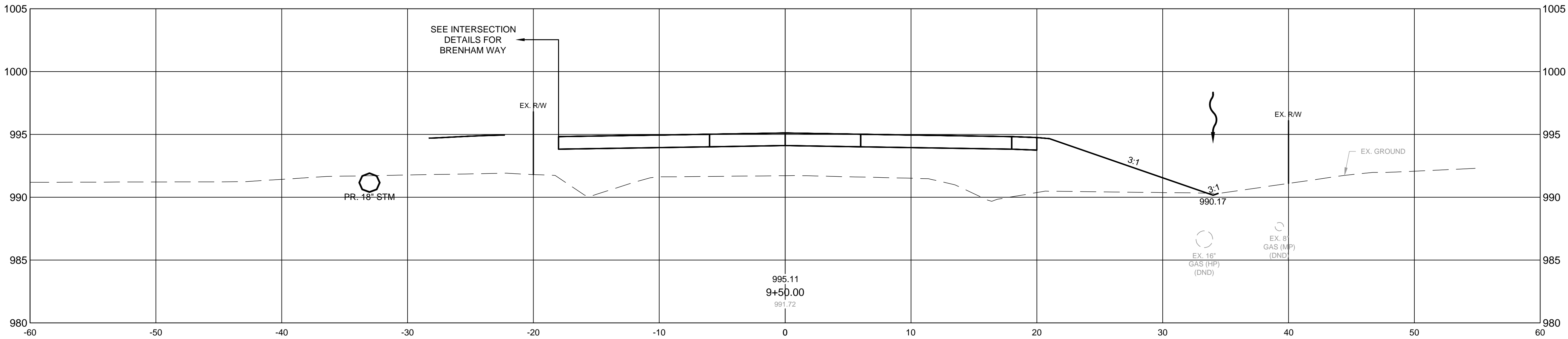
ENGINEER: JAP

CHECKED: DLT

PROJECT NO: 60-06838

SCALE: AS NOTED

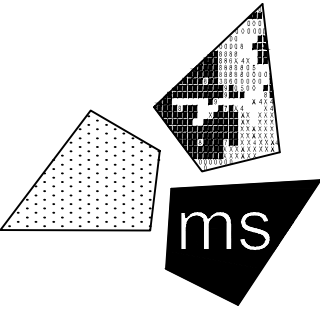
DRAWING



ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

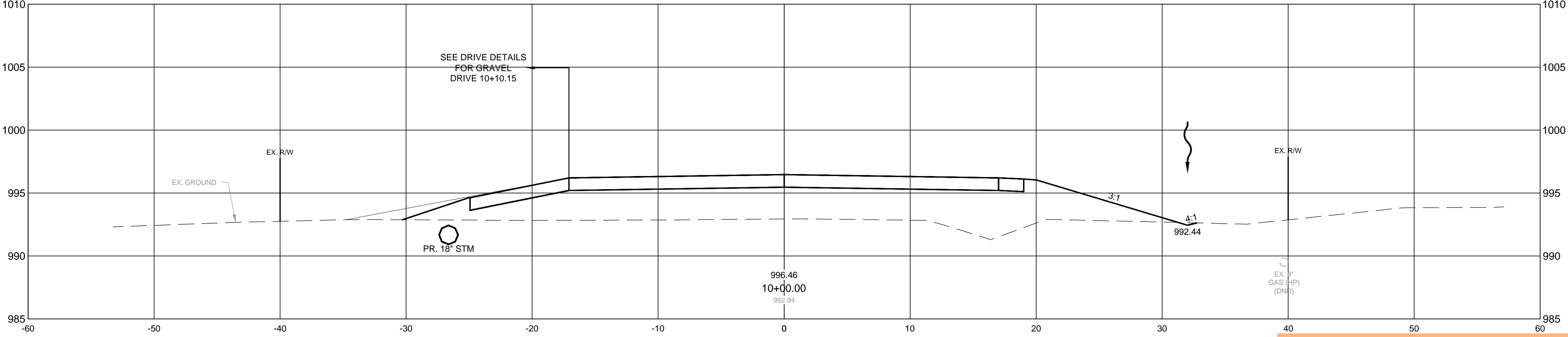
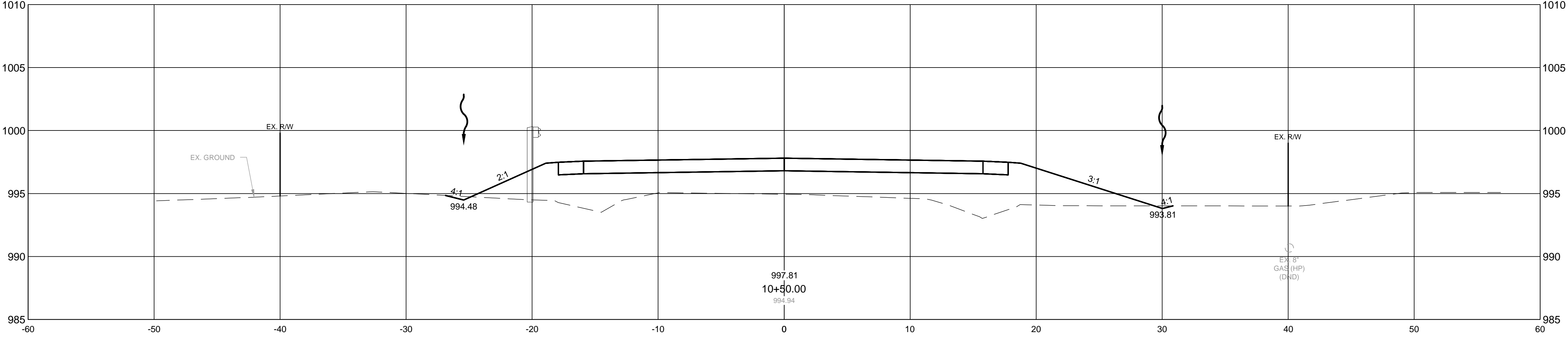
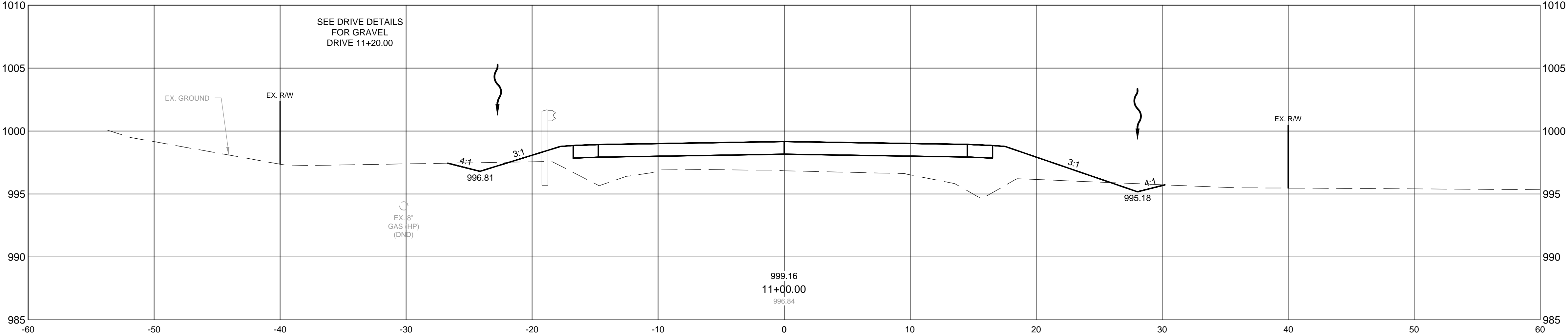
SHEET TITLE

CROSS SECTIONS

SEAL

DRAWN: LLK
ENGINEER: JAP
CHECKED: DLT
PROJECT NO: 60-06838
SCALE: AS NOTED

DRAWING



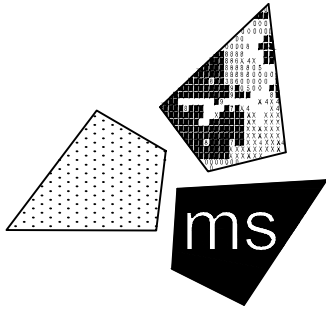
ISSUED:	DATE:

NOTICE

THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT

STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.

engineers, architects, planners

2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT

THE OVERLOOK AT TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

CROSS SECTIONS

SEAL

DRAWN: LLK

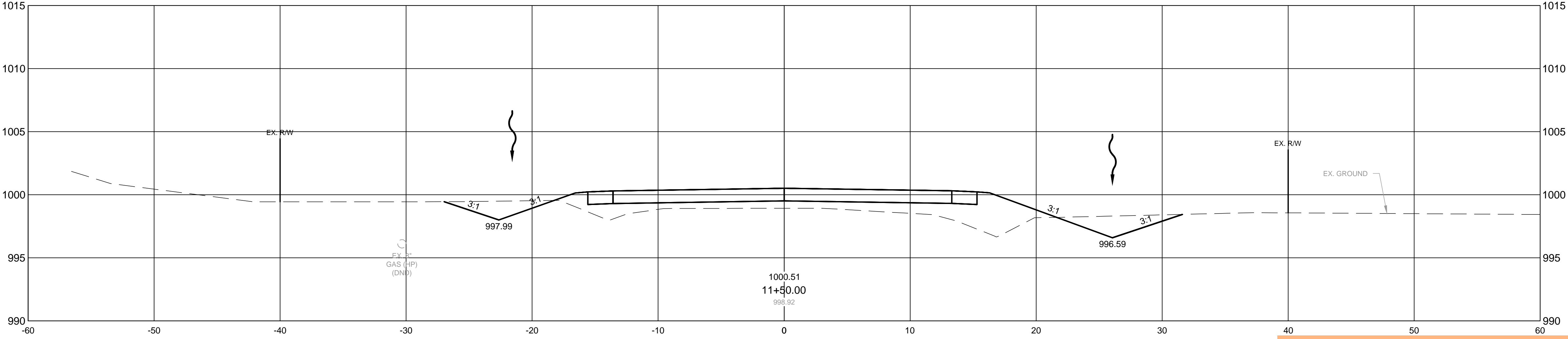
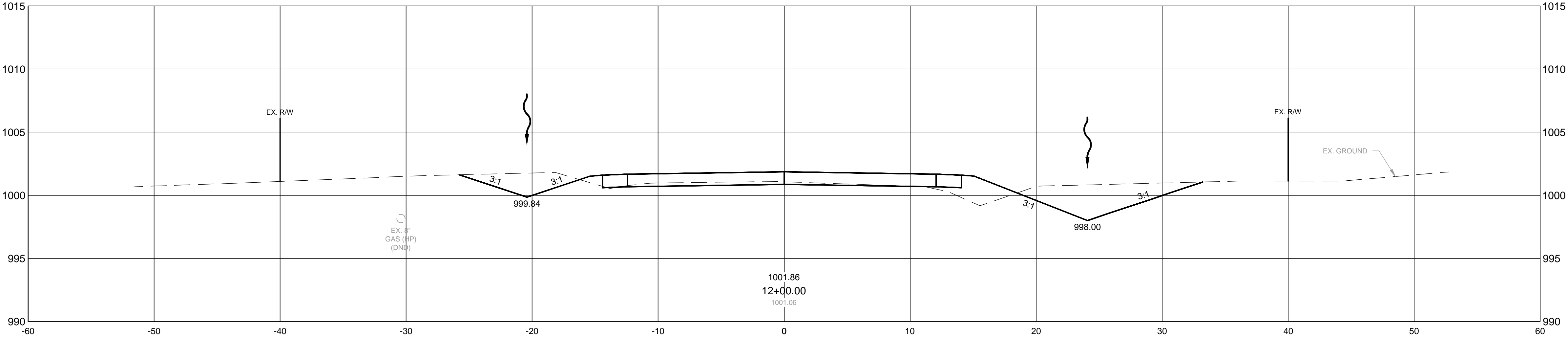
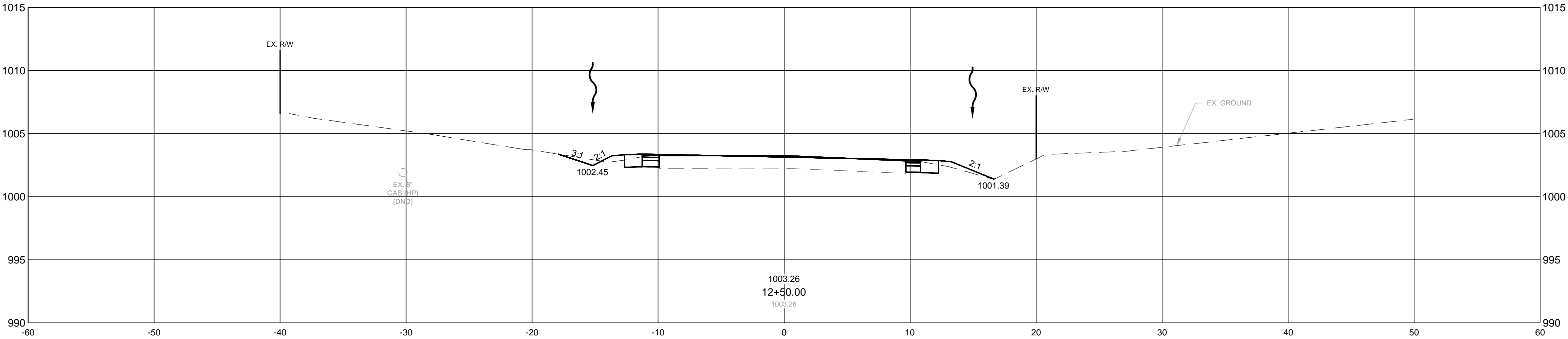
ENGINEER: JAP

CHECKED: DLT

PROJECT NO: 60-06838

SCALE: AS NOTED

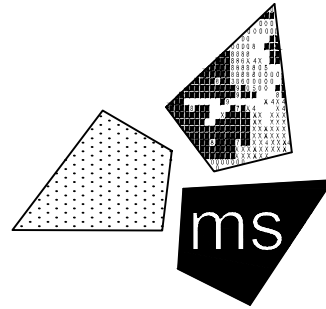
DRAWING



ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

CROSS SECTIONS

SEAL

DRAWN: LLK

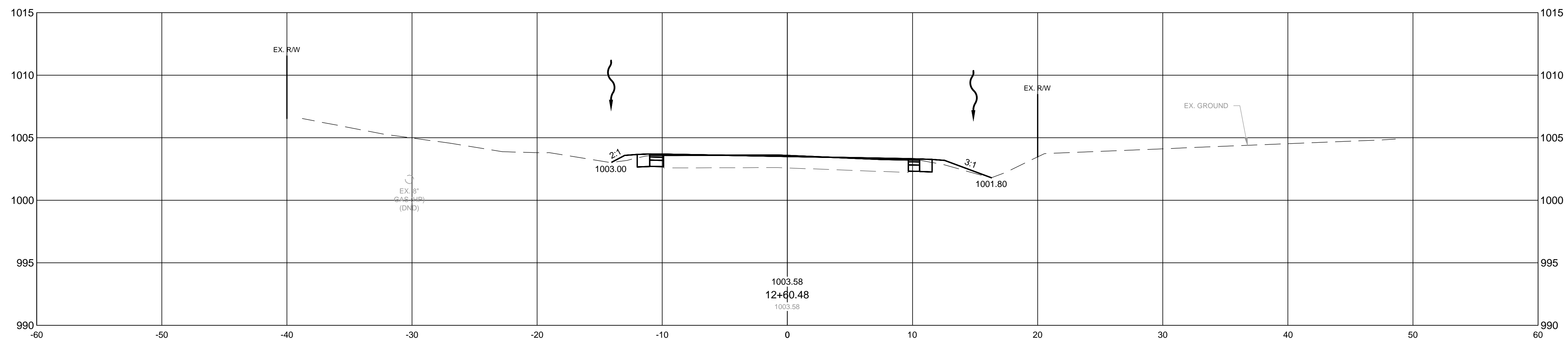
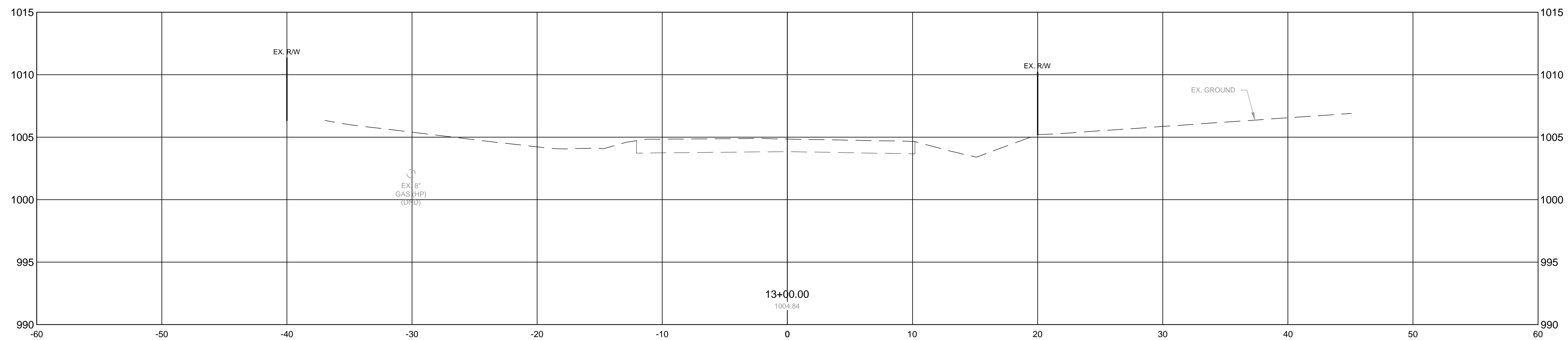
ENGINEER: JAP

CHECKED: DLT

PROJECT NO: 60-06838

SCALE: AS NOTED

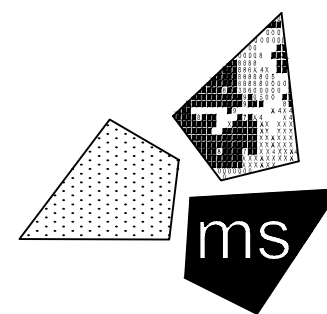
DRAWING

[illegible]

NOTICE

THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND
DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT
TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE

CROSS SECTIONS

SEAL

DRAWN: LLK

ENGINEER: JAP

CHECKED: DLT

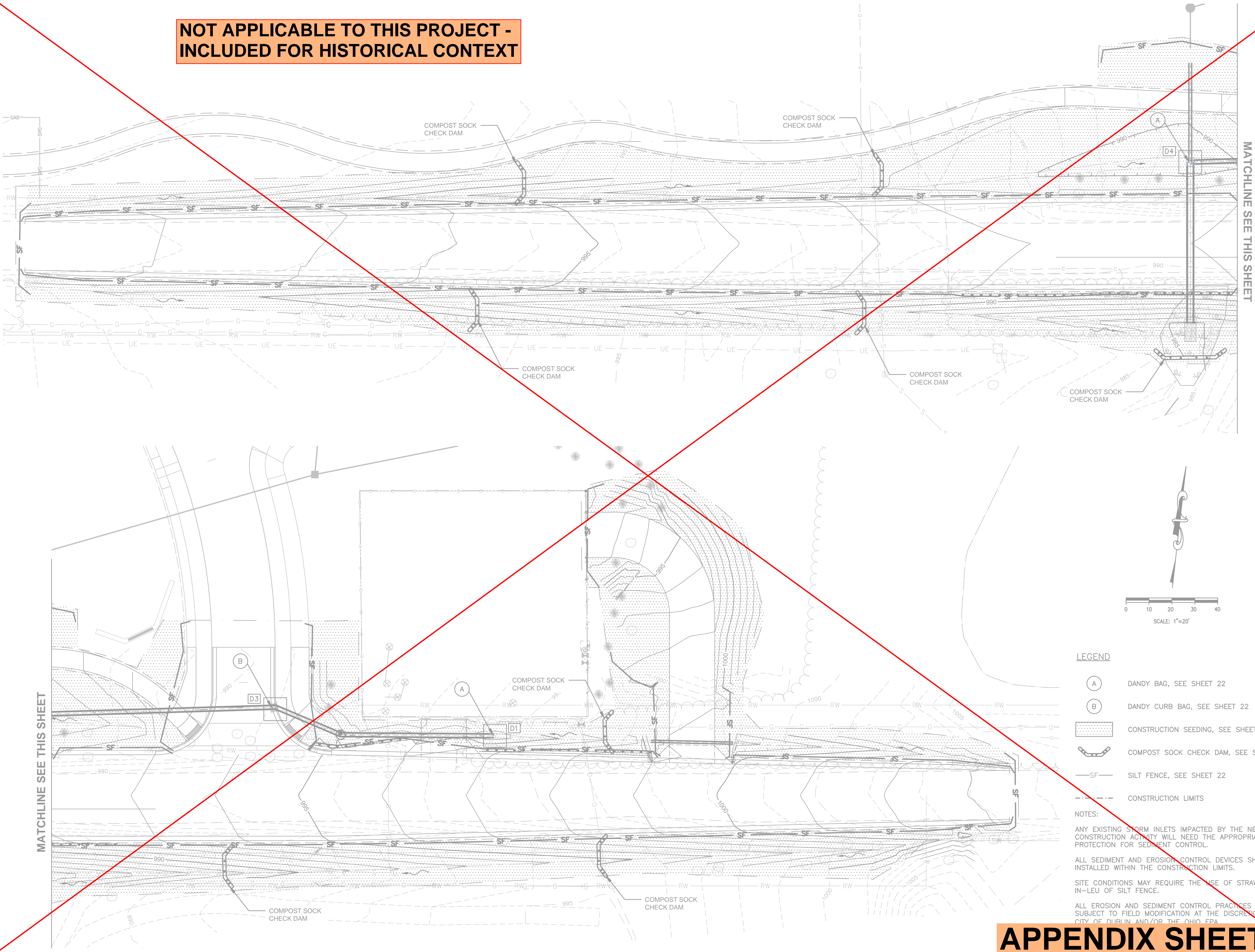
PROJECT NO: 60-06838

SCALE: AS NOTED

DRAWING

N:\0360\06838\00000_MCKITRICK RD\Design\Drainage\Sheets\06838_SWPPP003.dwg, 2/12/2021 1:07 PM, patton, john

NOT APPLICABLE TO THIS PROJECT -
INCLUDED FOR HISTORICAL CONTEXT



LEGEND

- (A) DANDY BAG, SEE SHEET 22
- (B) DANDY CURB BAG, SEE SHEET 22
- [Pattern] CONSTRUCTION SEEDING, SEE SHEET 22
- [Symbol] COMPOST SOCK CHECK DAM, SEE SHEET 21
- SF— SILT FENCE, SEE SHEET 22
- CONSTRUCTION LIMITS

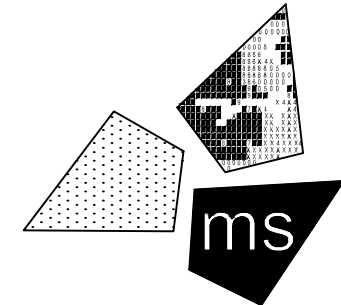
NOTES:
ANY EXISTING STORM INLETS IMPACTED BY THE NEW CONSTRUCTION ACTIVITY WILL NEED THE APPROPRIATE INLET PROTECTION FOR SEDIMENT CONTROL.
ALL SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSTALLED WITHIN THE CONSTRUCTION LIMITS.
SITE CONDITIONS MAY REQUIRE THE USE OF STRAW WATTLE IN-LEU OF SILT FENCE.
ALL EROSION AND SEDIMENT CONTROL PRACTICES ARE SUBJECT TO FIELD MODIFICATION AT THE DISCRETION OF THE CITY OF DUBLIN AND/OR THE OHIO EPA.

APPENDIX SHEET 23

ISSUED:	DATE:

NOTICE
THIS ARCHITECTURAL AND ENGINEERING DRAWING IS GIVEN IN CONFIDENCE AND SHALL BE USED ONLY PURSUANT TO THE AGREEMENT WITH THE ARCHITECT. NO OTHER USE, DISSEMINATION, OR DUPLICATION MAY BE MADE WITHOUT PRIOR WRITTEN CONSENT OF THE ARCHITECT. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED.

CLIENT
STAVROFF LAND AND DEVELOPMENT, INC.



ms consultants, inc.
engineers, architects, planners
2221 Schrock Road
Columbus, Ohio 43229-1547
phone 614.898.7100
fax 614.898.7570

PROJECT
THE OVERLOOK AT
TARTAN RIDGE
MCKITRICK ROAD
DUBLIN, OH 43064

SHEET TITLE
EROSION AND
SEDIMENT
CONTROL PLAN

SEAL

DRAWN: JAK
ENGINEER: JAK
CHECKED: CAB
PROJECT NO: 60-06838
SCALE: AS NOTED
DRAWING